

Application for a Context Sensitive Solutions (CSS) Transportation Project Site Plan Review

Municipality of Anchorage
Planning Department
PO Box 196650
Anchorage, AK 99519-6650



PETITIONER (Municipal or State Project Manager)		PETITIONER REPRESENTATIVE (IF ANY - Consultant)	
Name (last name first) Baski, Sean (AK DOT&PF)		Name (last name first) Noble, Steve (DOWL)	
Mailing Address 4111 Aviation Ave., Anchorage, AK 99502		Mailing Address 4041 B. St., Anchorage, AK 99503	
Contact Phone: Day: 907-269-0555	Night:	Contact Phone: Day: 907-562-2000	Night:
FAX:		FAX:	
E-mail: sean.baski@alaska.gov		E-mail: snoble@dowl.com	

PROJECT INFORMATION
Project Name: Seward Hwy: 36th Ave Interchange MOA/ADOT Project #: CSHWY00298
Community Council(s): Midtown, Rogers Park, Tudor Area
Project description (location): Alaska DOT&PF is proposing to reconstruct the signalized intersection of 36th Ave and the Seward Hwy to a controlled access interchange. The project will include extending frontage roads on both sides of the highway from Tudor Rd to Benson Blvd, ADA compliant pedestrian facilities, drainage improvements, utility relocations, illumination, vegetation, driveway modifications, improvements at adjacent intersections to align with the interchange, new bridges/overpasses and other structures. The project area encompasses 36th Ave between LaTouche St and Denali St, and approximately one mile of Seward hwy between the Tudor interchange and Benson Blvd intersection.

TRANSPORTATION PROJECT SITE PLAN APPROVAL REQUESTED
<input type="radio"/> Context Sensitive Solutions Concept Report (Planning and Zoning Commission) <input checked="" type="radio"/> Draft Design Study Report (Planning and Zoning Commission) <input type="radio"/> Plans in Hand (Urban Design Commission)

I hereby certify that (I am)(I have been authorized to act for) owner of the property described above and that I petition for a site plan review in conformance with Title 21 of the Anchorage Municipal, Code of Ordinances. I understand that payment of the application fee is nonrefundable and is to cover the costs associated with processing this application, and that it does not assure approval of the site plan. I also understand that assigned hearing dates are tentative and may have to be postponed by Planning Department staff, the Planning and Zoning Commission or Urban Design Commission for administrative reasons.

12/27/2021	
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Date

Signature (Agents must provide written proof of authorization)

Accepted by:	Poster & Affidavit:	Fee	Case Number	Meeting Date
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CONTEXT SENSITIVE SOLUTIONS CONCEPT REPORT DOCUMENTATION

- ☐ Transportation Project Site Plan Review Application Form with original signatures
- ☐ Concept report document
- ☐ Environmental Analysis or Environmental Impact Study, if applicable
- ☐ 15 complete sets of above items, including 1 copy on CD or USB drive, submitted 30 days prior to the desired Planning & Zoning Commission meeting date

DRAFT DESIGN STUDY REPORT (35% LEVEL OF DESIGN) DOCUMENTATION

- ☒ Transportation Project Site Plan Review Application Form with original signatures
- ☒ Narrative addressing AMC 21.03.190B.4.b.i. – xiii.
- ☒ Design Study Report Summary, including:
 1. Introduction
 - A. Location Map and Project Boundaries
 - B. Purpose
 - C. Need
 2. History (Project Origin) and Input from other Planning Documents
 - A. Anchorage Comprehensive Plan
 - B. Local Planning Studies/CIP/TIP/LRTP
 - C. Anchorage Pedestrian Plan or Areawide Trails Plan
 3. Existing Conditions
 - A. Right-of-Way Availability
 - B. Traffic Conditions
 - C. Pedestrian Conditions
 - D. Context (Land Use, Street Character)
 - E. Existing Landscape
 - F. Existing Utilities
 - G. Existing Drainage
 4. Design Standards

What Standard is the project being designed to? (Collector, Arterial, OSHP Classification, LRTP typology, etc.)

How do existing conditions impact the ability to meet those standards?
 5. Design Alternatives
 - A. Design narrative and graphic for each alternative considered. Note that the discussion of each alternative should address traffic (and traffic calming), parking, pedestrian facilities, drainage, and utilities (to include lighting), and right of way considerations (does right of way need to be purchased?)
 - B. Recommended Alternative with narrative (why is it recommended?) To include a discussion of the landscape approach and other enhancements (gateway features, fencing, etc.)
 6. Public Involvement Summary
 7. Rough Estimated Project Cost
 8. Maintenance Considerations
 9. Response to comments from Concept Report Review
 10. Preliminary Project Plans
- ☐ 17 Complete sets of above items, including 1 copy on CD or USB drive

PLANS IN HAND (55-65% LEVEL OF DESIGN) DOCUMENTATION

- ☐ Road Project Site Plan Review Application Form with original signatures
- ☐ Narrative addressing AMC 21.03.190B.5.c.i. – vii.
- ☐ Memo addressing Review Comments from DSR Review
- ☐ 55% to 65% Project Plans
- ☐ 17 Complete sets of above items, including 1 copy on CD or USB drive



THE STATE
of **ALASKA**
GOVERNOR MIKE DUNLEAVY

Department of Transportation and
Public Facilities

DESIGN & ENGINEERING SERVICES
Central Region Highway Design Section

PO Box 196900
Anchorage, AK 99519-6900
Phone Number: 907 269 0547
Toll Free: 800 770 5263
TDD: 907 269 0473
TTY: 800 770 8973
Web Site: dot.state.ak.us

November 4, 2021

Michelle McNulty, Director
Planning Department
Municipality of Anchorage
PO Box 196650
Anchorage, AK 99519-6650

Regarding: Seward Highway: 36th Avenue Interchange Project
IRIS Project No: CSHWY00298

Dear Ms. McNulty:

Alaska Department of Transportation and Public Facilities is the project manager for the Seward Highway: 36th Avenue Interchange Project. We authorize DOWL to act on our behalf to complete and submit all the Context Sensitive Solutions Transportation Project actions for the abovementioned project.

I can be reached via email at sean.baski@alaska.gov or via telephone 907-269-0547.

Sincerely,

A handwritten signature in blue ink that reads "Sean Baski".

Sean Baski, P.E.,
Project Manager

cc: Steve Noble, P.E., DOWL, Project Manager
Katie Conway, DOWL, Public Involvement

AMC NARRATIVE

TO: MOA Planning Department
FROM: DOWL
DATE: 12/27/2021
PROJECT: Seward Highway: 36th Avenue Interchange
SUBJECT: Narrative addressing AMC 21.03.190B.4.b.i.-xiii.

The attached report summary provides the following information for the Planning and Zoning Commission's review and decision issuance, as listed in Anchorage Municipal Code 21.03.190B.4.b.i.-xiii:

- Existing conditions
- Design standards and criteria
- Alternatives identification, evaluation, and recommended alternative
- Compliance with this title; Chapter 21.03: Review and Approval Procedures
Sec.21.03.190 Street and Trail Review Title 21: Land Use Planning December 3, 2013
Anchorage, Alaska Page 3-76
- Long-term impact on existing and projected land uses in the vicinity
- Short-term and long-term impact of property acquisition for right-of-way
- Impacts on utilities and other public infrastructure, including undergrounding of overhead utilities
- Street illumination
- Maintenance considerations
- Environmental constraints
- Pedestrian and other non-motorized access
- Public involvement summary
- Cost estimate.

SEWARD HIGHWAY: 36TH AVENUE INTERCHANGE

Planning & Zoning Commission Submittal Report

MOA Design Study Report Summary

State Project No: CSHWY00298

Prepared for:

State of Alaska Department of Transportation and Public Facilities
4111 Aviation Avenue
Anchorage, AK 99519

Prepared by:

DOWL
4041 B Street
Anchorage, AK 99503

December 2021

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Appendix A: Preliminary Plans

Appendix B: Public Involvement

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ACRONYMS

AADT	Average Annual Daily Traffic
AASHTO	American Association of State Highway Transportation Officials
ACS	Alaska Communication Systems
ADA	Americans with Disabilities Act
ADAAG	Americans with Disabilities Act Accessibility Guidelines
AMATS	Anchorage Metropolitan Area Transportation Solutions
AWWU	Anchorage Water and Wastewater Utility
CEA	Chugach Electric Association
DCM	Design Criteria Manual
DOT&PF	State of Alaska Department of Transportation & Public Facilities
ENSTAR	ENSTAR Natural Gas Company
EPA	Environmental Protection Agency
GCI	General Communications Inc.
H2H	Seward Highway to Glenn Highway Connection Project
HPCM	Alaska Pre-Construction Manual
LRTP	Long Range Transportation Plan
MADT	Mean Average Daily Traffic
MCR	Midtown Congestion Relief
ML&P	Municipal Light and Power
MOA	Municipality of Anchorage
MPH	miles per hour
MTP	Metropolitan Transportation Plan
NEPA	National Environmental Policy Act
NHS	National Highway System
OS&HP	Official Streets and Highways Plan
PEL	Planning and Environmental Linkages
PIP	Public Involvement Plan
ROW	Right-of-Way
STIP	Statewide Transportation Improvement Program
TIP	Transportation Improvement Program

PROJECT TEAM

State of Alaska Department of Transportation and Public Facilities

Sean Baski, PE
Project Manager
sean.baski@alaska.gov

DOWL

Steve Noble, PE
Project Manager
snoble@dowl.com

Renee Whitesell, PTP
Transportation Planning Lead
rwhitesell@dowl.com

Rachel Steer
Public Involvement Lead
rsteer@dowl.com

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1.0 INTRODUCTION

1.1 Introduction

The State of Alaska Department of Transportation and Public Facilities (DOT&PF) is proposing to construct a new interchange at the intersection of Seward Highway and 36th Avenue to improve safety and connectivity and reduce delay (Figure 1).

This intersection is one of the busiest and most crash-prone in Alaska. Pedestrians and bicyclists report that it is difficult to cross the highway, and users report a general lack of east-west connectivity in the area. Planned improvements include grade separations, highway ramps, non-motorized facilities, and new frontage roads connecting Tudor Road to 36th Avenue. This project will:

- Improve flow for all travel modes
- Support the separation of local trips from through trips
- Improve facilities for pedestrians and bicyclists
- Increase safety
- Reduce delays

This is currently a state-funded project. The DOT&PF plans to seek federal funding for future stages by adding this project to the Statewide Transportation Improvement Program (STIP).

This Planning & Zoning Commission Submittal Report summarizes the project background, outlines existing conditions, plans the framework for developing design solutions, and presents the design alternative recommended for further development toward the plans in hand submission.

1.2 Location Map and Project Boundaries

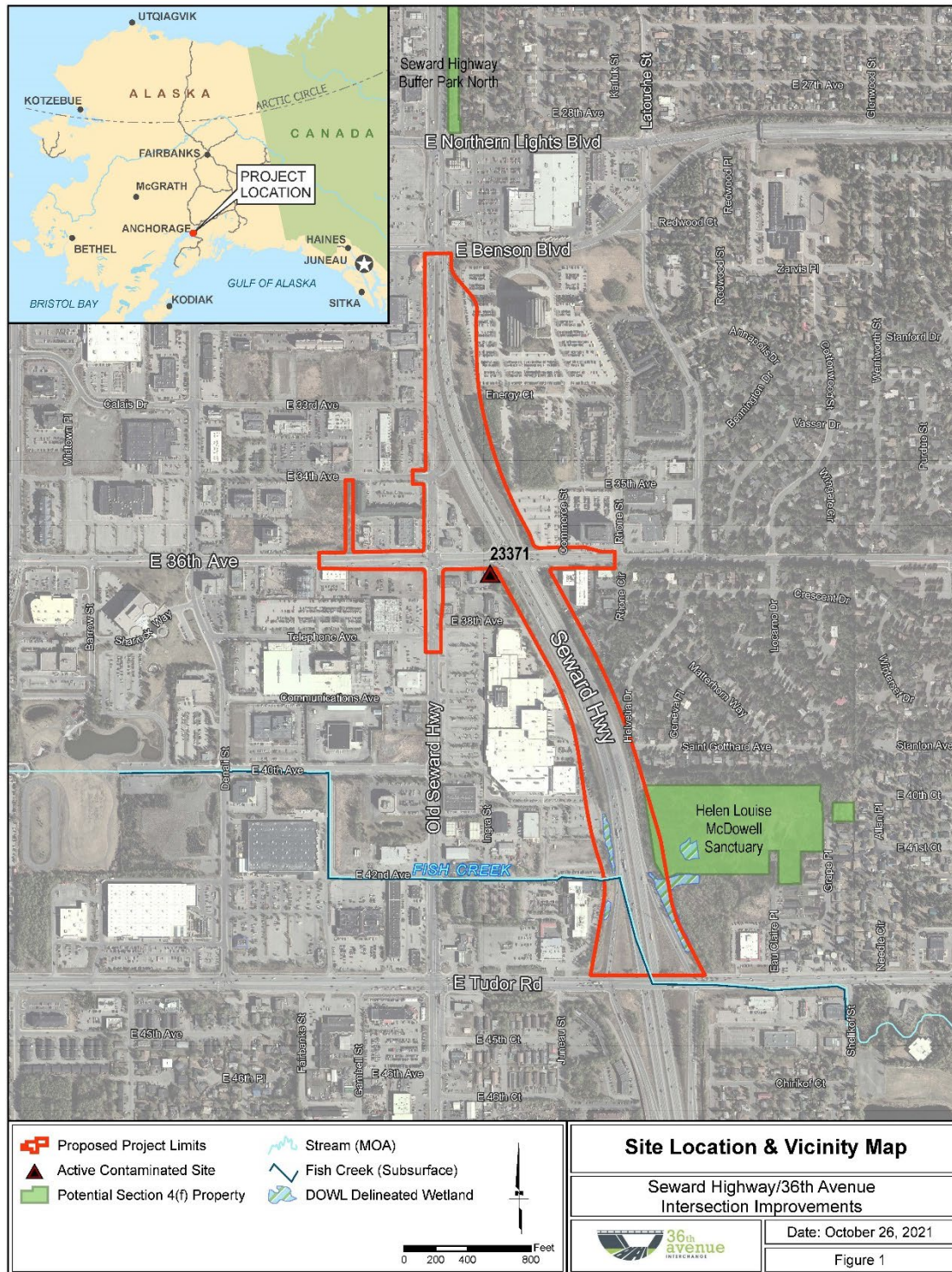


Figure 1: Project Location

Project boundaries are defined by 36th Avenue from approximately LaTouche Street to the east and Denali Street to the west, and by approximately one mile along the Seward Highway between the Tudor Interchange to the south and Benson Boulevard to the north.

1.3 Purpose

The purpose of the proposed project is to improve safety and connectivity and reduce delay by constructing a new grade-separated interchange at the intersection of 36th Avenue and the Seward Highway. This intersection is one of the busiest and most crash-prone in Alaska. Pedestrians and bicyclists report that it is difficult to cross the highway, and users report a general lack of east-west connectivity in the area. Planned improvements include grade separations, highway ramps, non-motorized facilities, and new frontage roads connecting Tudor Road to 36th Avenue. This project will:

- Improve flow for all travel modes
- Support the separation of local trips from through trips
- Improve facilities for pedestrians and bicyclists
- Increase safety
- Reduce delays

This is currently a state-funded project. DOT&PF plans to seek federal funding for future stages by adding this project to the STIP. The project is identified as a Short Term (2018 to 2030) Recommended 2040 Metropolitan Transportation Plan (MTP) Road Project in the Anchorage Metropolitan Area Transportation Solutions (AMATS) MTP 2040.

1.4 Need

DOT&PF is proposing to construct a new interchange at the intersection of the Seward Highway and 36th Avenue. The improvements are needed because of:

- Deficient and/or absent pedestrian and bicycle facilities that result in uncomfortable travel conditions and a potentially unsafe travel environment
- Elevated vehicular collision rates
- Mixing of travelers seeking to pass through Midtown to destinations north and south (regional travel) and travelers moving within Midtown (local travel), which creates conflict, delays, elevated traffic volumes, and mixing of travel modes
- Congestion and delay, particularly on 36th Avenue for motorized and non-motorized travelers seeking to cross the Seward Highway to travel east or west.

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2.0 HISTORY (PROJECT ORIGIN) AND INPUT FROM OTHER PLANNING DOCUMENTS

The Seward Highway: 36th Avenue Interchange project has a long history dating back at least to the AMATS Major Corridors Study (March 1982), which evaluated alternatives to connect the Seward Highway from its intersection with Tudor Road through downtown Anchorage to the Glenn Highway. This project grew into the Seward Highway to Glenn Highway Connection Project (H2H), which was paused in 2011 due to concerns about the scale of the project and public push-back.

In 2012, DOT&PF chose to focus only on the Midtown section of the previous H2H project, as alternatives for improvements between the Tudor Road interchange and Chester Creek shared the same configuration and would not jeopardize flexibility to ultimately create a connection between the Seward Highway and Glenn Highway. In late 2012, DOT&PF further revised the project scope to focus on a standalone project at the intersection of Seward Highway and 36th Avenue. This project was named Seward and 36th Improvements. The project proceeded to approximately 35 percent design, but there were significant public concerns raised about the preferred alternative interchange layout, particularly northbound access to Seward Highway from 36th Avenue. As a result of the public response, the project was paused in early 2015.

In 2017, DOT&PF moved forward with a Planning and Environmental Linkages (PEL) Study focused on the Seward Highway from the Tudor Road Interchange to approximately 20th Avenue (known as Midtown Congestion Relief, or “MCR”). The PEL Study recommend five separate projects with independent purpose and need and logical termini that could be constructed to alleviate congestion and enhance safety. The first project moving forward from the MCR PEL Study is the 36th Avenue Interchange, which is a combination of two projects recommended in the PEL Study.

The project team has and will continue to consider the following planning documents during the design development:

2.1 Alaska Statewide Long-Range Transportation Plan “Let’s Keep Moving 2036” (2016)

The Alaska Statewide Long-Range Transportation Plan “Let’s Keep Moving 2036” (LRTP) is a system-level transportation plan that addresses all modes and identifies policy priorities. It does not list projects and identify local transportation priorities. The 2036 vision for the transportation system is *“to provide a network that enables a robust and growing economy and meets the mobility needs of the state’s residents.”* The LRTP sets a policy framework, including prioritizing investments that increase productivity and reliability and reduce safety risks, focuses investment on the National Highway System (NHS), increasing pedestrian and bicycle facilities in urban areas through corridor plans, ensuring freight corridors are retained and improved, and addressing corridor preservation and access management in corridor plans.

2.2 MCR PEL Study (DOT&PF, April 2020)

The MCR PEL Study focused on the area surrounding the Seward Highway corridor from the Tudor Road interchange to approximately the 20th Avenue intersection through Midtown Anchorage. It sought to comprehensively develop and evaluate corridor concepts that would address safety and congestion issues where the Seward Highway transitions from a controlled

access freeway south of Tudor Road to a slower speed major arterial road at approximately 36th Avenue. The Seward Highway: 36th Avenue Interchange Project is the first project to move forward from the MCR PEL Study.

2.3 Alaska Statewide Active Transportation Master Plan (DOT&PF, July 2019)

The Alaska Statewide Active Transportation Master Plan is a modal plan focusing on walking and bicycling to improve safety, increase accessibility, promote healthy lifestyles, and develop a safer and more efficient active transportation network and infrastructure. The plan has a vision that *“people in Alaska will enjoy equitable, accessible, safer walking and bicycling opportunities as an integral part of daily life.”* The plan does not list specific projects, but sets out goal areas, objectives, and recommended actions to improve active transportation in Alaska.

2.4 Municipality of Anchorage Official Streets and Highways Plan – Maps, Policies and Standards (MOA Community Planning and Development, June 2014)

The Official Streets and Highways Plan (OS&HP) enables the community to prepare for future development and growth by establishing the location, classification, and minimum Right-of-Way (ROW) requirements of streets and highways required to accommodate future transportation needs of the community. The plan designates the Seward Highway as a Freeway, street class V, which requires a minimum 150-foot ROW to accommodate the Average Annual Daily Traffic (AADT) of more than 40,000.

The OS&HP notes that freeways are limited access, high-speed roadways with grade-separated interchanges whose only function is to carry traffic. They are highly efficient transporters of goods and people, but they can be major barriers separating land uses and communities. The OS&HP outlines the following general guidelines that should be followed in planning for and phasing freeway construction in Anchorage:

- Freeways should either connect or provide easy access to major traffic generators throughout the urban area. They should also be designed to handle through traffic, although this should be given secondary consideration due to the small percentage of local trips that are classified as through trips within the urban area.
- Freeway locations should not bisect communities, neighborhoods, or other areas whose function would be impaired by such construction, nor should they erect a barrier between populated areas and recreation areas. Where such an area is bisected, provision should be made for access across the freeway, particularly at those locations where non-motorized movement can be expected in the future.
- Construction of freeways should only be considered when the arterial system cannot meet the demand placed upon it. Traffic volumes must be more than the design capacity of major arterials before freeway construction is considered. The total cost of freeway construction, including socio-economic costs must be determined to ensure the best route is selected. Provisions for landscaping, to provide a buffer, improve aesthetics, and to serve as a major entrance to and through the community, should also be considered.

36th Avenue is classified in the OS&HP as a Class II minor arterial street. Minor arterial streets are intended primarily to move through traffic, but they also provide an important land access function. Access should be at block intervals wherever possible.

- Class II minor arterial streets typically carry 10,000 to 20,000 vehicles per day. They should have two to four moving lanes and paved shoulders for emergency parking, and a minimum ROW width of 80 feet.
- Residential development should be discouraged from abutting directly onto minor arterial streets. Direct access to commercial property must be carefully controlled to limit the number of permitted driveways. Where possible, driveway access should be shared with adjacent property owners.
- Guidelines for minor arterials:
 - Serve as the distribution link between major arterials and lower classification streets.
 - Discourage direct access to minor arterials from individual lots.
 - Connect smaller residential areas with community schools, neighborhood business areas, and recreation facilities.
 - Provide landscaping to buffer areas and improve aesthetics.
 - Connect neighborhoods by providing for safe pedestrian access facilities.

2.5 2040 MTP (MOA, 2020)

The 2040 MTP is the comprehensive, multimodal plan to create a safe and efficient transportation network that will provide transportation choices, support the local economy, and enhance quality of life in Anchorage. It identifies transportation needs that reflect the region's changing population and demand for housing, land use, and transportation choices. It identifies transportation and mobility needs, opportunities, policies, and strategies needed to support the multimodal transportation system. Relevant goals from the 2040 MTP include:

- **Goal 1 Preserve the Existing System:** Maintain the transportation system in a state of good repair.
- **Goal 2 Improve Safety:** Increase the safety and security of the transportation network.
- **Goal 3 Improve Travel Conditions:** Develop an efficient multimodal transportation system to reduce congestion, promote accessibility, and improve system reliability.
- **Goal 4 Support the Economy:** Develop a transportation system that supports a thriving, sustainable, broad-based economy.
- **Goal 5 Promote Environmental Sustainability:** In developing the transportation network, protect, preserve, and enhance the community's natural and built environment and quality of life, including the equity of all users and social justice, while considering our northern climate and supports planned land use patterns.
- **Goal 6 Quality Decision Making:** Make sound public investments.

Transportation projects must be programmed into the Transportation Improvement Program (TIP). The AMATS TIP is included in the STIP. Each programming document identifies the most likely funding source and ranks the projects and programs by priority. Table 7-1 of the 2040 MTP lists recommended short-term projects (2018 to 2030), which includes the Seward Highway: 36th Avenue Interchange project as part of Project 117, as shown in Table 1.

Table 1: Seward Highway: 36th Avenue Interchange in the 2040 MTP

MTP#	Project Name	Project Description	MTP Goals	Address Federal Performance Area	2018 Cost Estimate	Funded Prior to this MTP
117	Midtown Congestion Relief Project	<p>Upon completion of the Midtown Congestion Relief Planning and Environmental Linkages (PEL) Study and after the National Environmental Policy Act (NEPA) process identifies a preferred alternative purchase Right of Way for future improvements from Tudor Road to 20th Avenue and extend the Seward Highway frontage roads. Project would include interim projects identified as part of the Midtown Congestion Relief PEL. Project would include non-motorized improvements and consider adjacent land use.</p> <p>Purpose: Connectivity, Access, Congestion, Capacity, and Freight (Regional Truck Route).</p> <p>Key Land Use Features: None.</p>	3, 4, 6	Environmental sustainability, Freight movement/ Economic vitality, Performance of the NHS	\$193,288,000	

2.6 Anchorage Pedestrian Plan (AMATS, October 2007)

The purpose of the Anchorage Pedestrian Plan is to establish a 20-year framework for improvements that will enhance the pedestrian environment and increase opportunities to choose walking as a mode of transportation. The plan focuses on pedestrian facilities adjacent to streets and roadways as well as walkways to connect adjacent subdivisions and schools. It documents existing sidewalks in Anchorage and existing conditions for walking.

The north-western half of the project area (Denali Street to Seward Highway, 36th Avenue to Benson Boulevard) is identified in the Anchorage Pedestrian Plan as a Commercial District. Improving missing sidewalks throughout the District is listed in the Plan as project priority number 161. Additionally, a separated pedestrian crossing at the Seward Highway/36th Avenue intersection is listed as project 89 on the priority list. The Seward Highway: 36th Avenue Interchange project will significantly improve this crossing location.

2.7 Anchorage Bicycle Plan (AMATS, March 2010)

The purpose of the Anchorage Bicycle Plan is to expand bicycle infrastructure and the use of bicycles for transportation. Other integral parts of the plan include programs to improve enforcement, safety, education, and support facilities, such as bicycle parking and signage. The plan considers existing conditions and issues and recommends a bicycle network for the Anchorage Bowl. Within the project area, the Seward Highway is part of the core bicycle network, and Midtown is identified as a major employment center.

In the Anchorage Bicycle Plan, Figure 1 maps existing bicycle infrastructure and identifies an absence of bicycle facilities along the Seward Highway and 36th Avenue. Chapter 3 sets out the recommended bicycle network, and Figure 11 identifies a desire for the Seward Highway to be a key north-south route as part of the core bicycle network. The Plan's Figure 13 provides more detail on the proposed bicycle network, including the following facilities in the project area:

- **Off-Street Facilities:** An existing separated multi-use pathway is mapped on the east side of the Seward Highway between Tudor Road and 36th Avenue. No such facility currently exists. The Seward Highway: 36th Avenue Interchange project proposes to create a shared multimodal pathway on both sides of the Seward Highway to address gaps in the non-motorized network. Additionally, an existing multi-use pathway is mapped in the Bicycle Plan on the north side of 36th Avenue east of the Seward Highway. The Seward Highway: 36th Avenue Interchange project proposes to tie into existing facilities along 36th Avenue and provide for bicycle and pedestrian facilities on both sides of the road through the proposed interchange. This will address existing gaps and provide a significant improvement in bicycle and pedestrian infrastructure over what is existing currently in the project area.
- **On-Street Facilities:** A proposed bicycle lane is shown in the Anchorage Bicycle Plan extending north along the Old Seward Highway through the 36th Avenue intersection. Proposed interchange design and configuration may preclude this facility, and the Anchorage Draft Non-Motorized Plan includes the removal of this facility from the bicycle network (See Section 2.11 of this report). However, non-motorized facilities will be provided at and through the intersection of 36th Avenue and the Old Seward Highway to enable a safer crossing than the existing condition allows.

2.8 Areawide Trails Plan (Department of Community Planning and Development, AMATS, April 1997)

The Areawide Trails Plan is the planning and policy document for the development of trails in the Municipality. It is based on a 20-year planning period, seeks to achieve an integrated trail system that encourages travel by many means, and offers a variety of recreational opportunities. It sets out standards for new trails, including multi-use trails adjacent to existing roadways.

The Trails Plan notes that over six percent of Anchorage households have no automobiles and must rely on other modes of transportation, including safe pedestrian facilities. Neighborhoods with the highest number of households without amenities include Fairview, Downtown, Midtown, and East Anchorage.

2.9 Anchorage Land Use Plan (MOA, September 2017)

Most of the area surrounding and to the west of the Seward Highway within the project corridor is designated as Midtown City Center in Municipality of Anchorage's (MOA) Land Use Plan 2040. A small area of Commercial Corridor is located near the corner of 36th Avenue and LaTouche Street. The project area includes a significant swathe of single-family and two-family neighborhood designations on the east side of Seward Highway. The Helen Louise McDowell Sanctuary, which is east of the Seward Highway between 36th Avenue and Tudor Road, is designated as a park or natural area. A greenway-supported development growth supporting feature is shown generally along the alignment of Fish Creek through the Sanctuary, extending west across the Seward Highway, and approximately following the alignment of 40th Avenue.

2.10 Local Planning Studies/CIP/TIP/LRTP

Anchorage Transportation Improvement Program (TIP) (AMATS, December 2018): The project is not currently included in the AMATS TIP, which sets out transportation improvements for 2019 to 2022.

No other recent local planning studies have been completed by MOA in the project area.

2.11 Draft AMATS Non-Motorized Plan (AMATS, 2021)

The Draft AMATS Non-Motorized Plan, which was approved by the AMATS Policy Committee in July 2021 but has not yet been formally adopted, provides a vision for a non-motorized network within the AMATS metropolitan planning area. When implemented, it will merge bicycle, pedestrian, and shared use pathway planning efforts. The Plan has been through a public review process and is not yet adopted.

The Draft Non-Motorized Plan removes Seward Highway from the proposed bicycle network, and also removes the proposed bicycle lane through the Old Seward Highway and 36th Avenue intersection. It does provide for 36th Avenue to form part of a primary pedestrian corridor including crossing through the Seward Highway and 36th Avenue intersection. The Seward Highway: 36th Avenue Intersection project will support the realization of this corridor.

2.12 Additional Resources

Complete Streets Policy (AMATS, 2018)

MOA Vision Zero Action Plan (AMATS, 2018)

3.0 EXISTING CONDITIONS

The project area is centered on one of the busiest intersections in Anchorage, one of the few freeways in the state, and several smaller but heavily used roadways within Midtown. Existing conditions are variable within the area. The Seward Highway and 36th Avenue intersection is signalized and at grade. It is a high-traffic corridor with inadequate pedestrian and bicycle facilities, an elevated collision rate, high severity crash rate, history of fatal injury crashes, limited/absent north/south pedestrian corridors, and non-Americans with Disabilities Act (ADA) compliant pedestrian facilities.

3.1 Context (Land Use, Street Character)

The Seward Highway corridor is an important interface between the predominantly residential areas to the east and the commercial areas to the west. The Midtown City Center includes numerous medium to large office buildings and a variety of small to large retailers. The project area is located within three Community Council areas: Midtown, Rogers Park, and Tudor Area.

Most of the area surrounding and to the west of the Seward Highway within the project area is designated as Midtown City Center in MOA's Land Use Plan 2040. A small area of Commercial Corridor is located near the corner of 36th Avenue and LaTouche Street. The project area includes a significant swathe of single-family and two-family neighborhood designations on the east side of Seward Highway. The Helen Louise McDowell Sanctuary, which is east of the Seward Highway between 36th Avenue and Tudor Road, is designated as a park or natural area. A greenway-supported development growth supporting feature is shown generally along the alignment of Fish Creek through the Sanctuary, extending west across the Seward Highway, and approximately following the alignment of 40th Avenue.

The land use designations in the Land Use Plan largely align with existing land uses in the project area. The Midtown City Center is a regional employment center that has been growing over the past 20 years. The area includes extensive medium to large office buildings and a variety of large and small retailers.

The City Center designation encourages continued medium-to-high density development, walkable streets with wider sidewalks and pedestrian amenities, mixed use residential development, and strong connectivity to residential neighborhoods. The existing land uses, forecast growth, and City Center designation reinforce the need for improved connectivity across the Seward Highway corridor for non-motorized and motorized users and emphasizes the need for convenient, safe, and high-capacity access to the arterial and freeway network.

Residential land uses, primarily to the east of the Seward Highway, include large areas of single and two-family housing, and some of the oldest neighborhoods in Anchorage are within the study area. These neighborhoods include Geneva Woods, College Village, and Rogers Park. Other residential areas are more dispersed throughout Midtown, and include a mix of single and two-family housing, as well as higher density housing types. The American Community Survey estimates for 2014 to 2018 within a one-mile radius of the intersection of 36th Avenue and the Seward Highway contains approximately 3,674 households with 9,597 residents. Children under 18 make up 18 percent of household residents within the area, 15 percent are senior citizens,

and 48 percent of residents are people of color. This population area reports as diverse, with approximately 24 percent primarily speaking a language other than English at home.¹

The project is not expected to have any long-term negative impact on projected land use in the vicinity. The project will support the existing commercial land use, encourage residential mixed-use redevelopment consistent with the land use zoning, and improve access and safety for all transportation modes. It will also help to improve east-west connectivity for all transportation modes by creating more direct and safer crossings across the Seward Highway, and easing congestion on 36th Avenue.

3.2 36th Avenue

Thirty-Sixth Avenue, a MOA managed facility, is classified a minor arterial roadway in the OS&HP. Within the project area, 36th Avenue has two travel lanes in each direction until it approaches the Seward Highway, where it expands to include two left-turn lanes, two through lanes, and a right-turn lane in both directions.

3.2.1 ROW Availability

ROW width ranges from 70 feet to 80 feet. The minimum ROW for a minor arterial roadway as described in the OS&HP is 80 feet. Figures 2 and 3 show the existing typical sections of 36th Avenue near the LaTouche Street intersection and near the Seward Highway Intersection. Figure 4 shows the existing condition of the roadway at the 36th Avenue and LaTouche Street intersection.

3.2.2 Traffic Conditions

The roadway has a posted speed limit of 40 miles per hour (mph) west of the Seward Highway, and reduces to a posted speed limit of 35 mph, east of the highway. The most recently available AADT information provided by DOT&PF shows 36th Avenue's 2019 AADT ranging from 11,426 (near LaTouche Street) to 16,972 (near Old Seward Highway) vehicles per day². It is worth noting that volume estimates for 2020 are approximately 15 to 20 percent lower owing to the COVID-19 pandemic and the disruptions to normal routines.

¹ EPA. 2021. One-mile radius around the intersection of 36th Avenue and New Seward Highway ACS Estimates for 2014-2018. Accessed November 9, 2021. <https://ejscreen.epa.gov/mapper/>

² <https://alaskatrafficdata.drakewell.com/publicmultinodemap.asp> accessed 11/14/21.

3.2.3 Pedestrian Conditions

The existing pedestrian facilities (sidewalks) along both sides of 36th Avenue are relatively narrow, with an average width of 4 feet, and a maximum width of 8 feet. These sidewalks abut the adjacent roadway, which does not include space for roadway shoulders and prevents refuge from potentially errant vehicles. Additionally, the narrow width makes multimodal or cross directional passing along the sidewalks difficult. There are no separate bicycle facilities along the corridor. Current ADA standard requirements are not fully met throughout the project area. Sidewalks contain steep curb ramps that do not provide a smooth transition from/to the finished grade of the roadway. In addition, no detectable warning strips are provided along directional curb ramps while uplifted joints and worn concrete pose additional tripping hazards and safety concerns.

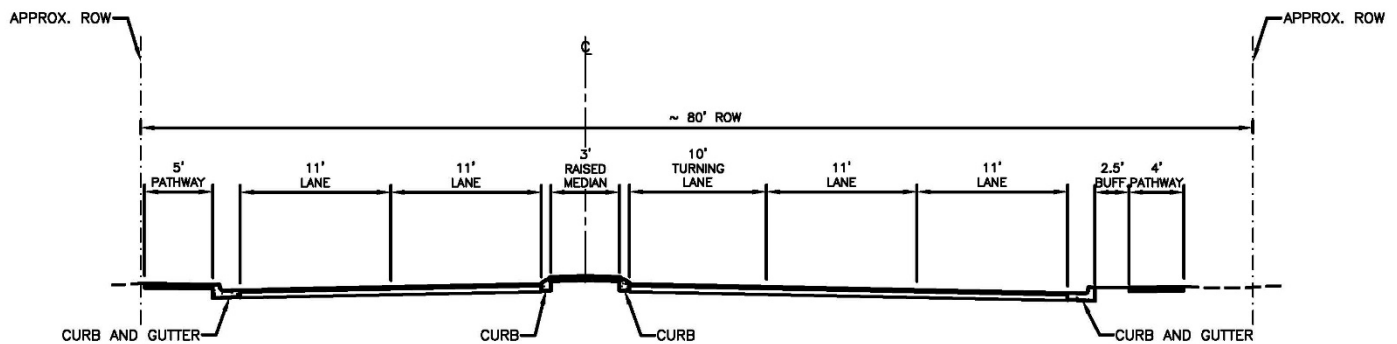


Figure 2: 36th Avenue just west of the intersection at LaTouche Street, looking east

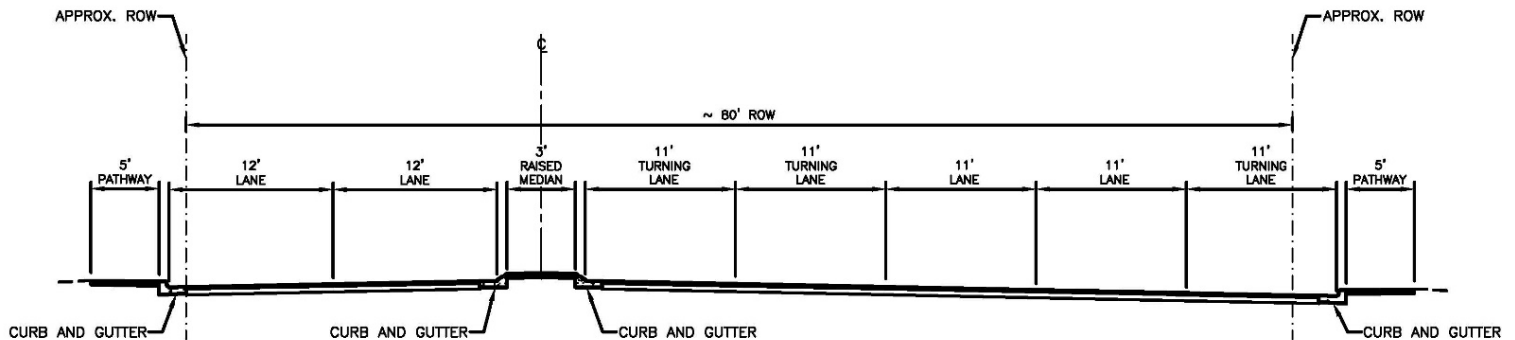


Figure 3: 36th Avenue looking east from the west side of the Seward Highway intersection



Figure 4: 36th Avenue looking west at the intersection with LaTouche Street

3.2.4 Existing Landscape

Existing landscape features are limited to hardscape roadways, sidewalks, and associated facilities.

3.3 Seward Highway

The Seward Highway is classified by DOT&PF as an interstate highway and strategic freight route while the MOA has classified the Seward Highway as a freeway and identified it as a regional truck route. The Seward Highway between Tudor Road and 36th Avenue has three travel lanes in each direction, with two left-turn lanes and one right-turn lane in each direction at the 36th Avenue intersection. North of 36th Avenue, the Seward Highway narrows significantly and in both north-bound and south-bound directions has three travel lanes; north-bound, a right-turn lane into Energy Court becomes a right-turn lane onto Benson Boulevard.

3.3.1 ROW Availability

ROW width ranges from 300 to 400 feet. Figure 5 shows the existing typical section of the Seward Highway south of the 36th Avenue Intersection. Figure 6 shows the existing typical section of the Seward Highway north of the 36th Avenue intersection.

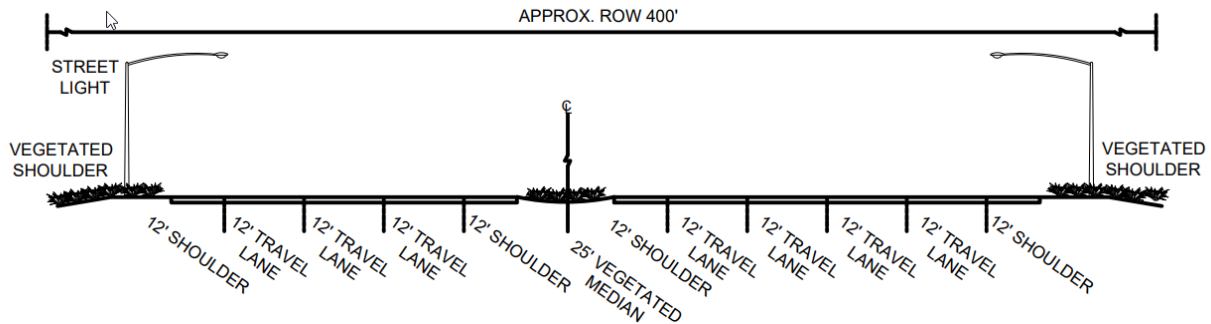


Figure 5: Seward Highway between Tudor Road and 36th Avenue

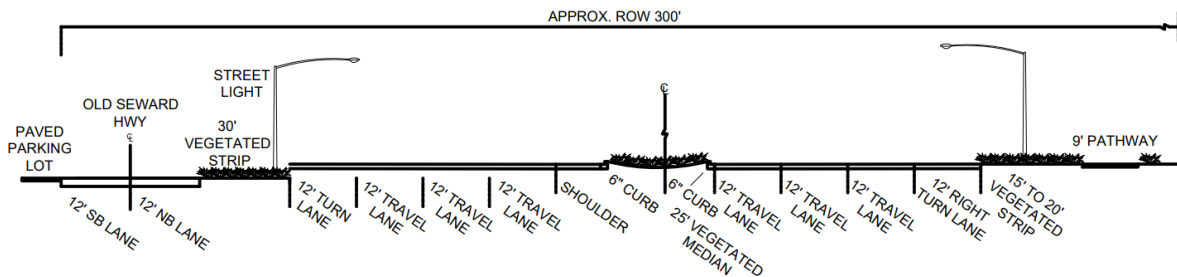


Figure 6: Seward Highway between 36th Avenue and Benson Boulevard

3.3.2 Traffic Conditions

The Highway has a posted speed limit of 55 mph between Tudor and 36th Avenue, which reduces to a posted speed limit of 45 mph just south of 36th Avenue to 15th Avenue.

The 2019 AADT for combined north and south-bound travel within the project area are 43,262 vehicles per day. The AADT for 2020 was lower at 37,000, but actual traffic data at the Permanent Traffic Recorder located this site is recording an AADT above the 2019 volumes. Refer to Figure 7 for Mean Average Daily Traffic (MADT) by month, showing the year-on-year trends.

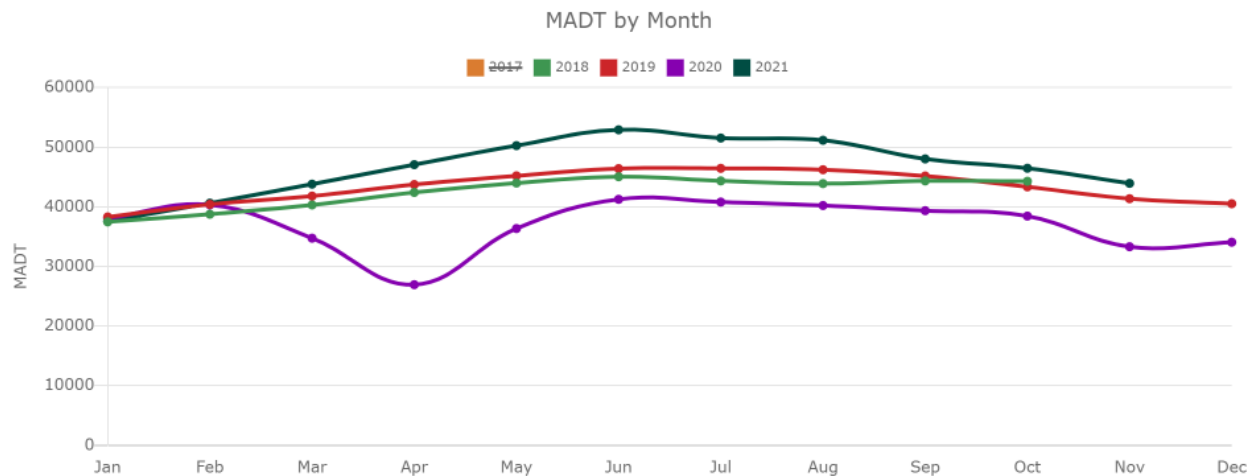


Figure 7: MADT by Month Recorded at the Seward Highway MP 125 Combo NB/SB Traffic Recorder at Mooses Tooth³

3.3.3 Pedestrian Conditions

Pedestrian and bicycle facilities are in relatively poor condition or absent and improvements are needed to either add these facilities or increase their safety and comfort.

3.3.4 Existing Landscape

Existing landscape features are limited to hardscape roadways, sidewalks, and associated facilities.

3.4 Existing Drainage

Drainage within the project area flows into existing storm drain systems within the Fish Creek and Chester Creek watersheds. These are managed by DOT&PF, MOA, and private entities. DOT&PF manages approximately 6,800 feet of trunk line collecting stormwater runoff north of Benson Boulevard along the Seward Highway corridor that discharges into Chester Creek via an outfall west of Seward Highway. An additional 2,000 feet of trunk line collects stormwater runoff south of 40th Avenue, which discharges into Fish Creek. There is no stormwater infrastructure along the Seward Highway between Benson Boulevard and 34th Avenue. Runoff throughout the corridor is directed toward grassed medians, vegetated swales, and open vegetated areas east of the highway for infiltration. The study area also includes numerous local collector storm drains.

Drainage infrastructure exists at the major intersections along the Seward Highway and the existing drainage patterns could be maintained such that all runoff would be pre-treated in the grassed median or adjacent vegetated areas before either infiltrating or discharging to the storm

³

https://alaskatraficdata.drakewell.com/adcharts.asp?node=AKDOT_CCS&cosit=000011393000&minyear=2011&maxyear=2021 Retrieved 11/14/21.

drain system. Urban runoff is a source of contamination and the project will need to consider Storm Water Pollution Prevention Plan procedures and guidelines for proper stormwater treatment.

3.5 Existing Utilities

Underground and overhead utilities including illumination, electric, telecommunication, cable, traffic, gas, storm drain, water, and sewer are present within the project area. The following utility companies have facilities in the project limits:

- Alaska Communication Systems (ACS)
- Anchorage Water and Wastewater Utility (AWWU)
- Chugach Electric Association (CEA)
- ENSTAR Natural Gas Company (ENSTAR)
- General Communications Incorporated (GCI)

Utilities include CEA overhead utilities on poles located near the ROW and an ENSTAR natural gas line buried on the west side of the Seward Highway between 36th Avenue and Benson Boulevard. Utilities may require relocation including undergrounding overhead facilities. Agreements will need to be developed, at select locations throughout the project to address conflicts. Many utilities are expected to have some degree of conflict with proposed construction activity.

Illumination

Street and pathway poles are found throughout the project area. Street lighting elements will be upgraded as part of this project to current standards found in the MOA Design Criteria Manual (DCM) Chapter 5 and DOT&PF Highway Preconstruction Manual (HPCM) Chapter 11.

Electric

CEA maintains overhead and underground electric lines throughout the project area. Overhead CEA lines are located on poles within the project area, near the Old Seward Highway and 36th Avenue intersection. These poles were originally owned by Municipal Light and Power (ML&P) but the company was fully acquired by CEA in 2020. Underground electric lines are also located by the University Center and follow the Seward Highway in the existing ROW. 36th Avenue has at least three underground electric lines as well. Major CEA utilities within the project area include:

- Numerous 12kv distribution crossings, both overhead and underground
- Tudor to Northern Lights Boulevard: 35kv transmission line
- 36th Avenue to Northern Lights Boulevard: 115kv transmission line

Telecommunications

ACS and GCI have underground and overhead telecommunications utilities in the project area. ACS has a multicable and fiber optic duct system along 36th Avenue. Buried commercial lines run along the west side of Seward Highway, between The University Center and Benson Boulevard. Additional residential and commercial buried lines are located east of Seward

Highway and south of 36th Avenue. GCI's major fiber optic lines supply service to areas such as Elmendorf Air Force Base and the Matanuska-Susitna Valley. The fiber optic cable crosses the Seward Highway at 36th Avenue, and it runs along the east side of the Seward Highway between 36th Avenue and Northern Lights Boulevard.

Natural Gas

ENSTAR's facilities in the project area include a 6-inch steel pipe that runs along the west side of the Seward Highway between 34th Avenue and Northern Lights Boulevard. Various service lines down to 3/4-inch lines serve surrounding properties.

Water

AWWU water mains run along and across the Seward Highway within the project area. A 12-inch cast iron pipe runs along the west side of the Seward Highway between 36th Avenue and Northern Lights Boulevard. Another 12-inch cast iron pipe crosses the Seward Highway at the 36th Avenue intersection. The local streets west of the Seward Highway have various sized ductile iron and cast-iron pipes that connect to businesses or to the main line along Old Seward Highway.

Sewer

AWWU also provides sanitary sewer services. A 12-inch asbestos concrete pipe crosses Seward Highway at 36th Avenue, and AWWU's Lift Station No. 5 is located on the east side of 36th Avenue and Seward Highway intersection.

4.0 DESIGN STANDARDS

Project design criteria are based on the roadway characteristics, functional classification, road ownership, and the ability of the design to meet the project's purpose and need in a manner that minimizes negative impacts and garners stakeholder support.

36th Avenue is classified as a Class II Urban Minor Arterial by the MOA OS&HP, and it is owned by the MOA. The Seward Highway is classified by DOT&PF as an interstate highway and strategic freight route.

The proposed project will not change the current functional classifications. The proposed one-way frontage roads will be designed to meet the Class III Urban Major Arterial classification as detailed in the OS&HP, and a Principal Arterial roadway by DOT&PF. This is based on anticipated traffic volumes and emphasis on providing access to the Midtown central business district between Tudor Road and Benson Boulevard. Improvements to smaller streets within the project area would also not change the existing classifications.

The Seward Highway: 36th Avenue Interchange project will be completed according to standards established by the MOA DCM and the State of Alaska HPCM. Both manuals interpret, amend, and supplement American Association of State Highway Transportation Officials (AASHTO) Standards, which are found in A Policy on Geometric Design of Highways and Streets (Green Book). The DCM has jurisdiction over 36th Avenue and other MOA-owned local roadways. The HPCM design procedures will be used to guide work in areas of DOT&PF ROW along the Seward Highway. In the event of conflict between the DCM, HPCM, and the Green Book, DCM provisions will prevail in MOA ROW and HPCM will prevail within DOT&PF ROW.

Existing conditions are not anticipated to impact the ability to meet these standards.

4.1 Project Design Criteria

The objective of establishing project design standards and criteria is to promote safe, functional, and durable transportation infrastructure. An overview of select criteria is listed below for the roadways and is summarized in Table 2.

Table 2: Design Criteria and Non-Motorized Facilities by Roadway

Roadway	Key Design Criteria	Non-Motorized Facilities
Seward Highway ("Mainline")	<ul style="list-style-type: none"> • Posted Speed Limit: 50 mph between Benson Boulevard to 36th Avenue slowing to 45 mph just south of Benson Boulevard • Lane Width: 12 feet • Inside Shoulder Width: 10 feet • Outside Shoulder Width: 10 feet 	<ul style="list-style-type: none"> • None
Frontage Roads	<ul style="list-style-type: none"> • Posted Speed Limit: 35 mph • Lane Width: 12 feet • Inside Shoulder Width: 4 feet • Outside Shoulder Width: 8 feet 	<ul style="list-style-type: none"> • Multi-use pathways: 10-ft with 8-ft buffer • Bicycle lanes: 6-ft wide, separated by 2-ft buffer
36th Avenue	<ul style="list-style-type: none"> • Posted Speed Limit: 40 mph west of the Seward Highway, and 35 mph, east of the Seward Highway (match existing conditions) • Lane Width: 11-12 feet • Inside Shoulder Width: 4 feet • Outside Shoulder Width: N/A (Curb and Gutter will be provided along the outside travel lane) 	<ul style="list-style-type: none"> • Multi-use pathways: will tie into existing facilities, width will range from 5- to 8-ft • Pathways separated by raised curb • Buffer provided where space is available in existing ROW

4.1.1 Seward Highway

The highway will primarily serve to facilitate mobility, providing fully controlled access between Tudor Road and Benson Boulevard. This project will grade separate the Seward Highway and 36th Avenue. This will allow vehicles to travel north and south past 36th Avenue without having to pass through an intersection, increasing safety and efficiency for all travelers within the project area.

4.1.2 Frontage Roads

One-way frontage roads are proposed on both the east and west sides of the Seward Highway to provide access to adjacent properties. They will be located within the existing DOT&PF ROW, with the potential exception of small strip acquisitions from adjacent properties. The frontage roads are proposed to remain as close to existing grade as possible.

4.1.3 36th Avenue

Thirty-Sixth Avenue will continue to serve as the primary access to and from the Midtown city center west of Seward Highway and the east side of the Seward Highway which is the residential areas and the U-Med District.

4.1.4 Non-motorized Facilities

Multi-use pathways will be provided along the frontage roads and on the north and south side of 36th Avenue. Pathways will be separated from the roadway by a raised curb, in addition to a buffer where additional ROW is available. Pathways along the frontage roads will be 10-feet wide, with an 8-foot buffer. Pathways along 36th Avenue will be improved but will tie into the existing pathways and sidewalks located at the eastern and western extents of the project and will range from 5 to 8 feet in width. Bicycle lanes (6 feet wide) are proposed along both the northbound and southbound frontage roads. These will be separated from the adjacent travel lane by a 2-foot-wide buffer.

The construction of pedestrian facilities along 36th Avenue and Seward Highway will be completed according to the standards established by the DCM Chapter 4 and HPCM chapter 12. This chapter of the DCM references the Areawide Trails Plan, and AASHTO's Guide for the Development of Bicycle Facilities. The AASHTO Green Book and AASHTO Guide for the Planning, Design, and Operation of Pedestrian Facilities may be used as additional guidance. Design considerations will also include ADA requirements, as outlined below.

Pathways and curb ramps will comply with the current ADA Standard, which is the 2010 ADA Standards for Accessible Design (2010 Standards). This is the current standard adopted by DOT&PF. Additionally, the 2011 Proposed Guidelines for Pedestrian Facilities in the Public ROW is considered best practice guidance by DOT&PF and will be used for areas not explicitly addressed by the 2010 Standards.

The DCM further states the Americans with Disabilities Act Accessibility Guidelines (ADAAG) must be adhered to. The ADAAG Manual, A Guide to the Americans with Disabilities Act Accessibility Guidelines for Buildings and Facilities (published July 2004, Amended August 2005) applies to many situations governing accessible routes, including curb ramps and signage. Pathway design will adhere to these manuals, so pathways provide a safe and accessible route for all travelers.

4.1.5 Exception to Standards

There are no exceptions to design standards anticipated at this time.

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5.0 DESIGN ALTERNATIVES

The project was recommended as part of the MCR PEL Study, as outlined in Section 2.0 of this report. The project team has taken a two-tiered approach to assessing and refining design alternatives forwarded from the concepts recommended in the PEL Study. Initially, the team focused on the horizontal layout and termini of the interchange, assessing variations of a median u-turn concept, which was determined to be fatally flawed, and a loop ramp intersection concept. After that, the vertical profile of the interchange was considered, exploring loop ramp interchange configurations that take the Seward Highway over an at grade 36th Avenue, under an at grade 36th Avenue, and a hybrid approach in which the Highway is slightly elevated while 36th Avenue is slightly depressed. This two-step screening process resulted in three design alternatives that were then evaluated for reasonable constructability, yielding one recommended alternative.

5.1 No Build Option (Dismissed)

A No Build Alternative was evaluated as part of the MCR PEL study but was not recommended. A no build option does not meet the purpose and need of the project. The current intersection layout, using signals with traffic interacting on the same surface (at grade), has reached its maximum capacity. A grade-separated interchange, where one lane of traffic crosses over another on a bridge, is needed to:

- Improve facilities for pedestrians and bicyclists
- Increase safety
- Support the separation of local trips from through trips
- Reduce delay

An interchange will increase roadway safety by reducing the potential for vehicle and pedestrian crashes. Roadways with interchanges generally allow traffic to move freely with fewer interruptions, which reduces the risk of collisions. An interchange at this location would mean that Seward Highway traffic will not have to stop at 36th Avenue. A grade-separated interchange is needed to allow the Seward Highway to flow freely while providing north, south, east, and west options for 36th Avenue traffic. This will improve local traffic movements and enable the Seward Highway to operate more effectively as a strategic freight route and part of the NHS.

5.2 Median U-Turn (Dismissed)

The median u-turn interchange provided a grade separated highway with northbound and southbound frontage roads that parallel the highway. Ramps provided access to and from 36th Avenue, and the frontage roads provided access to neighboring land uses along the corridor. The mainline provided mobility for vehicles traveling north or south past 36th Avenue. This Concept reduced travel delays by eliminating left turning movements at the proposed 36th Avenue and Seward Highway intersections. Vehicles intending to turn east or west onto 36th Avenue from Seward Highway, would travel along the frontage roads, past the intersection, and enter one of the two median u-turns located north and south of the intersection.

Though beneficial in terms of increased safety and efficiency, this concept was determined to be fatally flawed due to the size of the structure needed to fully grade separate the highway above

or below both median u-turns and 36th Avenue (which would remain at existing grade). There is not enough space to meet vertical clearance requirements, then tie back into existing grades at Tudor Road and Benson Boulevard while meeting vertical curve length requirements and not exceeding a maximum grade of four percent along the mainline. Additionally, the northbound entrance ramp from 36th Avenue would be unable to join with the highway until a few hundred feet before the Benson intersection which does not allow for adequate and safe weaving maneuvers before Benson Boulevard.

5.3 Loop Ramp Interchange (Preferred)

The Loop Ramp Interchange would also provide a grade separated highway with northbound and southbound frontage roads that provide access to the adjacent properties. Ramps provide access to and from 36th Avenue. A loop ramp located at the Old Seward Highway and 36th Avenue intersection will provide southbound access for vehicles traveling along 36th Avenue onto the Seward Highway.

Additionally, the northbound intersection at 36th Avenue and the northbound frontage road would experience reduced wait times as well due to limited turning movements. Reduced intersection widths and less opportunities for opposing vehicles to collide would increase the safety for all modes of travel. The increased distance between the Old Seward Highway and the northbound frontage road intersection along 36th Avenue also provides additional space for queuing vehicles.

A major advantage of the loop ramp interchange is the shorter structure length needed to grade separate the Seward Highway (mainline) from 36th Avenue. This separation provides more space for northbound vehicles to merge and weave onto the highway before Benson Boulevard, decreasing collision risk. This alternative has a slightly narrower footprint than the Median U-Turn Concept, which reduces potential ROW acquisitions required within the project area.

5.4 Alternatives Analysis

All three vertical profile alternatives considered share the same impacts to traffic, parking and property access, and pedestrian facilities. Key differences between the three alternatives are with impacts to drainage, resiliency, and anticipated maintenance and operations challenges. Minor differences between the three alternatives are with ROW, cost, and utilities impacts.

5.4.1 Traffic Impacts

Separating through traffic from vehicles entering and exiting Midtown city center will improve safety along the corridor while also reducing wait times at intersections. The separated highway structure will increase mobility and efficiency for vehicles heading north-south past 36th Avenue. The intersection at 36th and the northbound frontage road will have a higher level of service. There will be fewer conflict points at the intersection improved safety. The increased spacing between the intersections along 36th Avenue provides more space for queuing vehicles and improved signal operations.

Due to decreased speeds along the frontage roads, vehicles slowing to access land uses directly from the Seward Highway will be at reduced risk of rear-end collisions or causing delay to higher speed traffic passing through the project area.

A noise study will be performed to determine whether mitigation of noise impacts is warranted in accordance with the State of Alaska noise policy.

5.4.2 Parking & Property Access Impacts

Some strip acquisitions of adjoining properties will potentially be required for the project, and these may impact a limited number of parking spaces on the adjoining properties. The design process will seek to minimize ROW acquisition as much as possible. Impacts to private land uses will be explored in further detail as the design process proceeds. Property access (e.g., driveways) will be restored or reconstructed as part of the design process.

5.4.3 How the Project Will Tie into Other Projects

The project will be designed to tie into the existing street network and compatible with future projects recommended along the Seward Highway, as well as the existing alignment of 36th Avenue. In particular, the project will have clear and logical termini that will not impact flexibility on other projects, except for the ramps linking the Tudor Road interchange with the Seward Highway and associated frontage road network. The project team is currently working through the logical termini between this project and a future project to replace the Tudor Road interchange to make sure service can be maintained during and following construction.

5.4.4 Pedestrian Impacts

Existing pathways along 36th Avenue and local streets would be improved to meet current ADA standards and close gaps where facilities are currently absent. New pathways will be provided along both frontage roads. Proposed pathways are 10 feet wide, which makes passing and multi-modal use safer and more comfortable for non-motorized users. Additionally, a raised curb and 8-foot buffer will be provided, where feasible, between the roadway and the path for facilities that parallel the frontage road. This increases safety and comfort for pedestrians. The buffer also provides additional room for snow storage in the winter to help mitigate snow being stored on the pathways which makes them difficult for non-motorized users to navigate.

Bike lanes will be provided north-south between Tudor Road and Benson Boulevard, along the frontage roads, which will increase safety for bicyclists who currently ride on the side of the travel lane or shoulder along this corridor.

A separated pathway with a wide buffer is proposed between 36th Avenue and Benson Boulevard through private property. The proposed pathway placement provides space for additional MCR projects in the future. Right-of-way impacts from the pathway are anticipated to be similar in all three of the alternatives.

5.5 Alternatives

5.5.1 Loop Ramp Alternative 1: Elevated Seward Highway Over 36th Avenue at Grade (Preferred)

A grade separated highway will allow vehicles traveling north and south to bypass 36th Avenue without interacting with travelers heading east-west along 36th Avenue (see Figure 8). Of the three alternatives under consideration, an elevated highway over an at-grade 36th Avenue offers the lowest potential impact and least reconstruction of 36th Avenue.

The frontage roads would remain at existing grade and the Seward Highway would be elevated over 36th Avenue via a bridge or set of bridge structures. The vertical clearance requirement across 36th Avenue (i.e., the space between the surface of the 36th Avenue roadway and the bottom of the elevated bridge girder going across 36th Avenue) is 18 feet. The bridge girder adds an additional height between the bottom of the girder and bridge deck surface which will vary based on the bridge design and bridge type selection.

The visual and noise impacts of this concept are primary concerns voiced by the public and agencies who are adjacent to and/or regularly use the corridor. A noise study will be performed to determine potential noise impacts and mitigation will be provided, if warranted. Additionally, the existing ground between Tudor Road and 36th Avenue currently has a natural rise to it that is mostly indiscernible to both drivers and onlookers. The elevation of the raised bridge across 36th Avenue is anticipated to be about 10 feet higher than this existing high point (See Figure 9).

This alternative has several advantages over the alternatives that depress Seward Highway beneath 36th Avenue. First, as a major trucking route, a key consideration in reconstructing the Seward Highway is vertical obstructions. Placing the Seward Highway over 36th Avenue reduces the likelihood of a bridge strike at this intersection. Although traffic moving along 36th Avenue would pass under a bridge, the road is not a major trucking route and is less likely to accommodate truck traffic. Secondly, elevating Seward Highway over 36th Avenue will avoid the need to dewater the area in the vicinity of the intersection, which will eliminate the need for a pump station and the associated maintenance costs. This will also eliminate the need for back-up generators in the event of an emergency or electricity failure to keep the area dewatered. Finally, elevating the Seward Highway over 36th Avenue will be easier to construct as the design will avoid excavating below the water table and having to manage impacts associated with groundwater intrusion.



Figure 8: 36th Avenue at grade under an elevated Seward Highway

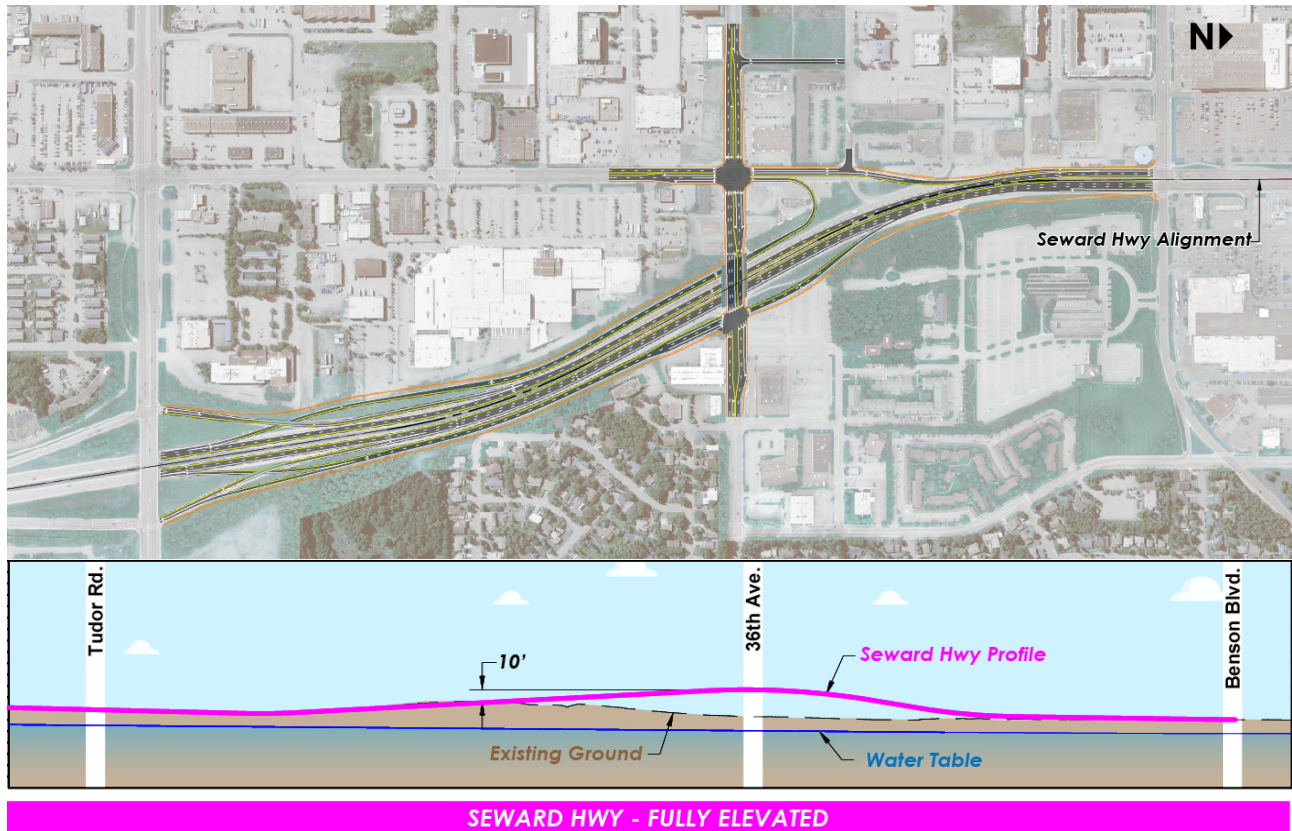


Figure 9: Profile of the Seward Highway elevated over 36th Avenue at grade

5.5.1.1 Drainage Impacts

Stormwater flowing from the frontage roads and mainline will be directed into the existing storm drain system through ditches and new pipe networks along these roadways. Because 36th Avenue will remain at existing grade and the mainline will be constructed as an elevated structure, the existing storm drain system below the roadway will be used for anticipated runoff. Impacts to the Fish Creek culvert are not anticipated with an elevated mainline profile.

5.5.1.2 Utilities Impacts

Because the footprint of the roadway network is being widened, utilities currently running parallel to the road will be relocated to preserve access for maintenance. Utility relocation will be required for both overhead and underground utilities. Electric, gas, water, and sewer lines will all be impacted within the project. Underground utilities that parallel 36th may be avoided.

5.5.1.3 ROW Impacts

The design process is seeking to minimize ROW impacts associated with the interchange project. However, since additional roadways are being constructed and the overall road width is being increased, some strip acquisitions of ROW are likely. Early communication with the public has occurred to explain potential ROW impacts as part of the MCR PEL Study, and these

communications will continue through the design and environmental process. Additional ROW impacts may be identified as designs progress.

5.5.2 Loop Ramp Alternative 2: Depressed Seward Highway Under 36th Avenue at Grade (Dismissed)

The second alternative for the loop ramp interchange is to provide a fully depressed mainline that travels under 36th Avenue, which will remain at existing grade (See Figures 10 and 12). Motorists and pedestrians would access land uses adjoining the Seward Highway via the frontage road network that parallels the mainline. The highway would be fully depressed beneath 36th Avenue and then rise back up to match the existing grade at both Tudor Avenue and Benson Boulevard.

The benefits of a fully depressed mainline are reduced visual impacts and potentially reduced noise impacts. However, depressing the mainline at 36th Avenue would create a low spot along the roadway that is well below the level of the water table (See Figure 11). To offset this and prevent water from re-entering the depressed mainline structure, a permanent lift station or pump would be needed to continuously dewater the area and lower the water levels.

Another option would be to create an impermeable trench section with a thick concrete slab to offset the buoyancy forces below the depressed roadway. Additional pumps or drains would still be needed along the sides of the depressed structure to maintain the structural integrity of both the roadway and structure itself. There are significant risks that are presented in the design, construction, and future maintenance and operations of a structure of this type.

Stakeholder feedback gathered through public involvement activities to date indicates a preference for depressing the Seward Highway under 36th Avenue. This option, however, is considered to be fatally flawed due to the high water table near the intersection as well as safety risks such as environmental conditions (e.g., freeze/thaw cycles), seismic considerations, risks associated with mechanical pumping failure, and potential for truck-bridge collision, all of which create a risk of temporary road closure and reduce network resiliency.

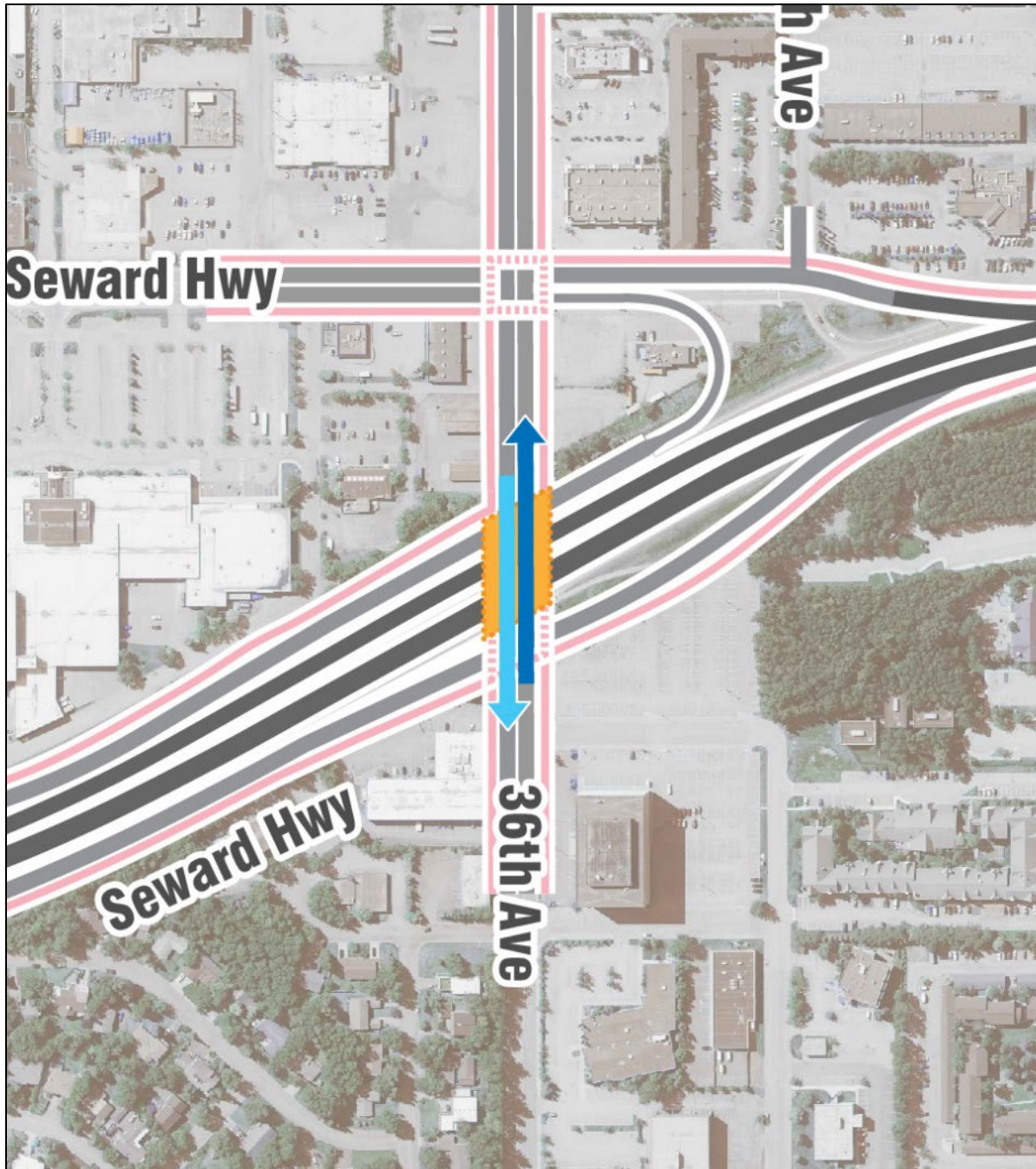


Figure 10: 36th Avenue at grade over a fully depressed Seward Highway

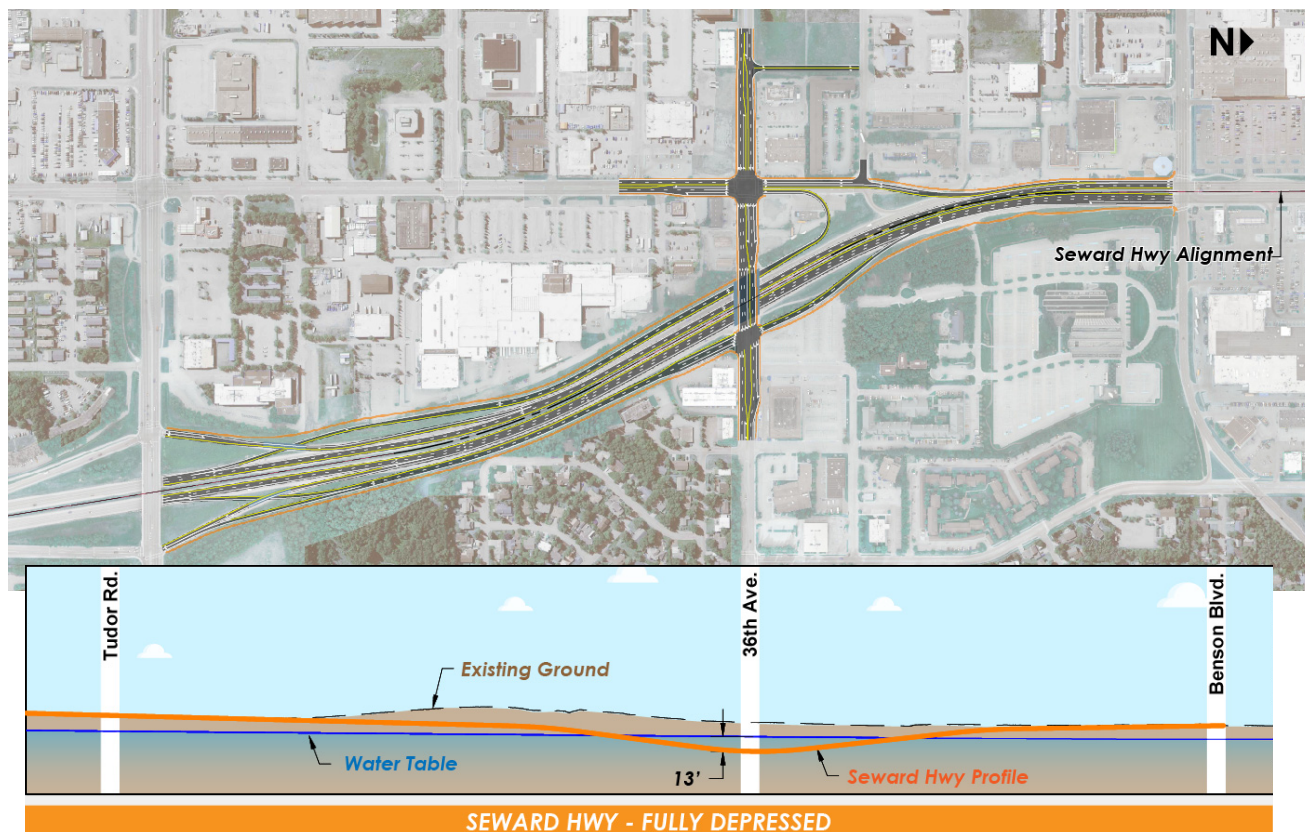


Figure 11: Profile of the Seward Highway fully depressed under 36th Avenue at grade

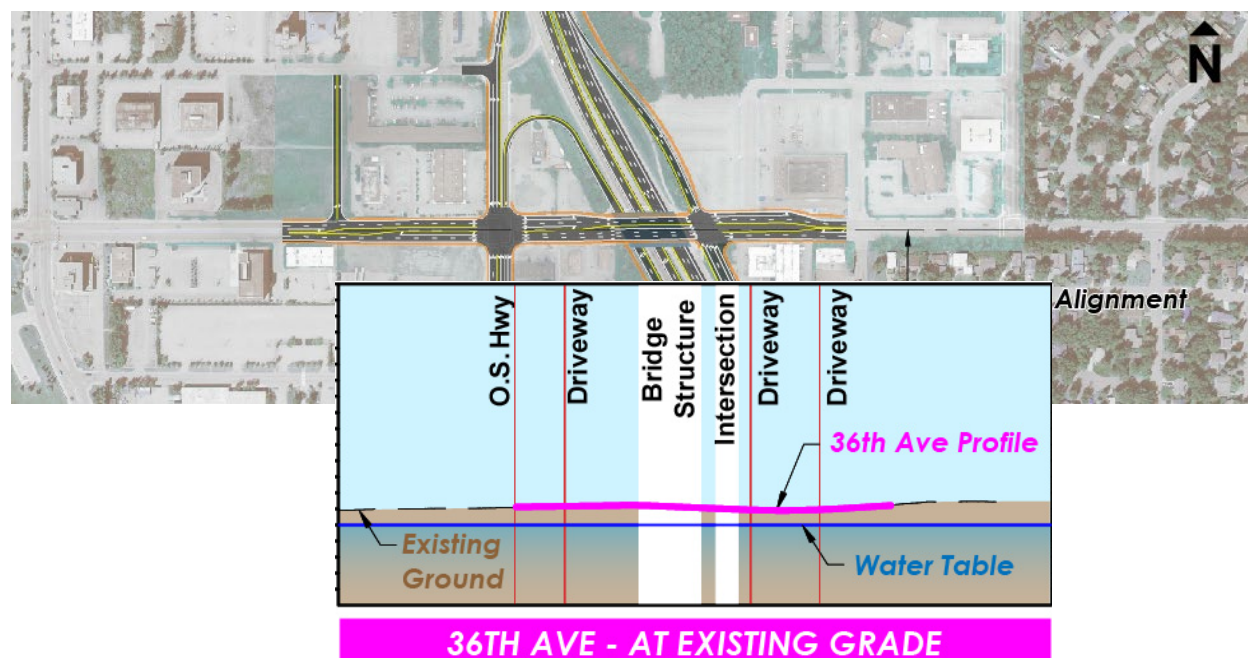


Figure 12: Profile of 36th Avenue at grade over a fully depressed Seward Highway

5.5.2.1 Drainage Impacts

Depressing the mainline at 36th Avenue would create a low spot that dips well below the water table elevation. To offset this and prevent water from re-entering the depressed mainline structure, a permanent lift station or pump would be needed to continuously dewater the area and lower the water levels. Another option would be to create an impermeable trench structure with a thick concrete slab to offset buoyancy forces below the depressed roadway. Additional pumps or drains would still be needed to maintain the structural integrity of the roadway and structure itself. Risks of these options include power outages, seismic concerns, storm events that exceed the capacity of the pump, and associated safety concerns if the pumping system was to fail and the area backfill with water or ice and snow. Therefore, back-up pumps and power sources would be needed which increases future and current maintenance and operations concerns.

The output of the water pumped from around the depressed structure would also have to be determined. Since there is no outfall area or treatment location within the extents of the project, stormwater runoff and pumped water would have to be directed into the existing storm drain network. The surface runoff could be accommodated but the added volume of water from continuous dewatering would exceed the current capacity of this network, resulting in major impacts to the storm drain system, potentially well outside the project area. Additionally, there is a contaminated property located directly adjacent to the project area, which may require additional containment and treatment of run-off prior to entering the storm drain system to avoid further contamination.

5.5.2.2 Utilities Impacts

Utility impacts will be similar to the fully elevated mainline. Existing underground and overhead utilities will need to be relocated due to the widened footprint of the roadway. However, a fully depressed mainline option will require relocation of underground utilities that cross the Seward highway at 36th Avenue.

5.5.2.3 ROW Impacts

ROW impacts would be similar to impacts of the fully elevated mainline alternative. The design process is seeking to minimize ROW impacts associated with the interchange project. However, since additional roadways are being constructed and the overall road width is being increased, some strip acquisitions of ROW are likely. Early communication with the public has occurred to explain potential ROW impacts as part of the MCR PEL Study, and these communications will continue through the design and environmental process. Additional ROW impacts may be identified as designs progress.

5.5.3 Alternative 3: Partially Elevated Seward Highway and Partially Depressed 36th Avenue (Dismissed)

A hybrid option has been considered to lower the overall height of the Seward Highway while not impacting the water table. This would be achieved by lowering the elevation of 36th Avenue a few feet. To maintain structural integrity of the roadway, the top surface of the road needs to sit at least 4 feet above the water table (See Figure 13). The mainline would still be elevated above 36th Avenue, meeting vertical clearance requirements, but would sit a few feet lower than the elevation of the fully elevated mainline profile (See Figure 14).

Based on the water level data acquired, 36th Avenue would be able to drop a few feet below existing grade at the location where the mainline would cross over it. However, spring water levels are higher than the annual average, due to snow melt and the ground thawing. During this season, the elevation of the water would rise to sit just 4 feet below the existing surface of 36th Avenue. Therefore, the roadway could not be depressed without having to seasonally dewater the intersection. Additionally, ROW impacts, adjacent driveways, and utility impacts would all be addressed along 36th Avenue.

The height difference between a fully elevated and partially elevated mainline is likely to be minimal, and noise impacts would likely be the same or similar. Therefore, this alternative is not recommended but a slight lowering of profile may be accommodated if possible as the design progresses.

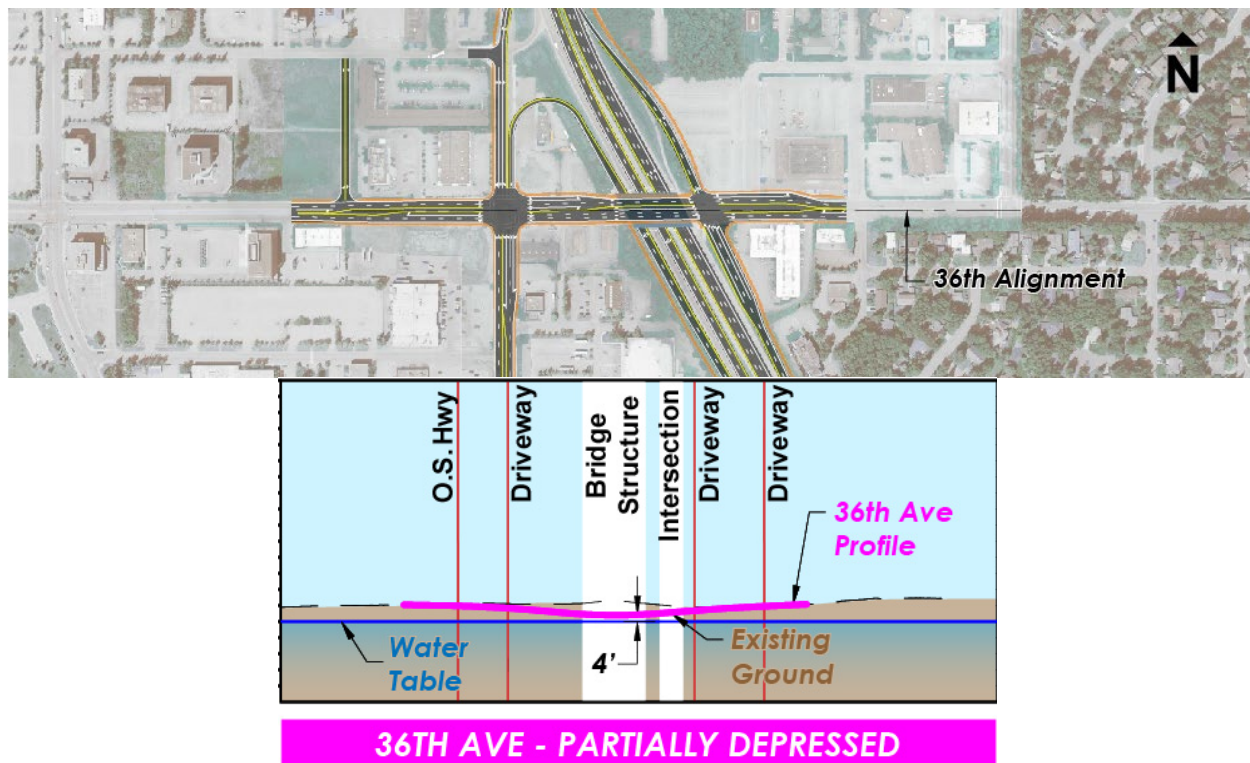


Figure 13: Profile of 36th Avenue partially depressed over a partially elevated Seward Highway

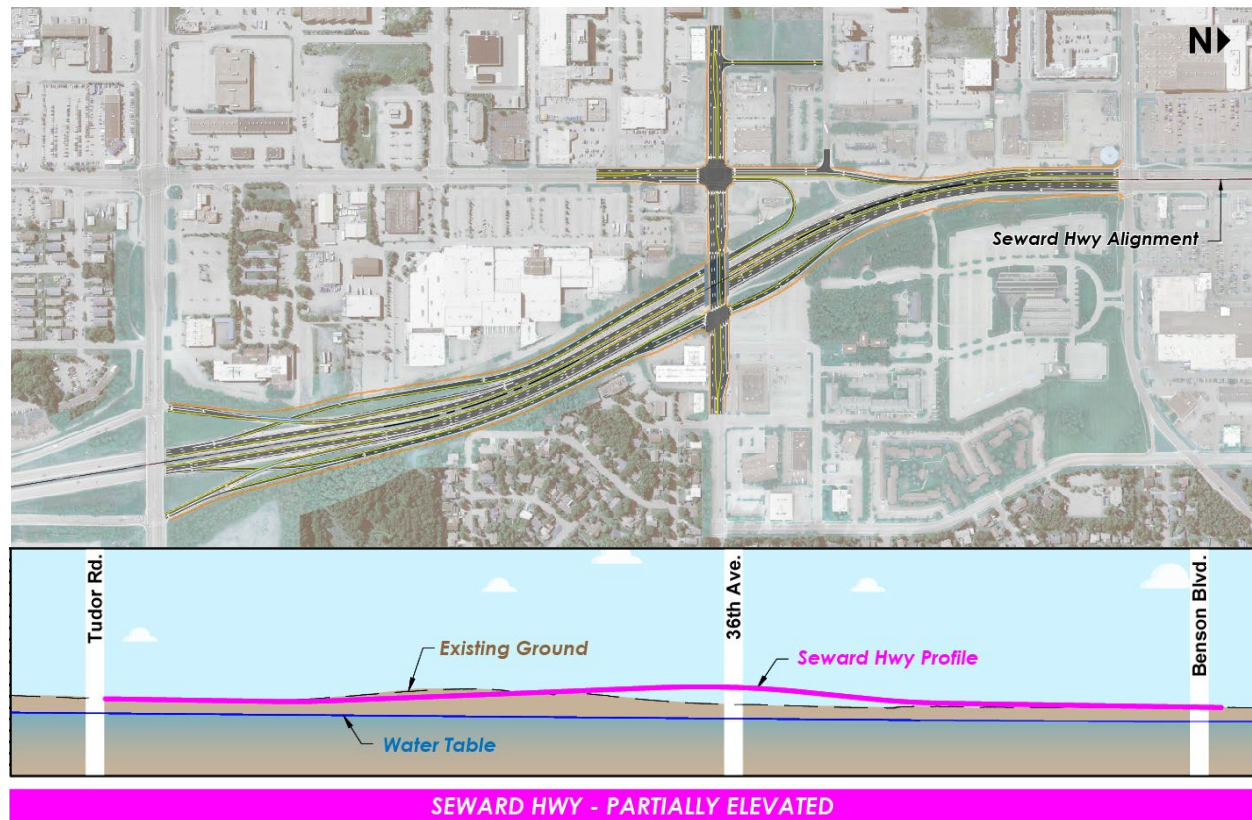


Figure 14: Profile of Seward Highway partially elevated over a partially depressed 36th Avenue

5.5.3.1 Drainage Impacts

Stormwater runoff would be directed to the existing storm drain network via ditching or pipes. A pump would be needed seasonally or during storm events, when the water table exceeds average levels, to prevent water from degrading the structural integrity of 36th Avenue.

Lowering the profile of 36th Avenue results in a low spot along 36th Avenue. This creates a basin where water or snowmelt could collect. Pooling water could erode the structural integrity of the road or pose a hazard for travelers. Therefore, a separate pump and additional piping would be needed to lift water out of this area and direct it into the existing storm drain network. Additional maintenance and operations concerns are introduced with this concept due to the low spot in 36th Avenue profile and the proximity of the roadway to the ground water elevation.

5.5.3.2 Utilities Impacts

Utility impacts would be similar to those of the fully elevated mainline. Existing underground and overhead utilities would have to be relocated due to the widened footprint of the roadway. Additional impacts to utilities running along 36th Avenue are anticipated due to the change in elevation along this roadway.

5.5.3.3 ROW Impacts

ROW impacts along the Seward Highway would be similar to the other profile alternatives. Impacts along 36th Avenue are potentially increased because the lowered elevation of the roadway would need to tie into the existing grade of adjoining properties. This could also adversely impact driveway access to adjoining properties along 36th Avenue. Additional ROW impacts may be identified as designs progress.

5.6 Preferred Alternative: Loop Ramp Alternative 1: Elevated Seward Highway Over 36th Avenue at Grade

The project team recommends a loop ramp interchange that is grade-separated with the Seward Highway elevated over 36th Avenue for several reasons:

- Resiliency: no pump station or backup generator is needed for emergency preparedness, therefore eliminating the risk of water intrusion into the roadway caused by water pump failure.
- Safety: reduced risk of Seward Highway closure or bridge damage/failure from the bridge being struck by a truck.
- Contamination containment: reduced risk of contamination from property located on south-west corner of project area migrating through groundwater associated with dewatering needed to accomplish the project
- Cost: lower construction cost and ongoing maintenance cost
- Constructability: roadway is above the water table.

Features of the proposed interchange include:

- Elevated highway over 36th Avenue that ties into existing grade at the Tudor Road interchange and Benson Boulevard intersection
- One-way frontage roads that parallel the highway and provide access to adjoining land uses
- Ramps to and from the highway to facilitate connection to and from Midtown city center, including a loop ramp

Improvements will include:

- Improved non-motorized facilities, including pathways and curb ramps that meet current ADA Standards and provide increased width to enhance buffering and improve non-motorized user comfort
- Improved east-west connectivity for all travel modes
- Improved safety by reducing conflict points between all travel modes and separating regional traffic from local traffic
- Improved traffic flow by separating regional traffic from local traffic, which reinforces Seward Highway's function as part of the National Highway System and a strategic freight route

The recommended alternative is consistent with current planning documents and meets the purpose and need of this project.

5.6.1 Landscaping Plan

A landscaping plan will be developed for the recommended alternative as a part of final design. Fencing needs and placement will also be determined as the design progresses, based on controlled access lines and design. Likewise, the need and placement of sound barriers will be determined following a noise study. DOT&PF anticipates the placement of topsoil and seed in disturbed locations. The Department will coordinate with the MOA on landscaping details when the project design is more advanced.

5.6.2 Street Illumination

Street illumination will be designed to meet existing design standards and blend in with the existing street environment. No hightower lighting is proposed for the interchange.

5.6.3 Long Term Impact on Existing and Projected Land Uses in the Vicinity

The proposed interchange will ease traffic and improve facilities for non-motorized users. Seward Highway in the vicinity of the interchange is already controlled access, and driveway access to properties along the section of Seward Highway between 36th Avenue and Benson Boulevard and along 36th Avenue will be provided as part of the project design. The highway and this intersection are noted in the land use plan and impacts on existing and projected land uses are expected to be minimal.

5.6.4 Short-Term and Long-Term Impact of Property Acquisition for ROW

With the exception of the acquisition of one property near the proposed interchange (currently occupied by the Sushi-Ya restaurant), property acquisitions are expected to be minimal and limited to small strips of land. The project will provide a long-term transportation solution and impacts on property acquisition for ROW are expected to be minimal.

6.0 PUBLIC INVOLVEMENT SUMMARY

Public involvement efforts for the Seward Highway: 36th Avenue Interchange project were initiated during the previous project, the MCR PEL Study, which began in 2017 and was completed in 2020. A mainstay of public involvement for the Seward Highway: 36th Avenue Interchange project has been regular updates to area Community Councils. In addition, the project team held one public open house in March 2021 and has another planned for spring 2022.

The 36th Avenue/Seward Highway intersection is used by a broad range of stakeholders. Stakeholders identified for this project, summarized in Table 3, consist of proximal residents, business and property owners, and agencies, as well as the public traveling in and through Midtown Anchorage.

Table 3: Project Stakeholders

Type of Stakeholder	Stakeholder
Public and Other	<ul style="list-style-type: none"> • Access Alaska • Alaska Trucking Association • Anchorage Chamber of Commerce • Bike Anchorage • Business owners and non-profit organizations in adjacent areas • Federation of Community Councils <ul style="list-style-type: none"> ○ Airport Heights ○ Midtown ○ Rogers Park ○ Tudor Area • Geneva Woods Homeowners Association • Property owners and residents in adjacent neighborhoods
Government, Regulatory, and Resource Agencies	<ul style="list-style-type: none"> • Alaska Legislature: Senators and Representatives • Alaska State Troopers • Anchorage School District • Environmental Protection Agency • Federal Highway Administration • MOA <ul style="list-style-type: none"> ○ Anchorage Assembly ○ Anchorage Fire Department ○ Anchorage Police Department ○ Department of Economic and Community Development ○ Maintenance and Operations ○ Mayor's Office ○ Parks and Recreation ○ Planning and Zoning Commission ○ Planning Department ○ Project Management and Engineering ○ Public Transportation Department

Type of Stakeholder	Stakeholder
	<ul style="list-style-type: none"> ○ Traffic Engineering ○ Urban Design Commission ● State Historic Preservation Office ● State of Alaska Department of Environmental Conservation ● State of Alaska Department of Fish and Game ● State of Alaska Department of Natural Resources ● United States Army Corps of Engineers ● United States Fish and Wildlife Service ● University of Alaska Anchorage
AMATS	<ul style="list-style-type: none"> ● Technical and Policy Committees ● Bicycle and Pedestrian Advisory Committee ● Freight Advisory Committee
Utility Companies	<ul style="list-style-type: none"> ● Alaska Communications Systems ● Anchorage Water and Wastewater Utility ● Chugach Electric Association ● ENSTAR Natural Gas Company ● General Communications Inc.

6.1 Community Council Meetings

Members of the project team have attended numerous area community council meetings since completion of the MCR PEL Study in late spring 2020 and continuing through fall 2021 to provide regular project updates and answer questions (see Table 4). Available community council meeting agendas, presentations, and summaries are included in Appendix B. Resolutions regarding the project that were deliberated and passed by the community councils are noted in the table with asterisks and are also included in Appendix B.

Table 4: Community Council Participation

Community Council	July 2020	February 2021	March 2021	April 2021	May 2021	June 2021	Sept 2021
Airport Heights		X		X*			X
Campbell Park		X					X
Fairview		X					X
Midtown		X			X	X	X
North Star		X					X
Rogers Park	X	X	X	X*	X		X
Tudor Area			X*		X		X

6.2 Open House #1 – March 16, 2021

The project team hosted a virtual open house on March 16 as two separate events: one from 12:00 to 1:30 PM and another from 5:00 to 7:00 PM. Both meetings started with a welcome, showed a 15-minute pre-recorded presentation, asked participants two informal poll questions, and then moved into two smaller breakout room groups for a moderated discussion. More than 80 people participated in the lunchtime meeting and approximately 70 people attended the evening meeting. The presentation included a detailed discussion of the alternatives under consideration with a particular emphasis on non-motorized facility improvements.

The project team asked participants to take a two-minute survey after the meeting, and a link was provided through the meeting chat. Twenty-four people took the survey. Survey results showed:

- Open house participants overwhelmingly indicated not feeling safe walking or bicycling through the existing intersection
- Improved traffic flow along 36th Avenue and improved pedestrian facilities are the two features that most excite people about the project
- Cost, ROW, construction impacts, property access, and non-motorized facilities are concerns expressed by participants

The open house meeting summary, which includes a summary of the stakeholder survey results, is attached in Appendix B.

6.3 Anchorage Transportation Fair – November 18, 2020

Members of the project team presented a summary of the project during the 2020 Virtual Anchorage Transportation Fair. The project was also represented at the fair with a dedicated page on the online platform hosting the meeting where the public could make comments and ask questions about the project. During the three-hour event the project's page received 129 views and six people left comments/questions concerning the following:

- Concern about the dangerous existing conditions for bicyclists and pedestrians traveling through the intersection and request for reduced vehicular traffic speed, improved non-motorized crossings, and improved lighting
- Support for the project's proposed bicycle and pedestrian improvements
- Concern about directional movement restrictions with an interchange
- Questions about the project's impact on the Best Western Golden Lion Hotel and the office buildings between the Seward Highway and LaTouche Street
- Questions about the environmental process and whether that will be an abbreviated process because of the work completed for the PEL
- Questions about the degree to which public input heard through the PEL process will be considered in selection of a preferred alternative for the interchange

Transportation fair participant comments are attached in Appendix B.

6.4 Planned Public Involvement

The project team has developed a Public Involvement Plan (PIP) that outlines how it will work with stakeholders to communicate the goals of the project and gather input. The PIP also defines how the project team will meet relevant Federal, DOT&PF, and MOA requirements (including the Context Sensitive Solutions Process) for public involvement. Key outreach strategies from the PIP are shown in Table 5. Dates from the original PIP have been updated in the table below as the project schedule has evolved. The full PIP is attached in Appendix B.

Table 5: Key Outreach Strategies

Strategy	Details
Project website and email	www.36thInterchange.com 36thInterchange@dowl.com
Public meetings	<p>March 2021: Present alternatives under consideration to stakeholders for input.</p> <p>Spring 2022: Provide update on design alternatives and analysis methodology, including likely recommended alternative.</p> <p>Fall 2023: Present environmental document and preferred alternative.</p>
Community Councils and Other Interest Group Meetings	<p>The project team will maintain regular interaction with various stakeholder groups within the corridor, including attendance at Midtown, Rogers Park, Campbell Park, and Airport Heights Community Council meetings at key milestones during the project. The project team will be available to present and solicit comments from other interested groups upon request.</p>
Government/Agency Presentations	<p>Presentations will be made to the AMATS Technical and Policy Committees as well as the AMATS Bicycle and Pedestrian Advisory Committee and Freight Advisory Committee (if requested) at relevant project milestones. Assembly members representing this part of Anchorage and members of the MOA Planning and Zoning Commission and Urban Design Commission will be included in all project outreach. If requested, additional government/agency presentations will be scheduled.</p>

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7.0 ROUGH ESTIMATED PROJECT COST

It is anticipated that more than 90 percent of the funding for the Seward Highway: 36th Avenue Interchange project will be federally funded and developed in accordance with Federal Highway Administration guidelines. The project is currently proceeding using State funding in lieu of the State match required for Federal funds.

Estimated costs for an elevated loop ramp concept are summarized in Table 6.

Table 6: Summary of Estimated Costs

Description	Estimated Cost
Earthwork	\$8,200,000
Concrete Flatwork	\$2,300,000
Bases and pavements	\$3,600,000
Miscellaneous Construction	\$3,100,000
Demolition	\$2,600,000
Roadway	\$18,600,000
Utilities	\$1,500,000
Structures	\$35,800,000
Landscaping (Topsoil & Seeding)	\$400,000
Field Services (Mob/Demob, Traffic Control, etc.)	\$11,400,000
Sub Total	\$67,700,000
Construction Management (15%)	\$10,200,000
ICAP (4.64%)	\$3,200,000
Construction Subtotal	\$81,100,000
Right of Way	\$2,600,000
Utility Relocation	\$7,500,000
Design Engineering (10%)	\$8,200,000
ICAP (4.64%)	\$900,000
Preconstruction Subtotal	\$19,200,000
Project Subtotal	\$100,300,000
Contingency 20%	\$20,100,000
Total	\$120,400,000

8.0 MAINTENANCE CONSIDERATIONS

Periodic maintenance will be required following construction. It will remain the responsibility of the State of Alaska and local DOT&PF Maintenance and Operations. The MOA will assume maintenance of 36th Avenue upon completion of construction. This project will increase maintenance efforts by adding lanes to the mainline, constructing new frontage roads, new bridge, retaining walls at the grade separation, adding new drainage pipes and structures, and adding pedestrian and bicycle facilities.

During winter months, snow removal will be required on an as-needed basis for vehicle and pedestrian facilities. Snow removal requires both an on-street area large enough for temporary snow storage and a clear area large enough to load snow into trucks for off-site disposal. Snow removal level of service of all State facilities will be commensurate with funding availability.

Winter sand must be removed in the spring and periodic sweeping may continue during the summer months. Regular inspection of drainage facilities and structures will be necessary to determine if any cleaning or repairs are required.

Periodic maintenance will also be required for street lighting, traffic signals, and traffic striping and signs.

9.0 RESPONSE TO COMMENTS FROM CONCEPT REPORT REVIEW

The Concept Report will be reviewed as an informational item at the MOA Planning and Zoning Commission meeting on January 10, which is after the date this document will be submitted. Staff comments resulting from the pre-application conference held on December 9, 2021 and an informal review of the concept report are addressed below in Table 7; comments are included in Appendix C. Formal comments from the MOA staff review of the Concept Report may be added as an addendum to this report prior to the Planning & Zoning Commission meeting at which this document will be considered.

Table 7: Staff Comments and Adjudication

MOA Division	Staff Comment	Adjudication
Planning	Public involvement activities and public feedback need to be addressed in the DSR	See Section 6 and Appendix B.
Planning	Application needs to explain how the project meets planning documents, including 2010 Anchorage Bicycle Plan, Draft Non-Motorized Plan, and 2040 Land Use Plan.	See Section 2, particularly: <ul style="list-style-type: none"> • 2.6 – Pedestrian Plan • 2.7 – Bicycle Plan • 2.9 – 2040 Land Use Plan • 2.11 – Draft Non-Motorized Plan
Planning	Application needs to address landscaping, fencing, and sound barriers, and show bridges and wetlands/streams impacts in the preliminary plans.	See Section 5.1.8.1, Landscaping Plan. Streams will not be impacted. A Wetland Delineation was completed as a part of the MCR PEL Study and is included as Appendix D. Bridge and ramp locations are identified in preliminary plans, Appendix A.
Planning	Address utilities and storm drains in the application.	See Sections 3.4, 3.5, and 5.0 – Existing Drainage, Existing Utilities, and Design Alternatives.
Long Range Planning	Explain how the project would tie into other nearby projects.	See Section 5.1.4.3.
Long Range Planning	Clarify intent of project's changes to roadways and surrounding land, specifically whether the priority is safety or vehicle flow.	See Sections 1.3 and 1.4, Project Purpose and Project Need. The intent of the project's changes is both safety and access. Inadequate safety and vehicular congestion are inextricably linked existing conditions that this project seeks to remedy.
Private Development	Access changes to Moose's Tooth will be beneficial.	N/A
Private Development	Asked questions about signal replacement in the area over the summer.	Sean Baski responded during the meeting that the referenced signal replacement was not related to this project.
Traffic Engineering	Commented that Traffic Engineering is not likely to have issues with the concept Report level design, and that there are no comments at this stage of the project.	N/A
Traffic Engineering	Noted the importance of signage.	MOA Planning instructed the team to submit plans for and answer questions about sign location and sign design at the next CSS submittal, the Plans in Hand phase.

10.0 PRELIMINARY PROJECT PLANS

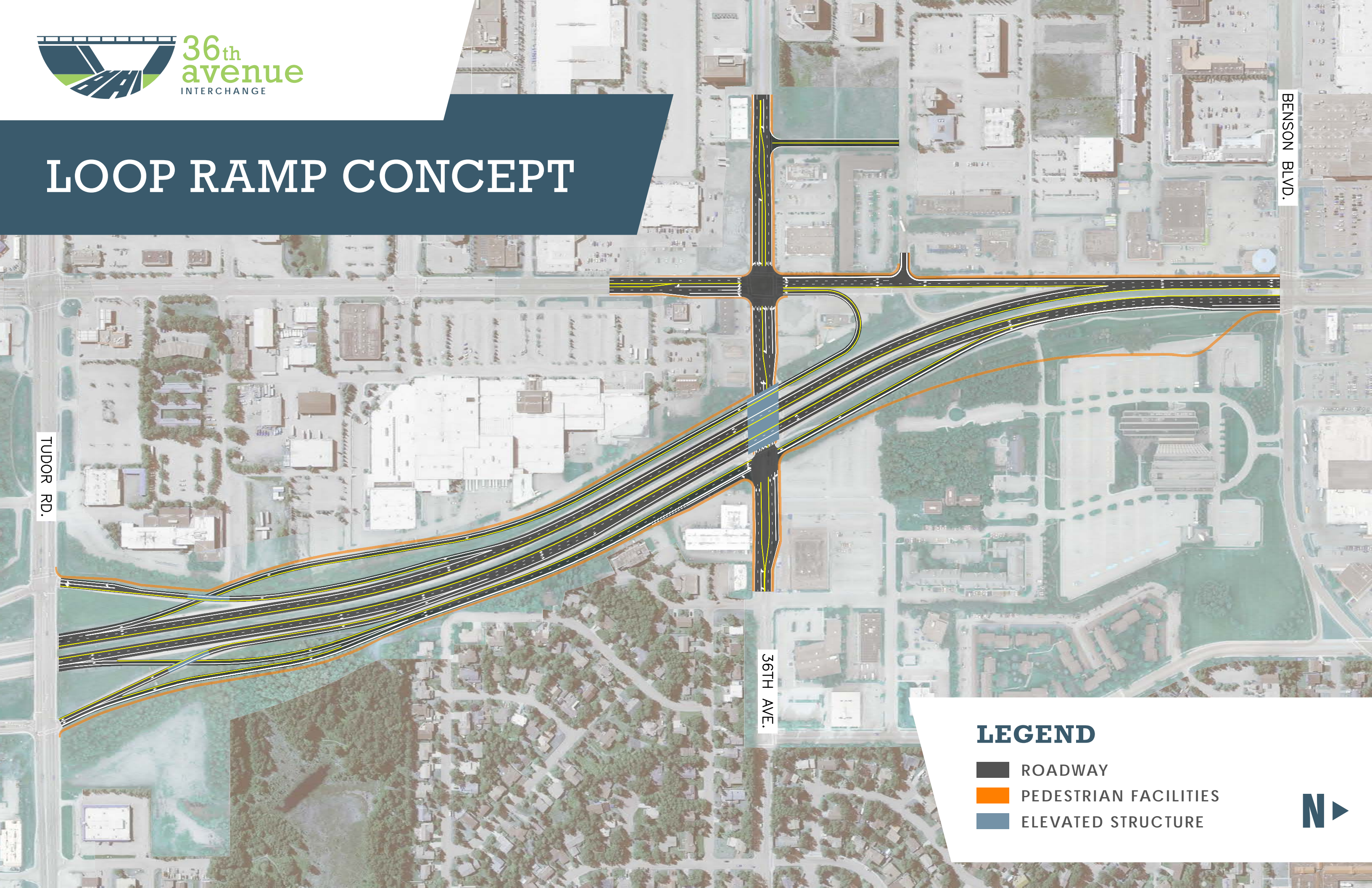
Preliminary project plans are attached as Appendix A.

APPENDIX A: PRELIMINARY PLANS



36th
avenue
INTERCHANGE

LOOP RAMP CONCEPT



TUDOR RD.

BENSON BLVD.

36TH AVE.

LEGEND

- ROADWAY
- PEDESTRIAN FACILITIES
- ELEVATED STRUCTURE



APPENDIX B: PUBLIC INVOLVEMENT

Appendix B Contents

1. Seward Highway: 36th Avenue Interchange Public and Agency Involvement Plan
2. Open House #1 Meeting Summary
3. 2020 Anchorage Transportation Fair Meeting Summary
4. Community Council Meeting Documents
 - A. Airport Heights Community Council
 - 1) February 2021
 - Meeting Agenda
 - Meeting Notes
 - 2) April 2021
 - Meeting Agenda
 - Resolution
 - Meeting Notes
 - 3) September 2021
 - Meeting Agenda
 - Project Update Presentation
 - Meeting Notes
 - B. Campbell Park Community Council
 - 1) February 2021
 - Meeting Agenda
 - Meeting Notes
 - 2) September 2021
 - Meeting Agenda
 - Project Update Presentation
 - Meeting Notes
 - C. Fairview Community Council
 - 1) February 2021
 - Meeting Agenda
 - Meeting Minutes
 - 2) September 2021
 - Meeting Agenda
 - Project Update Presentation
 - Meeting Notes
 - D. Midtown Community Council
 - 1) February 2021
 - Meeting Notes (Notes include summary of North Star Community Council meeting held on the same day)
 - 2) May 2021
 - Meeting Notes
 - 3) June 2021
 - Meeting Presentation

- Meeting Notes
- E. North Star Community Council
- 1) February 2021
 - Meeting Agenda
 - *Meeting Notes are included in the February 2021 Midtown Community Council notes*
 - 2) September 2021
 - Meeting Agenda
 - Meeting Presentation
 - Meeting Notes
- F. Rogers Park Community Council
- 1) July 2020
 - Meeting Agenda
 - Meeting Notes
 - 2) February 2021
 - Meeting Agenda
 - Meeting Minutes
 - 3) March 2021
 - Meeting Agenda
 - Meeting Notes
 - 4) April 2021
 - Meeting Minutes
 - Resolution
 - 5) May 2021
 - Meeting Agenda
 - Meeting Presentation
 - Meeting Notes
 - 6) September 2021
 - Meeting Agenda
 - Meeting Notes
- G. Tudor Area Community Council
- 1) March 2021
 - Resolution
 - 2) May 2021
 - Meeting Notes
 - 3) September 2021
 - Meeting Agenda
 - Project Update Presentation
 - Meeting Notes

PUBLIC INVOLVEMENT PLAN

Seward Highway: 36th Avenue Interchange

State Project No: CSHWY00298



Prepared for:

State of Alaska
Department of Transportation and Public Facilities
Central Region
4111 Aviation Drive
Anchorage, AK 99519

Prepared by:

DOWL
4041 B Street
Anchorage, AK 99503

January 2021

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ACRONYMS

ACS	Alaska Communications Systems
ADA	Americans with Disabilities Act
ADF&G	Alaska Department of Fish and Game
AMATS	Anchorage Metropolitan Area Transportation Solutions
AMC	Anchorage Municipal Code
AST	Alaska State Troopers
AWWU	Anchorage Water and Wastewater Utility
CEA	Chugach Electric Association
CSS	Context Sensitive Solutions
DEC	State of Alaska Department of Environmental Conservation
DOT&PF	State of Alaska Department of Transportation and Public Facilities
DNR	State of Alaska Department of Natural Resources
EPA	Environmental Protection Agency
EPM	Environmental Procedures Manual
FHWA	Federal Highways Administration
GCI	General Communications Inc.
HPCM	Highway Preconstruction Manual
MOA	Municipality of Anchorage
NEPA	National Environmental Policy Act
PEL	Planning and Environmental Linkages
PIP	Public Involvement Plan
PZC	Planning and Zoning Commission
SHPO	State Historic Preservation Office
STIP	Statewide Transportation Improvement Program
UAA	University of Alaska Anchorage
USACE	United States Army Corps of Engineers
USFWS	United States Fish and Wildlife Service

1.0 PROJECT TEAM

State of Alaska Department of Transportation and Public Facilities (DOT&PF) – Central Region

Sean Baski, P.E., Project Manager, (907) 269-0547, sean.baski@alaska.gov

John Farr, EIT, Project Engineer, (907) 269-0597, john.farr@alaska.gov

DOWL Steve Noble, P.E., Project Manager, (907) 560-2000, snoble@dowl.com

Renee Whitesell, PTP, Planner, (907) 562-2000, rwhitesell@dowl.com

Rachel Steer, Public Involvement, (907) 562-2000, rsteer@dowl.com

Katie Conway, Public Involvement, (907) 562-2000, kconway@dowl.com

2.0 GENERAL INFORMATION

2.1 Project Overview

The Seward Highway: 36th Avenue Interchange project will improve safety and reduce delays by constructing a new interchange at the intersection of Seward Highway and 36th Avenue. This is one of the busiest and highest-crash-rate intersections in the Anchorage Bowl. Bicyclists and pedestrians report that it is difficult to cross the highway and users report a lack of east-west connectivity in the area. Planned improvements include grade separations, highway ramps, non-motorized facilities, and new frontage roads connecting Tudor Road to 36th Avenue.

This project will increase connectivity by:

- Improving flow for all travel modes
- Supporting separation of local trips from through trips
- Improving facilities for pedestrians and bicyclists
- Increasing safety
- Reducing delays

Figure 1: Project Area



2.2 Project History

The Seward Highway: 36th Avenue project has a long history dating back at least to the Anchorage Metropolitan Area Transportation Study Major Corridors Study (March, 1982), which evaluated alternatives to connect the Seward Highway from its intersection with Tudor Road through downtown Anchorage to the Glenn Highway. This project grew into the Seward Highway to Glenn Highway Connection Project (H2H), which was paused in 2011 due to concerns about the scale of the project and public push-back.

In 2012, DOT&PF chose to focus only on the Midtown section of the previous H2H project, as alternatives for improvements between the Tudor Road interchange and Chester Creek shared the same configuration and would not jeopardize flexibility to ultimately create a connection between the Seward Highway and Glenn Highway. In late 2012, DOT&PF further revised the project scope to focus on a standalone project at the intersection of Seward Highway and 36th Avenue. This project was named Seward and 36th Improvements. The project proceeded to approximately 35 percent design, but there were significant public concerns raised about the preferred alternative interchange layout, particularly northbound access to Seward Highway from 36th Avenue. As a result of the public response, the project was paused in early 2015.

In 2017, DOT&PF moved forward with efforts to relieve congestion in Midtown (Midtown Congestion Relief), which focused on the Seward Highway from the Tudor Road Interchange to approximately 20th Avenue. The first step was completion of a Planning and Environmental Linkages (PEL) Study to determine purpose, need and scope for roadway improvements along this section of the Seward Highway, and recommend one or more projects with independent purpose and need and logical termini that could be constructed to alleviate congestion and enhance safety. The PEL Study concluded in April 2020, and five separate projects were recommended. Preliminary environmental work and alternatives development commenced on two of the projects recommended in the PEL Study, which sought to complete a frontage road network and at-grade intersection improvements.

In July of 2020, DOT&PF revised the project scope to focus on the 36th Avenue Interchange, which will focus on creating a grade-separated interchange between the Seward Highway and 36th Avenue, with associated ramps and other access improvements.

2.3 Purpose of the Public Involvement Plan

This Public Involvement Plan (PIP) details the proposed methods for communicating information with agencies, organizations, and the public for the project. The objective of this effort is to work directly with the interested public and other stakeholders to communicate the goals of the project and gather input. It will also ensure the project team is meeting the relevant Federal, DOT&PF, and Municipality of Anchorage (MOA) requirements for public involvement.

Public and agency involvement on transportation projects completed by the DOT&PF within the MOA is guided by a range of documents including:

- Alaska DOT&PF Environmental Procedures Manual (EPM)
- Alaska DOT&PF Highway Preconstruction Manual (HPCM)

- National Environmental Policy Act (NEPA) Assignment Program Memorandum of Understanding between the Federal Highway Administration (FHWA) and DOT&PF
- Title VI of the Civil Rights Act of 1964
- Title 21 of the Anchorage Municipal Code (Chapter 21.03.190) (MOA)
- A Strategy for Developing Context Sensitive Transportation Projects (MOA, 2008)

The HPCM is the FHWA-approved guide for developing and designing federal-aid projects in Alaska, and is being used for this project on the expectation that federal funding will be allocated for design and construction in a future amendment to the Statewide Transportation Improvement Program (STIP). This PIP has been developed in accordance with the requirements and best practices as outlined in the HPCM and the EPM.

The DOT&PF is required by federal agreement to include a disclosure regarding Title VI of the Civil Rights Act of 1964, and Americans with Disabilities Act of 1990 (ADA), compliance as part of agency outreach and public involvement. DOWL will work with the DOT&PF Civil Rights Office to confirm the most current language prior to publication. At the time of writing this PIP the current language is:

The environmental review, consultation, and other actions required by applicable Federal environmental laws for this project are being, or have been, carried out by DOT&PF pursuant to 23 U.S.C 327 and a Memorandum of Understanding dated November 3, 2017 and executed by FHWA and DOT&PF.

The DOT&PF Civil Rights Offices notices will be published with all public announcements and Title VI documents will be displayed at public meetings and documented in the meeting record.

The project will follow the MOA's CSS process. This will include submittals to the MOA Planning and Zoning Commission at the Concept Report and Alternatives Analysis (commonly referred to as the Draft Design Study Report) stage. The third submittal to the Urban Design Commission will occur at no more than 65 percent design. Further detail on the CSS process is included in Part 6 of this PIP.

3.0 IDENTIFICATION OF STAKEHOLDERS

As noted in Section 2.2, this project follows a long history of project starts. The most recent effort, the Midtown Congestion Relief PEL Study, developed an extensive mailing list and as this project is immediately following that effort, the stakeholder list will be used as a starting point for communication on this project.

Below is an initial list of stakeholders. Where possible, these entities will be included in the project outreach/ mailing list. This list will expand throughout the project.

3.1 Public and Other

- Property owners and residents in adjacent neighborhoods
- Business owners and non-profit organizations in adjacent areas

- Commuters
- Anchorage Chamber of Commerce
- Anchorage School District
- University of Alaska Anchorage (UAA)
- Federation of Community Councils
 - Rogers Park
 - Tudor Area
 - Midtown
 - North Star
 - Fairview
 - Campbell Park
 - Airport Heights
- Alaska Trucking Association
- Bike Anchorage
- Access Alaska

3.2 Government, Regulatory, and Resource Agencies

- Federal Highway Administration
- United States Fish and Wildlife Service (USFWS)
- Alaska State Senators and Representatives
- State of Alaska Department of Environmental Conservation (DEC)
- Alaska Department of Fish and Game (ADF&G)
- State of Alaska Department of Natural Resources (DNR)
- State Historic Preservation Office (SHPO)
- United States Army Corps of Engineers (USACE)
- Alaska State Troopers (AST)
- Environmental Protection Agency (EPA)

- MOA
 - Mayor's Office
 - Anchorage Assembly
 - Planning Department
 - Parks and Recreation
 - Public Transportation Department
 - Anchorage Fire Department
 - Anchorage Police Department
 - Department of Economic and Community Development
 - Planning and Zoning Commission
 - Project Management and Engineering
 - Traffic Engineering
 - Maintenance and Operations
 - Urban Design Commission
- Anchorage Metropolitan Area Transportation Solutions (AMATS)
 - Technical and Policy Committees
 - Bicycle and Pedestrian Advisory Committee
 - Freight Advisory Committee

3.3 Utility Companies

- Alaska Communications Systems (ACS)
- Anchorage Water and Wastewater Utility (AWWU)
- Chugach Electric Association, Inc. (CEA)
- ENSTAR Natural Gas Company
- General Communications Inc. (GCI)

4.0 STAKEHOLDER ISSUES AND CONCERNS

The project team has identified the following potential concerns. Several of these are drawn from the Midtown Congestion Relief PEL Study and are specific to this project. Additional issues and concerns will be documented throughout the design phase.

Issue	Level of Perceived Controversy
Area is used by a broad range of stakeholders that travel from throughout the Anchorage Bowl and beyond (residents, businesses, commuting public)	There will be some who support the project and some who do not because of higher perceived impacts than benefits. Public involvement will be closely evaluated throughout the project to ensure adequate public outreach and feedback, given the numerous potential stakeholders and interested public in this project.
Current corridor conditions	Agencies and the public are concerned about safety, access, and congestion.
Access for all transportation modes	There are existing challenges with access along and across the Seward Highway, because of congestion levels, and traffic conflicts at intersections and driveways. Without improvements, delays from congestion and access conflicts are forecast to increase over time. Both motorized and non-motorized users experience issues associated with poor east-west connections across the Seward Highway corridor.
Noise impacts	Area residents are concerned about noise impacts related to future projects, particularly associated with grade separated improvements.
Light/illumination impacts	Area residents are concerned about light/illumination impacts from future projects.
Visual impacts	Area residents are concerned about the visual impacts of roadway improvements, particularly grade-separated interchanges that create visual barriers.
Non-motorized travel improvements/accommodations	Strong focus on non-motorized accommodations is needed to ensure there is adequate accommodations for all modes of travel in the project area, and all travelers feel they can use the space comfortably and safely.
Right-of-way impacts	Loss of parking and direct access for property owners could be perceived negatively.
Disruption to area businesses and residents that may result from construction	Unavoidable disruption and inconvenience could have a negative impact on stakeholders, including temporary loss of direct access to some properties.
Impacts related to future projects	Concern that constructing a grade-separated improvement at the 36 th Avenue interchange may limit options for improvements at other at-grade intersections (i.e., Benson Boulevard, Northern Lights Boulevard, and Fireweed Lane). Also concern that constructing improvements at the 36 th Avenue intersection may exacerbate safety and congestion issues at other at-grade intersections.

5.0 PUBLIC INVOLVEMENT METHODS

The following subsections identify the methods proposed to reach agencies and stakeholders with project information. The project team will discuss outreach efforts and select other tools for involving the public and agencies if feedback is considered insufficient.

5.1 Mailing Lists

Mailing lists for electronic and postal distribution will be purchased or created and maintained for all property owners, businesses, and residents within 500 feet of the project corridor. Constant Contact will be used to deliver electronic newsletters and other project updates. The project website will include a mechanism for interested persons to request to be added to the email distribution list.

The most recent effort, the Midtown Congestion Relief PEL Study, developed an extensive mailing list and as this project is immediately following that effort, the mailing list will be used as a starting point for communication on this project.

5.2 Public Open House Meetings

Three public meetings are planned to inform the public about the project and to solicit comments or concerns. Given current public health uncertainties around the global COVID-19 pandemic, these open houses may be virtual, in-person, or a combination on both, depending on the circumstances and guidance from local, state, and federal governments at the time the meetings are being planned. Any pre-recorded presentations created for public open house meetings will be posted on the project website to enable people to review materials and comment if they are unable to attend an open house at a specified time.

For all open house meetings, project stakeholders will be notified via postcard mailed 21 days prior to the meeting, by public notice on the State of Alaska website, by advertisement in the Anchorage Daily News, and by notification to a Constant Contact email list. Meeting notes will be prepared as documentation for project records that will include meeting materials, presentation, and a summary of public comments.

Further detail about each of the public meetings is provided in the subsections below. This part of the PIP will be updated as the project progresses.

5.2.1 Public Open House Meeting 1

The first public open house meeting is scheduled for March 2, 2021, on completion of the Concept Report to present alternatives under consideration. The alternatives will include intersection configurations outlined in the “preferred concepts” in the Midtown Congestion Relief PEL Study, specifically the:

- Loop ramp grade-separated interchange
- Median u-turn grade-separated interchange

Other alternatives will be developed and presented, together with descriptions of the strengths, weaknesses, and compromises needed to accommodate each alternative. The purpose of the material presented will be to allow the public to understand the layout of each alternative and provide feedback on what elements they would like to see prioritized as each interchange alternative is evaluated (i.e., access, speed, multimodal facilities, visual impact, noise, etc.). This feedback will be used to help guide the design process and will contribute to the future identification of a preferred alternative.

The design decision about whether the Seward Highway will go over or under 36th Avenue will be introduced conceptually during the first open house meeting, but a recommendation on the profile will be reserved until second open house meeting.

5.2.2 Public Open House Meeting 2

The second public open house meeting is scheduled for late summer 2021. The project team will provide an update on development of the alternatives for the interchange, and will focus specifically on the design profile (i.e., whether the Seward Highway will go over or under 36th Avenue).

The project team will present information and create opportunities for the public to comment on their priorities relating to different aspects of the over vs. under profile (i.e., cost to construct, maintenance costs, visual impacts, noise impacts, connectivity between the east and west side of the highway, etc.). This feedback will be used to help guide the design process and will contribute to the future identification of a preferred alternative.

5.2.3 Public Open House Meeting 3

The third public open house meeting is scheduled for December 2021, prior to the submittal of the Alternatives Analysis to the MOA's Planning and Zoning Commission. At this open house the project team will provide an update on the alternatives for the interchange and progress toward identifying a preferred alternative.

5.3 Website, Email, and Social Media

A project website has been established at www.36thInterchange.com using the State of Alaska Look and Feel Standards (<http://alaska.gov/LookAndFeel/>), which is ADA compliant. The website will be maintained with project background information, a current schedule, meeting information, and contact information for the project team.

The email address 36thInterchange@dowl.com will be included on all communication materials. The email address will be linked to a distribution list that includes members from the DOT&PF and DOWL project team.

The project team will work with the DOT&PF public information team to craft and schedule project-specific content for the DOT&PF Facebook page, Twitter feed, and Instagram account. Social media advertising campaigns will be initiated in advance of the public open house meetings.

5.4 Community Councils and Other Interest Group Meetings

The project team will maintain regular interaction with various stakeholder groups within the project area, including attendance at and updates to the Rogers Park, Tudor Area, Campbell

Park, Midtown, North Star, Fairview and Airport Heights Community Councils at key milestones during the project.

Following Section 450.17, subsection “Communities with a Community Council” in the HPCM, a formal letter will be sent at every major project milestone to Community Councils within a half mile of the project area. These Community Councils are Rogers Park, Tudor, and Midtown.

5.5 Project Postcards and Newsletters

A postcard will be distributed to announce the three public open house meetings at least 21 days in advance. The postcard will be mailed, and an electronic version will be emailed to the contact list. Both will contain graphics and text explaining the alternatives under consideration and ask for public input. Additionally, e-newsletters will be distributed at relevant project milestones.

All reasonable accommodation will be made to ensure visually impaired individuals are able to access digital outreach materials, and best practices for ADA compliant email communication will be closely followed. For example, descriptive meta data will be embedded in PDFs used as digital outreach materials in emails or on websites, and text will not be flattened or outlined so that it remains readable by a machine. Alternate formats of print outreach materials can be produced upon request.

5.6 Advertising

Public meetings will be advertised in the Anchorage Daily News, on the statewide DOT&PF Facebook page, through email notices to the project contact list, and on the State of Alaska Online Public Notice system. Meeting advertisements will be prepared in advance to allow for DOT&PF approval.

5.7 Government/Agency Presentations

Presentations will be made to the AMATS Technical and Policy Committees as well as the AMATS Freight Committee and Bicycle and Pedestrian Advisory Committee at relevant project milestones. If requested, additional government/agency presentations will be scheduled.

5.8 Media Communications

The project team will coordinate with the DOT&PF project manager and public information officers prior to any media communications. Media communications may include press releases, media briefings, telephone interviews, and communication with elected officials.

5.9 Additional Public Involvement

The project team will conduct additional public involvement, as required, throughout the project. Additional public involvement may include (as directed and agreed by the DOT&PF Project Manager):

- Community Council presentations
- Targeted literature drops to key stakeholders

- Pop-up open houses (road shows)
- Responses to informal requests for information from individuals and/or agencies.

6.0 MUNICIPALITY OF ANCHORAGE CONTEXT SENSITIVE SOLUTIONS (CSS) PROCESS

The MOA's CSS process is set out in Title 21 of the Anchorage Municipal Code (AMC), and guided by A Strategy for Developing Context Sensitive Transportation Projects adopted by the MOA in October 2008.

Chapter 21.03.190 Street and Trail Review notes that new construction and reconstruction of street and intersection projects involving streets of collector classification or greater in the Official Streets and Highways Plan are required to follow a review process by the planning and Zoning Commission (PZC) and Urban Design Commission (UDC). The stages are:

- A concept report or equivalent, distributed to the PZC as an information item
- The draft design study report, reviewed by the PZC
- The plans in hand design drawings, review by the UDC

The project team will prepare and submit a concept report shortly after the first public open house meeting, in winter of 2021. An alternatives analysis (referred to as a design study report Title 21) will be prepared and submitted around the time of the third public open house, to enable the PZC to comment on alternatives being considered for the interchange. The third stage will be accomplished as part of future work but at no more than 65 percent design.

The project team will maintain communication with the MOA Planning Department to ensure the project progresses smoothly through the identified CSS project stages.

7.0 COMMUNICATING WITH THE PUBLIC AND DOCUMENTATION

Comments received via email, mail, phone, or in written form will be distributed to the project team and archived in the project comment database managed by DOWL. All comments and questions will be replied to within two (2) business days.

8.0 PUBLIC INVOLVEMENT PLAN IMPLEMENTATION

Guidelines for a successful public involvement process using context sensitive solutions requires a strong commitment from all members of the project team to reach out to potentially affected interests. Below are goals developed for obtaining public outreach throughout the progression of the project.

- The roles of the project team and the public will be defined.
- A working relationship between the community and the project team will be formed.
- Goals and expectations will be clearly defined.

- The project team will keep an open mind to all points of view.
- The project team will keep the public informed, listen to and acknowledge concerns and provide feedback on how public input has influenced project decisions.
- Information will be shared openly and directly to help the public become familiar with the issues and the public process.
- Milestones in the process will be clearly defined, acknowledged, and communicated to the public.
- Visualizations and illustrations will be used to convey design alternatives.
- The project team will encourage the public to provide feedback on elements of the design that are affected by input (landscaping, lighting, etc.).
- Feedback opportunities will be designed in a way that each stakeholder can communicate in their desired form.
- The project team will follow up on all inquiries and comments during project development.

APPROVAL

Sean Baski, P.E.
Project Manager, DOT&PF

Date

James Amundsen, P.E.
Chief, Central Region Highway Design Group, DOT&PF

Date

Brian Elliott
Manager, Regional Environmental Program, DOT&PF

Date

Project Number:	State CSHWY00298 / Federal 0001659
Date/Time/Location:	March 16, 2021 12:00 pm – 1:30 pm and 5:00 pm – 7:00 pm Virtual – Zoom
Staff Present:	DOT&PF: Sean Baski, John Farr, Heidi Zimmer Kittelson: Andrew Ooms DOWL: Steve Noble, Renee Whitesell, Rachel Steer, Kelly Kilpatrick, Katie Conway, Sam Tyler, Brad Coy, Alexa Greene
Elected Officials:	State Senator Roger Holland; Anchorage Assembly – John Weddleton and Crystal Kennedy; various AMATS committee members
Total Participants:	150

Meeting Summary

The Seward Highway: 36th Avenue Interchange project team hosted two virtual project open houses yesterday: one from 12:00 to 1:30 PM and another from 5:00 to 7:00 PM. Both meetings started with a welcome, showed the 15-minute pre-recorded presentation, asked two informal poll questions, and then moved into two smaller groups in breakout rooms for a moderated discussion.

More than 80 people participated in the lunchtime meeting, including members from the project team. Of note, State Senator Roger Holland and his staff, and Anchorage Assembly member John Weddleton attended.

Approximately 70 people participated in the evening meeting, including members from the project team. Of note, Anchorage Assembly member Crystal Kennedy attended, as well as several members from various AMATS committees.

The informal poll questions indicated that many of the participants were residents or regularly traveled through the project area. There was particular interest from residents of the Geneva Woods, College Village, and Rogers Park neighborhoods.

Attendees were then randomly broken out in to two-breakout rooms. Breakout Room 1 was led by Sean Baski and moderated by Rachel Steer and Breakout Room 2 was led by Steve Noble and moderated by Katie Conway, during both meetings each of the breakout rooms had between 25-30 attendees

Questions and Comments Summary

- Impacts to Geneva Woods residents associated with raising the Seward Highway, particularly access, noise, and visual impacts
- Emergency access to the UMED district
- Project cost
- Drainage, pertaining to potential drainage into Chester Creek
- Impacts to snow removal needs

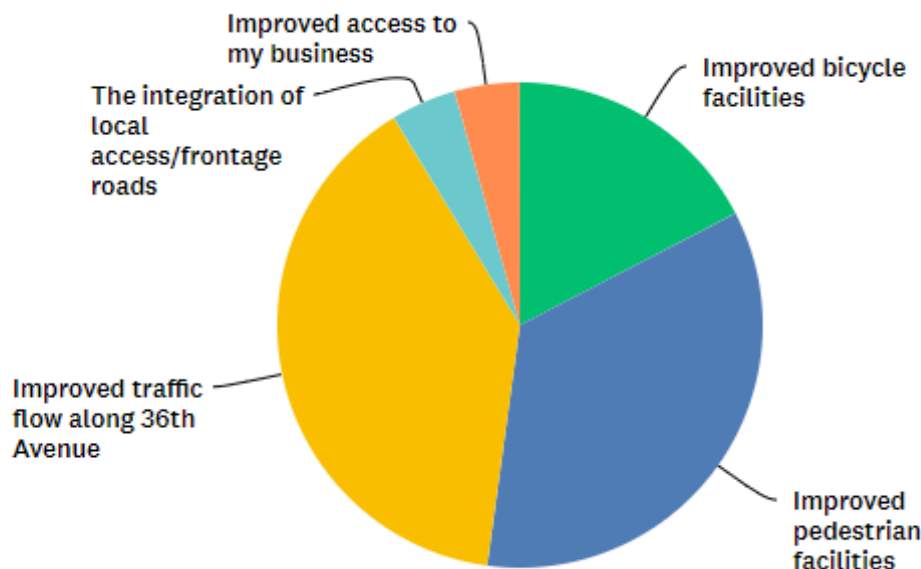
- Design elements (Under/Over, frontage roads, jug handles, tunnels for non-motorized crossings)
- Pedestrian and bicycle facilities (improve overall safety for all non-motorized users)
- Potential ramifications to the MOA plan for the Golden Lion hotel
- Project schedule
- Noise study (noise study is required as part of this project)
- Whether the project is considering the traffic impacts of changing travel modes (ride-share, non-motorized), and population changes associated with a declining population trend in Anchorage.

Stakeholder Survey Summary

We asked participants to take a two-minute survey after the meeting and a link was provided through the meeting chat. Twenty-four people took the survey over the two-week period it was live. Results show:

- Open house participants overwhelmingly indicate not feeling safe walking or bicycling through the existing intersection
- Improved traffic flow along 36th Avenue and improved pedestrian facilities are the two things that most excite people about the project
- Cost, right-of-way, construction impacts, property access, and non-motorized facilities are concerns expressed by participants

The following graph summarizes preliminary feedback on the elements people would most like to see from the project:



Detailed Survey Responses

1. Which road do you more frequently drive at the existing intersection?
 - a. 50% Seward Highway (north/south)
 - b. 42% 36th Avenue (east/west)
 - c. 8% Neither (I do not drive)
2. What is your level of comfort when walking or bicycling through this intersection?
 - a. 54% I don't feel safe at all and avoid crossing the intersection on foot or bicycle
 - b. 21% Not applicable
 - c. 13% I don't feel very safe, but I still cross on foot or bicycle
 - d. 8% I feel very safe
 - e. 4% I feel moderately safe
3. What are your primary concerns about the project?
 - a. High financial cost to construct and maintain
 - b. Geneva Woods access
 - c. Aesthetics
 - d. ROW impacts
 - e. Connectivity (36th traffic connecting to Seward Highway north)
 - f. Noise, during construction and after
 - g. Questionable need
 - h. Property impacts
 - i. Access for adjacent businesses
 - j. Panhandlers at intersection
 - k. Concern about non-motorized improvements being sacrificed
 - l. Traffic flow
4. Do you have any additional feedback or suggestions you'd like the project team to consider?
 - a. Earthquake damage mitigation that allows 36th to stay open for access to hospitals in the event of a large earthquake that causes the new overpass to fail
 - b. Design without prohibiting 36th to become a Primary Transit Corridor connecting the U-Med and Spenard areas in the future
 - c. Show winter conditions in future project graphics
 - d. Reduce highway speeds to make Benson Blvd safer
 - e. In future outreach, include information about construction impacts and impacts from the completed project to adjacent neighborhoods along 36th Ave, as well as mitigation plan for managing negative impacts
 - f. Incorporate non-motorized stakeholder concerns/requirements into the project design
 - g. Integrate technology that will provide real-time traffic information to the traveling public
 - h. When construction starts do it quickly
 - i. Keep 36th at grade
5. Would you like to be added to the project mailing list? (10 people responded with their contact information)

Seward Highway: 36th Avenue Interchange

Project Engagement

VIEWS	PARTICIPANTS	RESPONSES	COMMENTS
129	9	0	18

MOVE What should the team consider? FILTER BY SEGMENT OPTIONS

- lhajduk@gmail.com** This is one of the most dangerous intersections in Anchorage for pedestrians and it has a recommended bike route along only the northern end. This intersection will not be safe unless pedestrians are safe. I'd like to see safer intersection with: reduced traffic speed, head start crossing for pedestrians and long enough to not strand pedestrians in the middle of the crossing, adequate lighting at corners, flashing ped light, etc.
- 21 days ago 4 Agree
- Craig Wood** Strongly concur! I often walk across this intersection departing & returning from College View (Stanford Dr neighborhood) to access various Midtown establishments such as library, work & dining. I'm in good running shape but I find the pedestrian crossing almost as dangerous as some of my military assignments in Afghanistan! I really feel for the first older pedestrians & families who cross here. I'd love to take my grandchildren with me but it's not safe so I end up driving & contributing to the traffic congestion & pollution. This is a project that's long overdue! Please proceed!
- 21 days ago 1 Agree
- AK DOT&PF (Alaska Department of Transportation and Public Facilities) (Alaska Department of Transportation and Public Facilities)** Thank you for your feedback. Improving non-motorized safety will be a key part of this project. We appreciate your suggestions for enhanced pedestrian facilities.
- 16 days ago
- Tim Musgrove** reducing the posted speed limit as most drivers go over the speed limit in Anchorage
- 21 days ago 2 Agree
- AK DOT&PF (Alaska Department of Transportation and Public Facilities) (Alaska Department of Transportation and Public Facilities)** Thank you for your feedback. The posted speed limit will be an important consideration as we move forward with design of this project, including potentially reducing the speed limit or changing the intersection to improve safety for all transportation modes.
- 16 days ago
- Craig Wood** Strongly concur! I often walk across this intersection departing & returning from College View (Stanford Dr neighborhood) to access various Midtown establishments such as library, work & dining. I'm in good running shape but I find the pedestrian crossing almost as dangerous as some of my military assignments in Afghanistan! I really feel for the first older pedestrians & families who cross here. I'd love to take my grandchildren with me but it's not safe so I end up driving & contributing to the traffic congestion & pollution. This is a project that's long overdue! Please proceed!
- 21 days ago 1 Agree
- AK DOT&PF (Alaska Department of Transportation and Public Facilities) (Alaska Department of Transportation and Public Facilities)** Thank you for your feedback. Improving non-motorized safety will be a key part of this project. We appreciate your suggestions for enhanced pedestrian facilities.
- 16 days ago
- Karen Carson** yes by all means..
- 21 days ago
- AK DOT&PF (Alaska Department of Transportation and Public Facilities) (Alaska Department of Transportation and Public Facilities)** Thank you for your feedback. Please refer to our response to question 4.
- 16 days ago



Pat Redmond Has a design option been selected? I know there were several presented in the past but I haven't been following it this year. We do need to be able to go North from East and West bound 36th. I seem to recall that at one time there would not be ability to travel north from 26th. Also how will this intersection design affect the Best Western property (which by now may be Muni property) and the office buildings between Seward Highway and LaTouche? Things to think about.

21 days ago



AK DOT&PF (Alaska Department of Transportation and Public Facilities) (Alaska Department of Transportation and Public Facilities) A design option has not yet been selected for the intersection. The Midtown Congestion Relief Planning and Environmental Linkages Study recommended two interchange concepts for this location: a loop ramp concept and a median u-turn concept. Early work is underway to explore a broad range of interchange alternatives and a preferred alternative has not yet been identified. We are planning further public involvement, including public open house meetings, in 2021 to present the range of interchange alternatives and receive feedback. We have added you to our mailing list so you will receive notice of the project's progress and upcoming meetings.

The impacts of the intersection design on the Best Western Golden Lion property and office buildings between Seward Highway and LaTouche Street are yet to be confirmed and will be a key element for consideration as we explore alternatives for the new interchange. Our early analysis suggests some small strip right-of-way acquisitions may be required around the Seward Highway: 36th Avenue intersection.

16 days ago

What questions or concerns do you have about the project?



Anne Brooks Has the interchange design been determined? What is the environmental process and will it truly be shortened by the PEL step?

21 days ago



AK DOT&PF (Alaska Department of Transportation and Public Facilities) (Alaska Department of Transportation and Public Facilities) The PEL supported an early review of environmental resources in the corridor, developed a draft purpose and need for the project, and identified two potential interchange concept alternatives at 36th Avenue. This work done as part of the PEL study will likely shorten and simplify the environmental process. DOT&PF is currently seeking a Class of Action determination for the 36th Avenue Interchange project. The project team is planning additional public involvement in 2021 to advance design and determine the preferred alternative.

15 days ago



lhajduk@gmail.com There were MANY challenges and opportunities for this intersection identified in the DOT's "Midtown Congestion Relief" public input process. I'd like to know that this input is incorporated for this project.

21 days ago



AK DOT&PF (Alaska Department of Transportation and Public Facilities) (Alaska Department of Transportation and Public Facilities) Public input gathered as part of the Midtown Congestion Relief Planning and Environmental Linkages Study will inform the design of the Seward Highway: 36th Avenue interchange. We plan to continue the dialogue started through the PEL Study process and captured through resolutions from the Anchorage Assembly and Community Councils, and will continue public involvement activities as the preliminary design and environmental process moves forward. We have added you to our mailing list so you will receive notice of the project's progress and upcoming meetings.

16 days ago



When is this project expected to be completed by?

22 days ago



AK DOT&PF (Alaska Department of Transportation and Public Facilities) (Alaska Department of Transportation and Public Facilities) The project completion date is still to be determined. We are currently embarking on preliminary design and environmental analysis, and the project is yet to be incorporated into the Statewide Transportation Improvement Program (STIP), which will enable Federal funding to be allocated for further design and construction activities. We recommend you join the mailing list for the project, and we can keep in touch with you to provide updates as the project progresses.

16 days ago

Would you like to join the project mailing list? If yes, please let us know your name, mailing address and email address.



Craig Wood Yes I'd like to join the project mailing list.

21 days ago



lhajduk@gmail.com Yes, Lindsey Hajduk lhajduk@gmail.com

21 days ago



Mark Erickson Mark Erickson 1332 Matterhorn Way Anchorage, AK 99508 mark50eric@gmail.com

21 days ago

February 18, 2021 Meeting – 7pm to 9:00pm
Airport Heights Community Council – Held via ZOOM

ZOOM meeting login info on zoom.us or zoom app.

Join Zoom Meeting

<https://zoom.us/j/92937817203?pwd=MFJsQUpvbGNYWmtYVU1OVGhzODRjZz09>

Meeting ID: 929 3781 7203

Passcode: 215692

Dial by your location

+1 253 215 8782 or +1 669 900 9128

7:00 pm Call to Order, establish quorum, introductions (with each party saying where they live)

7:05 pm Approve Agenda & approve January Meeting Minutes

7:10 pm Legislative Reports

7:30 pm 36th Avenue & New Seward Update DOWL

7:40 pm FCC Report

7:50 pm President's Report

8.00 pm Assembly/School Board/ PTA Reports

8:30 pm AWWU water project – Russ Porter

8:45 pm Community Concerns and Announcements

Next Meeting: March 18, 2021 – - most likely via ZOOM.

From: [Sean Baski](#)
To: [Shannon McCarthy](#); [Reese, Jill \(DOT\)](#); [Linnell, John R \(DOT\)](#); [Rotkis, Diana L \(DOT\)](#); [david.post](#); [Gardner, Shawn C \(DOT\)](#); [Starzec, James A \(DOT\)](#); [Jim Amundsen](#)
Cc: [Zimmer, Heidi \(DOT\)](#); [Farr, John D \(DOT\)](#)
Subject: [EXT] 36th Ave - Airport Heights Community Council 2/18/21
Date: Friday, February 19, 2021 8:00:29 AM

WARNING: *External Sender - use caution when clicking links and opening attachments.*

Presented at the Airport Heights Community Council meeting last night. Others on the team covered Campbell Park Community Council, that debrief forthcoming.

Questions came about our environmental document and why it wasn't expected to be something larger than a CE, otherwise no issues. More notes follow:

- Notable attendees were: Senator Begich, Representative Tarr, Assemblywoman Zaletel, and School Board Member Bellamy.
- Prior to the meeting kicking off AHCC President and Assemblywoman Zaletel had a sidebar conversation that referenced the 36th Interchange project. Carolyn Ramsey asked Meg Zaletel if she would like to do her update early on the agenda so she would be free to go to the Campbell Park Community Council and Meg responded with: "Well they have Felix and I on the agenda after a discussion of the 36th Avenue and Seward Highway project so I think we won't be up until way after 8:00."
- I gave a brief overview of the project and walked through the project website, emphasizing the unknowns of when construction will happen and the critical need for public feedback. I mentioned the March 16 open house, and the two times of day for it, and let people know we will have more information on the project website soon.
- There were several questions about the environmental process for this project, mostly from Carolyn Ramsey. The tone from participants asking questions was curious and somewhat defensive. Participants wanted to know what type of environmental document this project will be completing. After I told them that a categorical exclusion is expected there was a discussion about the difference between a CE and EA and then participants asked additional questions about what is more appropriate for a project of this size and impact. I gave a high level explanation of how federal funding requirements influence which environmental document is selected, as well as the unique outcomes of the PEL process as related to the environmental process. As a comparison I also referenced another of the MCR PEL recommended projects, Benson Blvd North, how it is anticipated to be an EA due to the size of its impact. As a wrap up I offered to connect interested participants with members of the environmental team for a more detailed conversation about this part of the project. Carolyn Ramsey said farther down the road that would be a welcome conversation.
- Zoom Chat:

- Meg Zaletel in the Zoom chat: “Projects may be designated as CEs upon the submission of documentation which demonstrates that the specific conditions or criteria for those CEs are satisfied and that significant environmental impacts will not result.”
- Another person responded to Meg Zaletel’s message with a question suggesting a lack of trust: “Are they splitting the project to avoid EA?”
- I responded to them and said this was not the case and briefly covered PEL’s and their purpose.

--

Sean Baski, P.E., Project Manager

State of Alaska DOT&PF, Central Region Highway Design Section

P.O. Box 196900, Anchorage, Alaska 99519-6900

Phone 907.269.0547 | Fax 907.248.1573

Keep Alaska Moving through service and infrastructure.

Airport Heights Community Council – Held via ZOOM
April 15, 2021 Meeting – 7pm to 9:00pm

ZOOM meeting login info on zoom.us or zoom app.

Join Zoom Meeting

<https://zoom.us/j/92937817203?pwd=MFJsQUpvbGNYWmtYVU1OVGhzODRjZz09>

Meeting ID: 929 3781 7203

Passcode: 215692

Dial by your location

+1 253 215 8782 or +1 669 900 9128

7:00 pm Call to Order, establish quorum, introductions (with each party saying where they live)

7:05 pm Approve Agenda & approve February and March Meeting Minutes

7:10 pm Assembly/Legislative/School Board Reports

7:30 pm PTA Report and Resolution introduction
Principle Web update

7:40 pm Mayday tree smackdown in Tikishla and along the trails

8: 00 pm 36th and the New Seward Interchange Resolution

8:15 pm CIP List for introduction, to be finalized at the May meeting

8:25 pm Erin Whitney Chugach Board Candidate

8:35 pm Presidents Report

8:40 pm FCC Report

8:50 Community Concerns

Last meeting before summer break: May20, 2021 – via ZOOM.

**AIRPORT HEIGHTS COMMUNITY COUNCIL
RESOLUTION 2021-2**

**A RESOLUTION OF THE AIRPORT HEIGHTS COMMUNITY COUNCIL REGARDING
THE SEWARD HIGHWAY: 36th INTERCHANGE PROJECT**

WHEREAS, Airport Heights Community Council (AHCC) was closely involved with the recent Alaska Department of Transportation and Public Facilities (DOT&PF), Midtown Congestion Relief Planning and Environmental Linkages (PEL) study;

WHEREAS, DOT&PF and its consultant DOWL are proceeding with preliminary design and environmental permitting of the first PEL project to move forward, the Seward Highway 36th Interchange (hereafter “project”);

WHEREAS, on March 16, 2021, the project team held its first public meeting; presented four highway alternatives for the interchange; stated that they are evaluating whether the Seward Highway should be elevated above 36th or depressed below 36th, with 36th remaining approximately at grade for all alternatives, and requested feedback on all project issues;

WHEREAS, the project was based on overly optimistic population growth of Anchorage, however the Anchorage population continues has continued to decrease since its high in 2013;

WHEREAS, transportation as a whole is being reimagined due to the COVID-19 pandemic; many offices have closed, relinquished their office space and switched to a permanent work from home model including some State of Alaska offices;

WHEREAS, the AHCC’s January 16, 2020 Resolution 2020-1 regarding the PEL supported a future depressed freeway through Midtown, AHCC opposed an elevated freeway, advocated for the thoughtful design of non-vehicular components of future PEL projects, and advocated for a 35 mph speed limit on frontage roads;

WHEREAS, in the March 16 open house, the project team stated that all highway alternatives for the 36th interchange, whether the highway passes over or under 36th, would be designed to allow future extension of the highway under Benson, Northern Lights, and Fireweed;

WHEREAS, in the March 16 open house, the project team stated that the DOT&PF had not decided whether an Environmental Assessment was needed;

WHEREAS, the no-build and at-grade alternatives will not be evaluated unless an Environmental Assessment is performed;

WHEREAS, an Environmental Impact Statement (EIS) for the complete MCR project must be completed; a section by section approach does not address the project impacts or ensure environmental justice for the entire project as a whole;

NOW THEREFORE, BE IT RESOLVED that the Airport Heights Community Council continues to support:

1. a future depressed freeway through the council area, passing under 36th, as well as under Benson, Northern Lights, and Fireweed in the future;
2. non-motorized facilities along and across the highway corridor that are accessible, safe, buffered, well-lit, usable year-round, and designed to minimize the winter maintenance effort;
3. a maximum speed limit of 35 mph on frontage roads and 45 mph on the highway through the entire midtown area;
4. an Environmental Assessment for the 36th Interchange Project;
5. an Environmental Impact Statement for the entire MCR project.

NOTE: this resolution is intended to reinforce and confirm the relevance of Resolution 2020-01 as it pertains to the project at hand. Resolution 2020-01 requests several accommodations that will be pertinent to other future PEL projects.

THIS RESOLUTION WAS APPROVED by the Airport Heights Community Council on April 15, 2021 by a vote of: FOR ____ AGAINST __, ABSTAIN ____.

Carolyn Ramsey, President

From: [Katie Conway](#)
To: [Sean Baski](#)
Cc: [Renee Whitesell](#); [Steven Noble](#); [Farr, John D \(DOT\)](#)
Subject: Airport Heights Community Council meeting notes
Date: Thursday, April 15, 2021 8:40:00 PM
Attachments: [AHCC Resolution 2021-1.docx](#)

Sean,

Notes from tonight's Airport Heights CC meeting for your consideration/use:

The 36th Avenue Interchange was discussed at tonight's Airport Heights Community Council meeting. Approximately 20 participants included the following notable attendees: Sen. Tom Begich, Rep. Geran Tarr, Assembly members Meg Zaletel and Felix Rivera, and Anchorage School Board member Margo Bellamy.

A resolution was introduced, discussed, and passed regarding the 36th Avenue Interchange project. A copy of the draft resolution is attached. Discussion of the resolution suggests the perception by the resolution authors that the PEL recommended projects should be viewed as one comprehensive project and an EIS done for the aggregate rather than separating out projects for implementation.

One community council member asked what the resolution says in plain language. CC President Carolyn Ramsey summarized the main points of the resolution as follows:

1. The freeway needs to be depressed at 36th
2. There needs to be a focus on nonmotorized facilities on and across the highway
3. The max speed limit of 35 mph for local streets
4. The need for a full EIS to be done on the project
5. The need for a comprehensive EIS for the full suite of MCR projects

Sean reminded the community council of the DOT&PF's and project team's availability to answer questions or discuss the project in more detail, including what is and is not allowed under the federal process, and referenced the project website as a source of information.

Assembly member Meg Zaletel asked if there's any indication of whether new federal infrastructure or FAST Act funding will be used for the project, and how that might impact the project timeline.

Senator Begich stated that he is working independently from DOT&PF to prioritize the Highway to Highway project, Port of Alaska projects, and elimination of KGB easements as higher priorities than the 36th Interchange for new federal funding through the Biden Administration's infrastructure plan.

Geran Tarr voted in favor of the resolution. The final count was nine yes, four abstention, zero no.

Katie Conway
Public Involvement Manager

DOWL

(907) 562-2000 | office
(907) 865-1289 | direct
4041 B Street
Anchorage, AK 99503

www.dowl.com

AHCC Agenda September 16, 2021

The September meeting will be held via Zoom (see link below) All attendees will be asked to provide their full name and street they live on for the record.

Join Zoom Meeting

<https://zoom.us/j/92937817203?pwd=MFJsQUpvbGNYWmtYVU1OVGhzODRjZz09>

Meeting ID: 929 3781 7203

Passcode: 215692

Dial by your location

+1 253 215 8782 or +1 669 900 9128

7:00 pm Call to Order, establish quorum, introductions

7:05 pm Approve September 2021 Agenda & Approve May 2021 Minutes

7:10 pm Assembly/Legislative/School Board Reports

7:45 pm PTA Report

7:50 pm Nominating Committee / Amber, chair

7:55 pm Title 21 Update, Muni Planning, Tom Davis

8:15 pm Old Seward and 36th / MCR update Sean DOT & DOWL

8:30 pm Presidents Report: Update on FS3, Tikishla Park,

8:40 pm FCC Report

8:50 pm Community Concerns

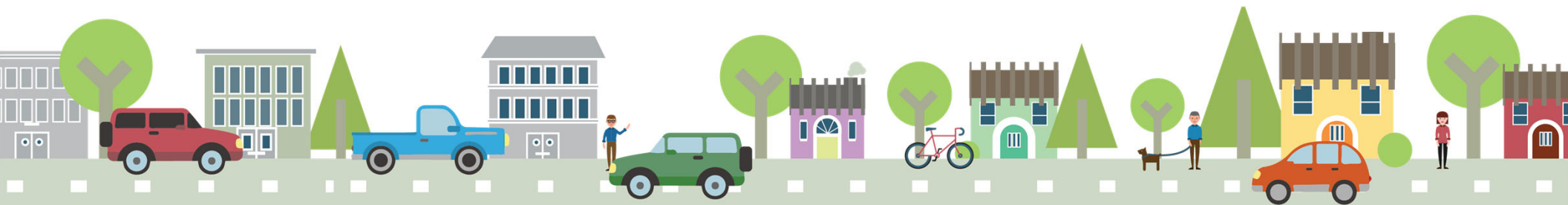
09.16.2021



36th avenue

INTERCHANGE

Seward Highway: 36th Avenue Interchange Airport Heights Community Council Meeting



PROJECT HISTORY AND BACKGROUND

- Midtown Congestion Relief Planning and Environmental Linkages (PEL) Study concluded in 2020 and recommended 5 projects
- 36th Avenue Interchange is the first project moving forward
- Interchange was studied in 2013-2015 but did not progress
- This is a fresh design and environmental permitting effort



WHY WAS THE 36TH INTERCHANGE SELECTED? — — — — —

- PEL Study recommended at-grade intersection improvements to construct frontage roads and implement two-phase signals
- Improved traffic flow on Seward Highway in short-term (modelled through 2028), but still had delays and safety issues associated with at-grade crossings (albeit much less than current)
- Full grade-separated interchange still needed to fully improve intersection past modelled 2028 volumes, improve safety, reduce delay above LOS F
- Concern that full interchange would be needed immediately on completion of at-grade project, creating several years of consecutive construction and rework of only recently completed investments
- Statewide Planning chose to move to full interchange immediately to overcome this concern and responsibly program constrained funds in STIP



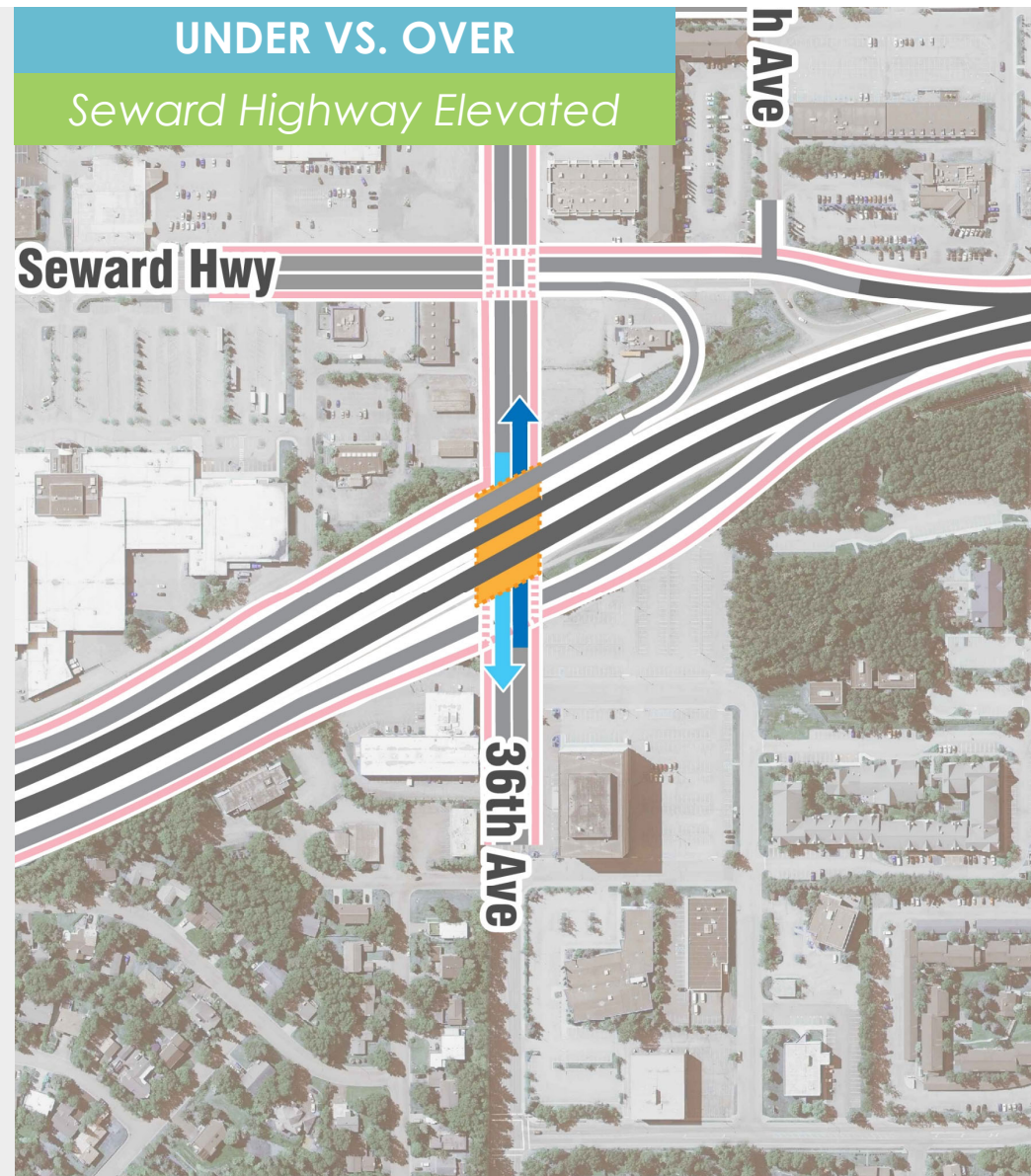
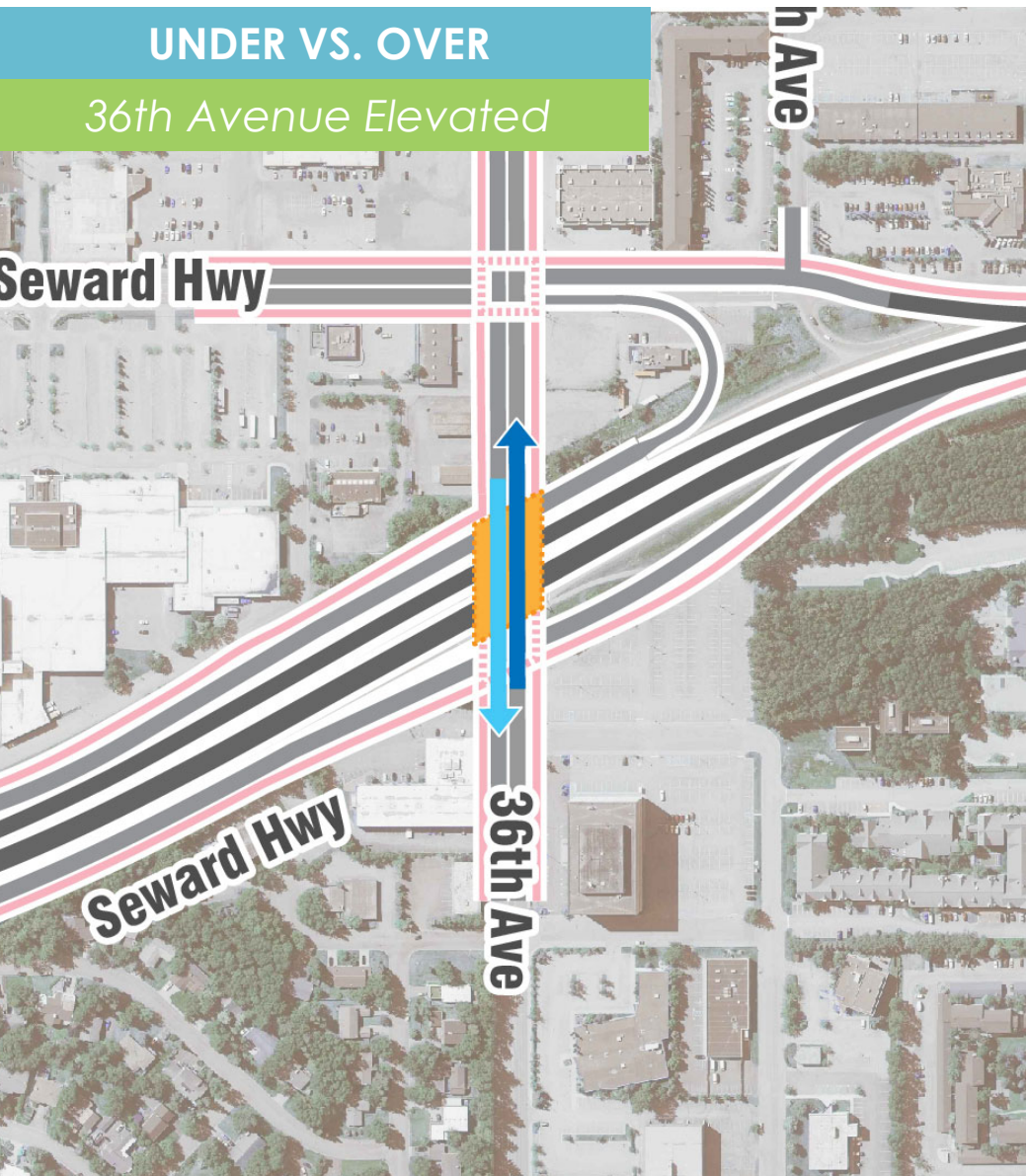
PROJECT STATUS — — — — —

- **Working on most promising alternatives:** Two loop ramp concepts
- **Refining vertical and horizontal geometry:** Minimizing vertical impacts
- **Initiating noise study:** 2022
- **Open House #2:** scheduled for late 2021

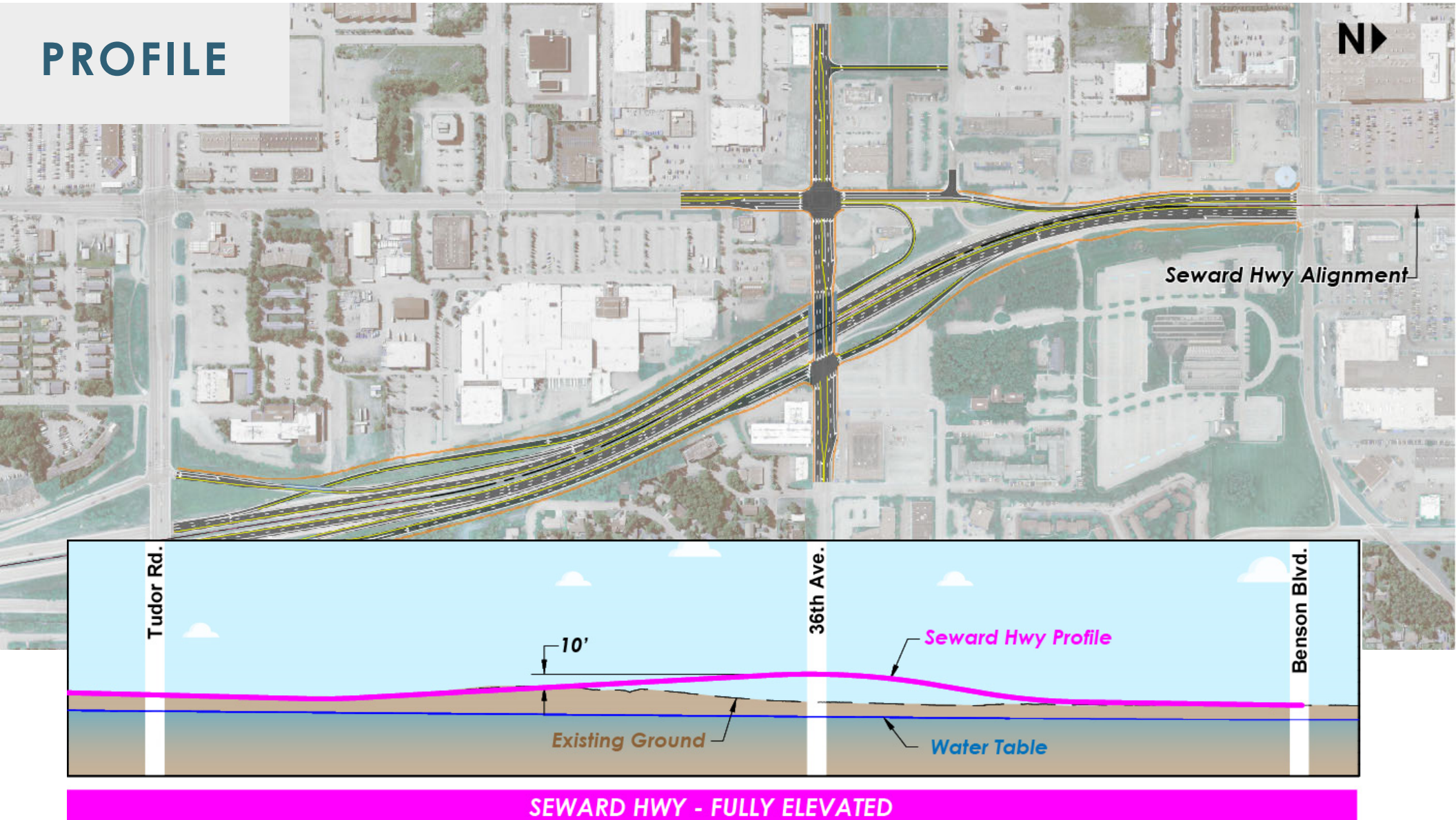
Everything at 36th Avenue is being designed to be forward compatible with a depressed freeway at Benson Boulevard, Northern Lights Boulevard, and Fireweed Lane, as recommended in the

PEL Study

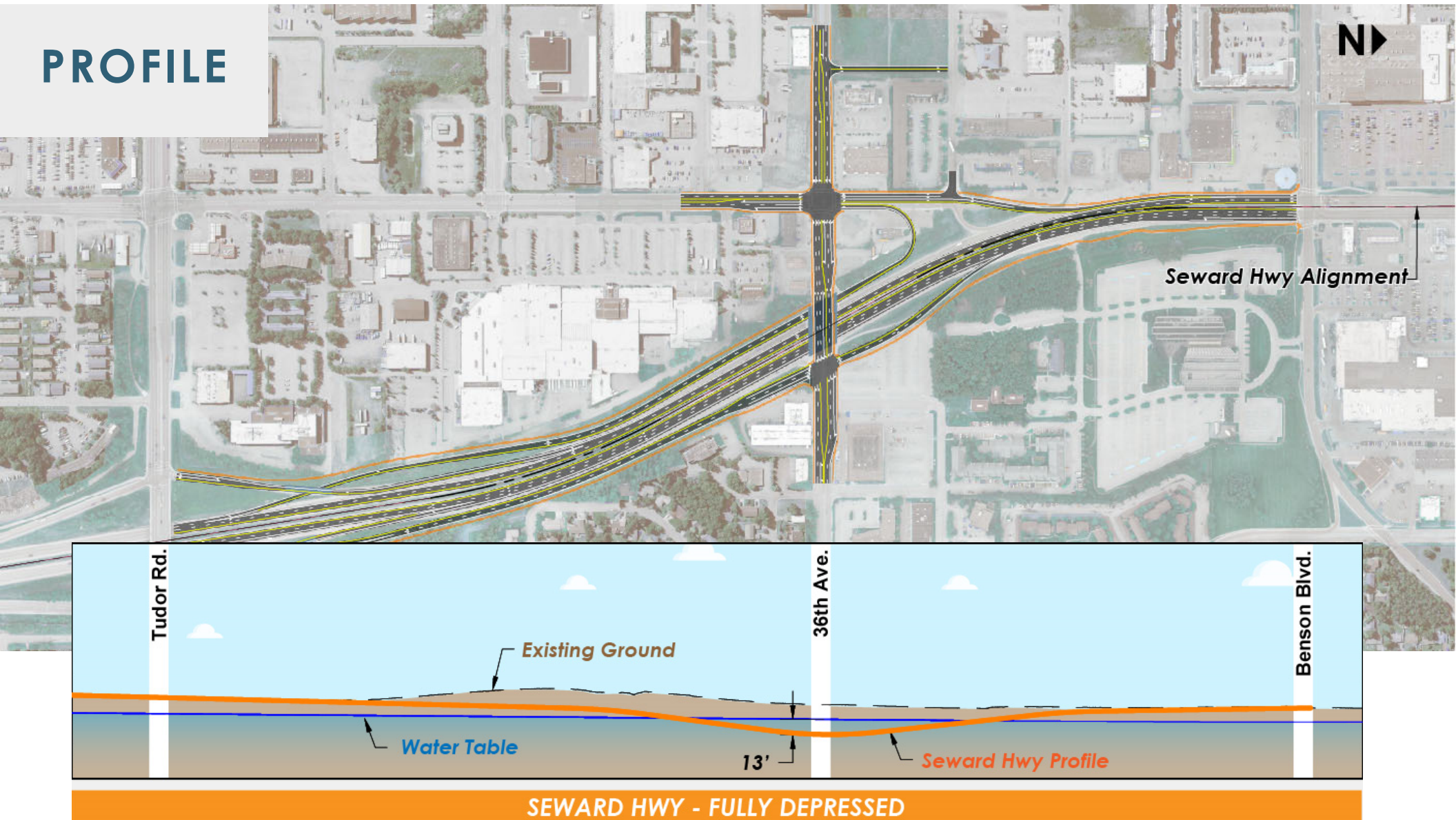




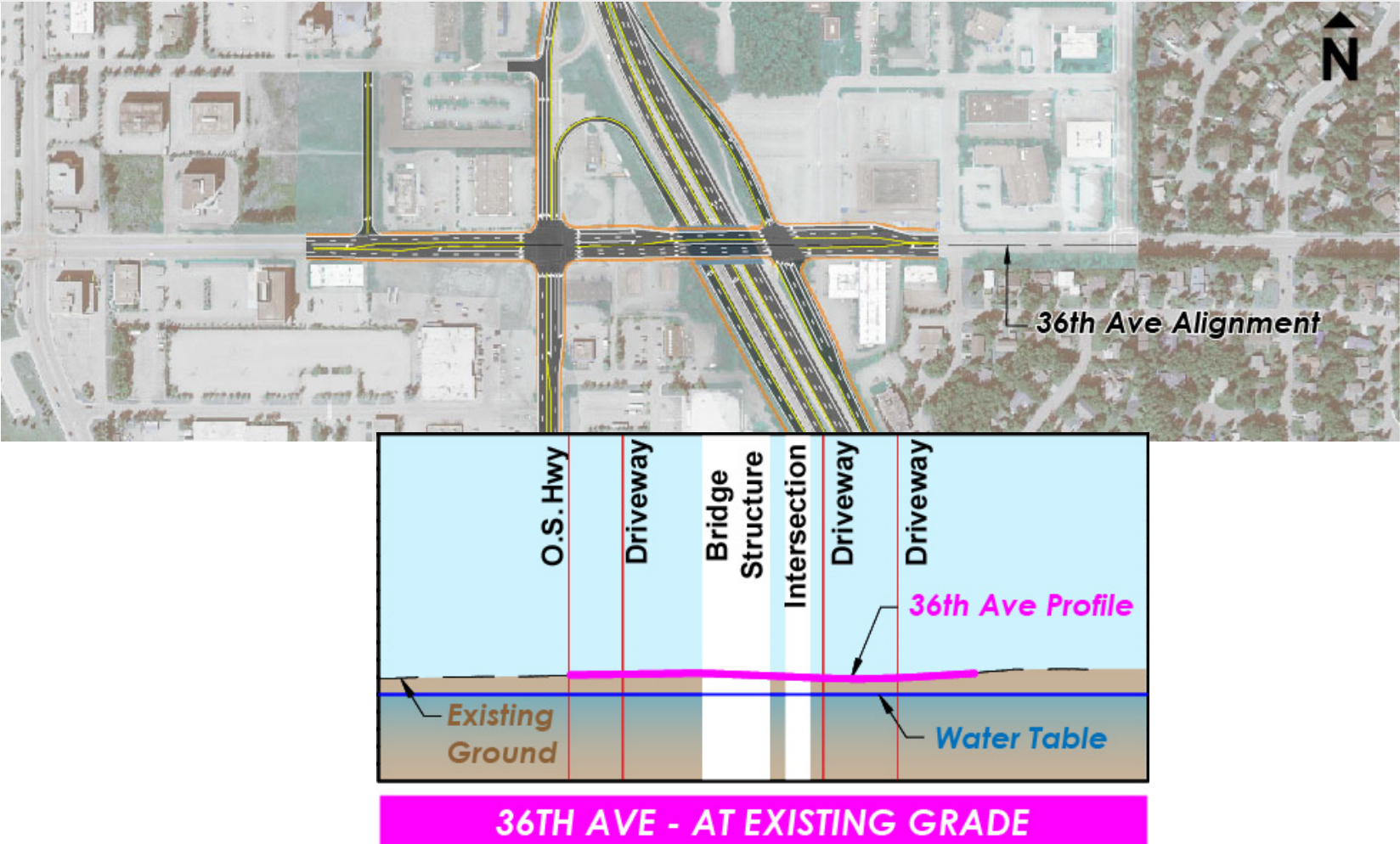
PROFILE



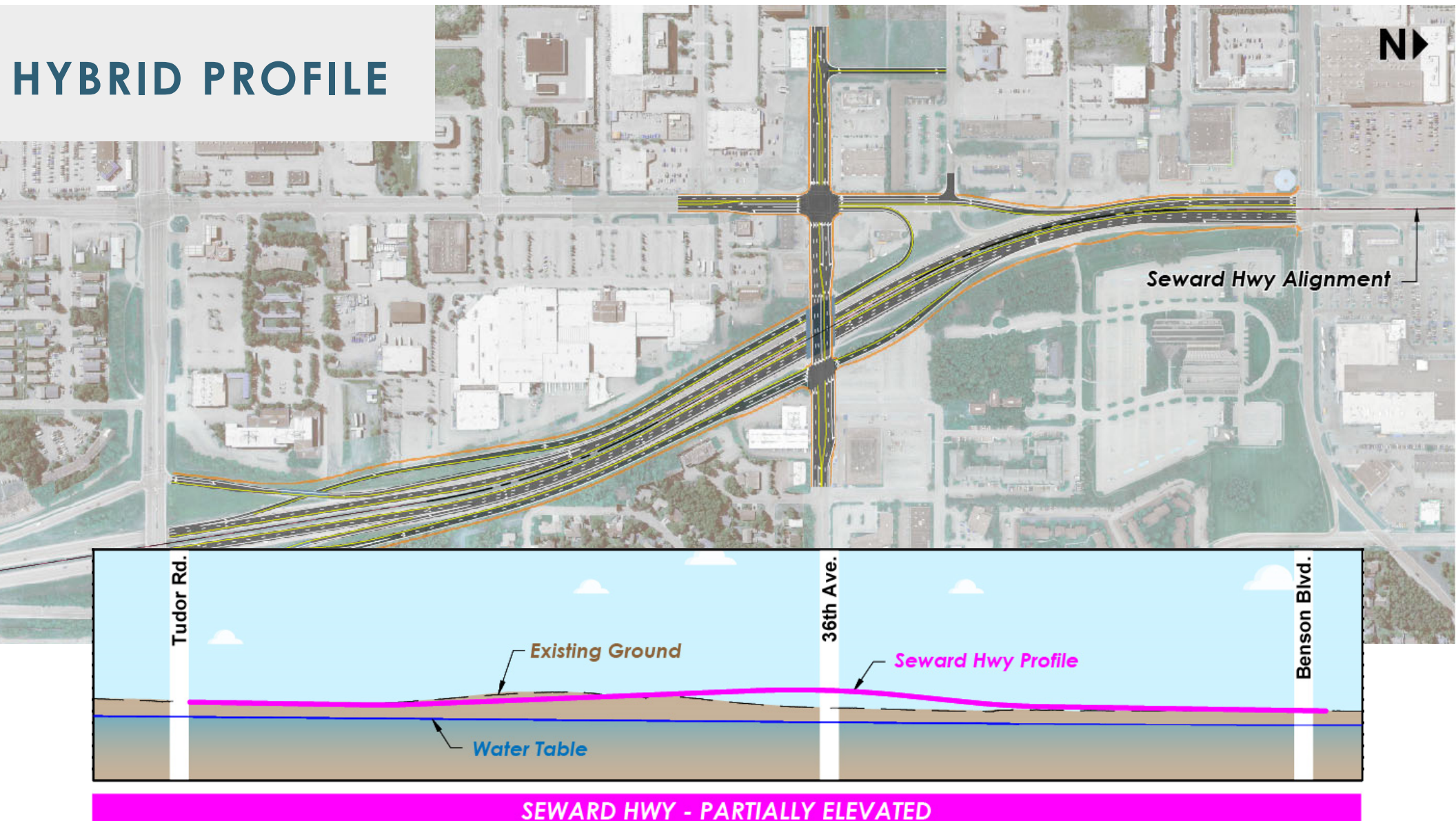
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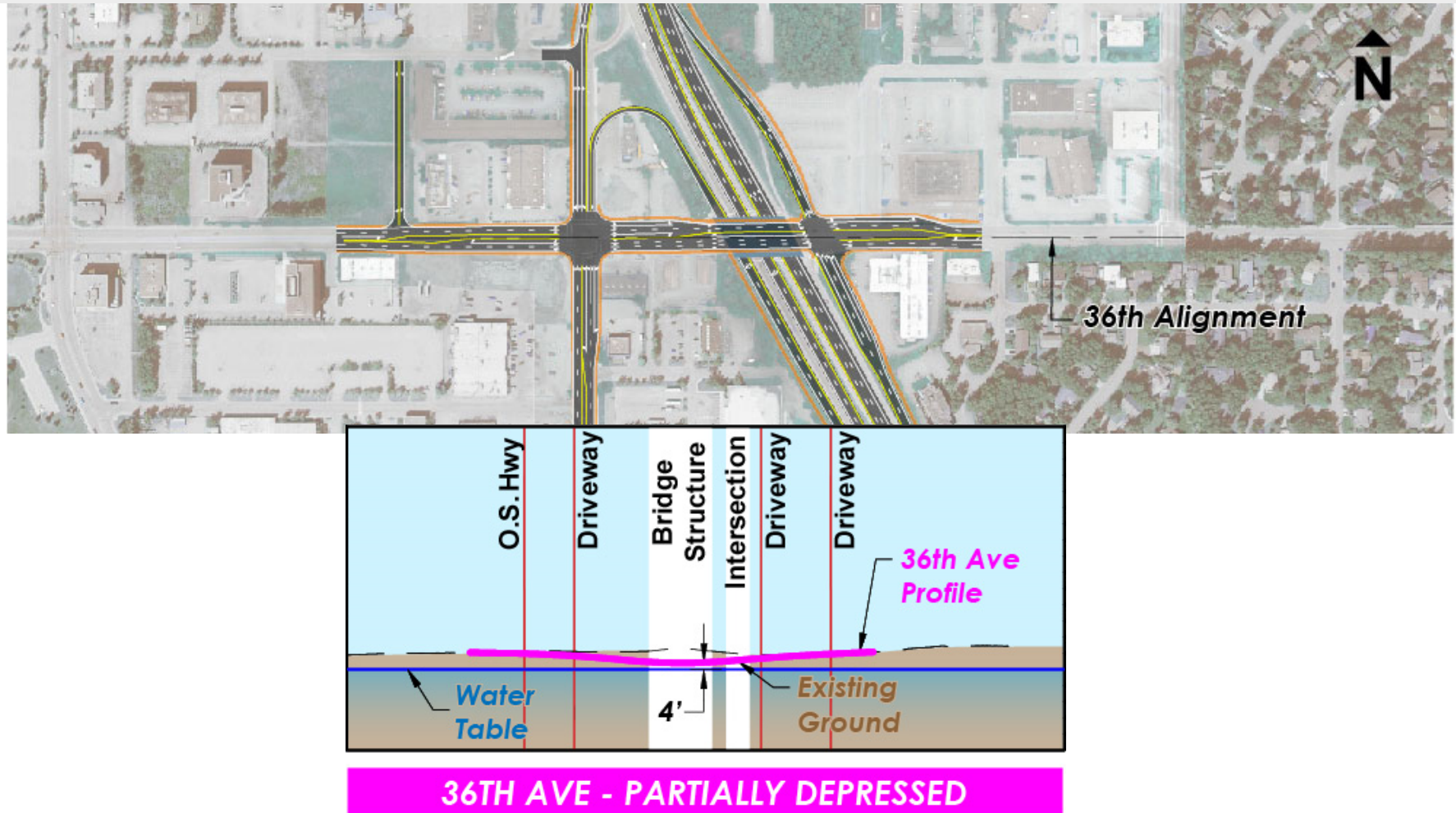
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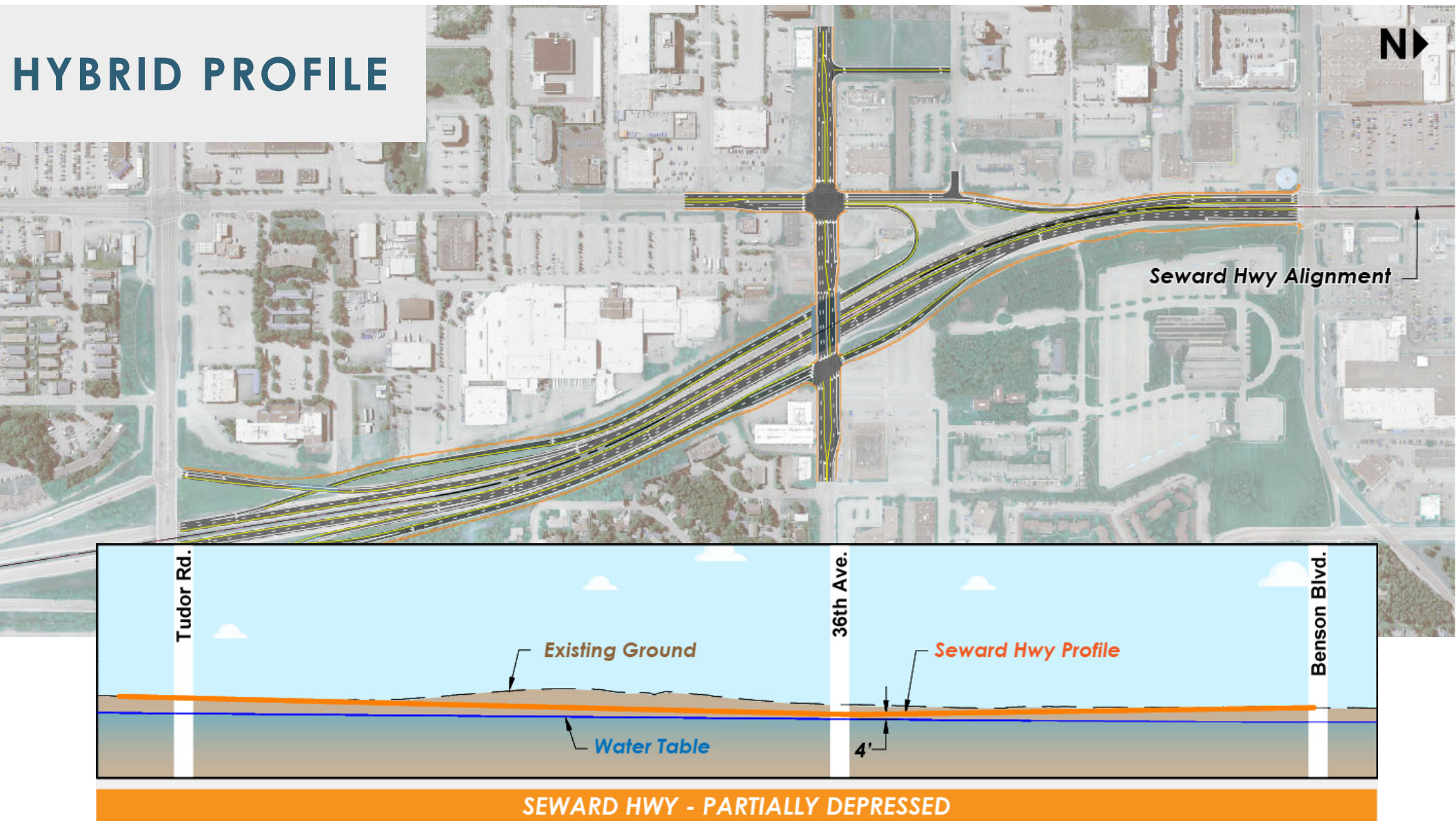
HYBRID PROFILE



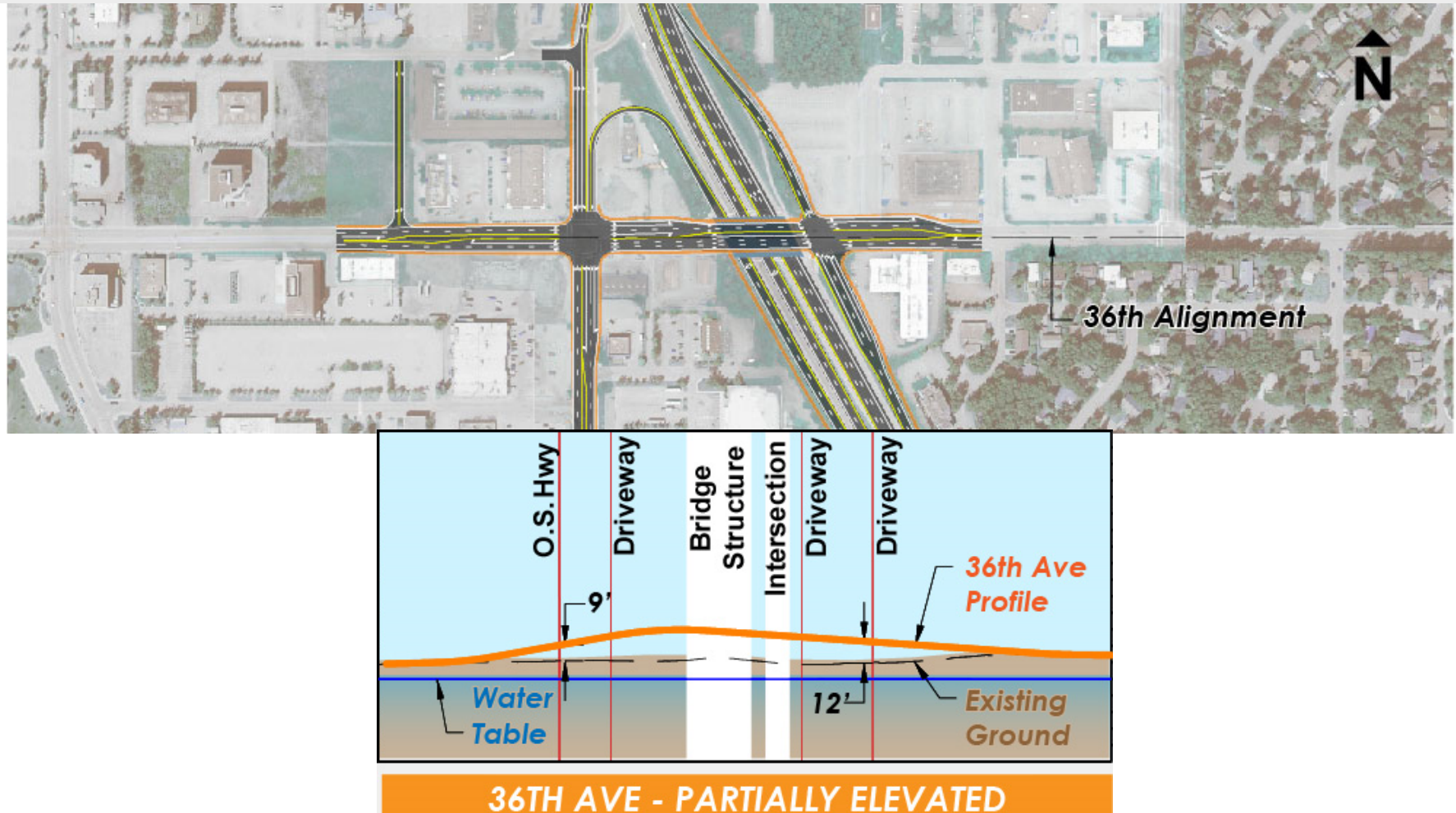
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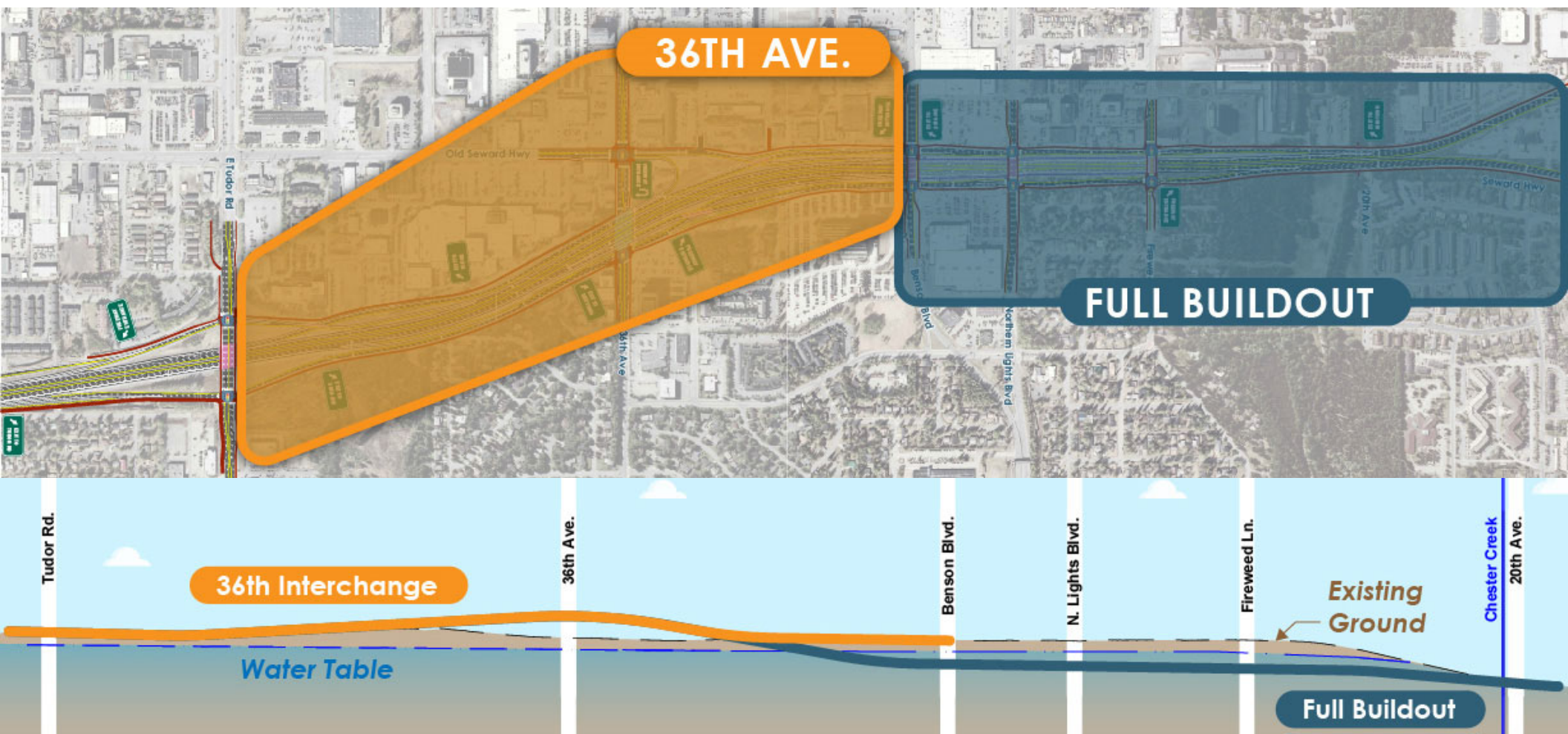
HYBRID PROFILE



HYBRID PROFILE



FULL BUILDOUT



OBSERVATIONS

- **Water Table:** high water table will require intersection to be pumped continuously (both groundwater and stormwater). Pumping volumes are significant and dewatering impacts could extend beyond the project area (currently being evaluated)
- **Contamination:** There is contamination on a property near the intersection, which will require treatment prior to discharge
- **Visual:** The entirely elevated highway is approximately 10 feet above the existing ground level between Tudor Road and 36th Avenue. Visual renderings are going to be prepared to show indicative views for upcoming open house
- **Noise:** Noise Study methodology has been developed and is currently under review. There are 7 residential properties that are “first row receivers” (immediately next to the project area)
- **Driveways:** Driveways on 36th Avenue are very close to the intersection, which create challenges with elevating or depressing 36th Avenue to reduce the height of an overpass



SCHEDULE

Task	Date
PEL Study	2017-2020
Public Involvement	Ongoing
Preliminary Environmental & Design	2020 - Ongoing
Final Design/ROW Acquisition	TBD
Construction	TBD





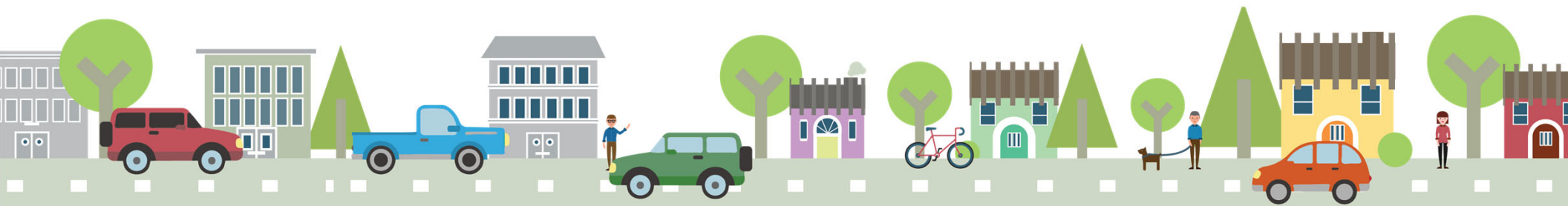
36th avenue INTERCHANGE

Thank You

Questions or Comments?

36thinterchange@dowl.com

www.36thinterchange.com



From: [Baski, Sean M \(DOT\)](#)
To: [Katie Conway](#)
Cc: [Steven Noble](#); [Renee Whitesell](#); [Rachel Steer](#); [Morgan McCammon](#)
Subject: [EXT] RE: Airport Heights Community Council
Date: Monday, September 20, 2021 6:25:39 AM

WARNING: *External Sender - use caution when clicking links and opening attachments.*

Sean Baski and Katie Conway attended the Airport Heights Community council meeting this evening, September 16, 2021. Sean provided an update on the 36th Avenue Interchange project, specifically focusing on early engineering to explore the profile of the interchange.

29 participants attended the meeting. Notable attendees included Representative Geran Tarr, Assembly Members Meg Zaletel and Felix Rivera, School Board Member Carl Jacobs, and staff to Senator Begich.

Questions/comments included:

Paige: Asked if this project is being designed to meet the needs of people traveling across town during rush hour, a short amount of time. Sean responded that the purpose of the project is about improving safety and connectivity. During the MCR PEL we consistently heard that people want better connection across the highway. We also heard that Midtown is the destination, and a lot of traffic crossing the highway is people going to and from Midtown.

Paige: Asked what kinds of traffic calming measures will be implemented. Sean noted that it's unlikely we'd influence the highway speeds. However, on the frontage roads, we're looking to have bike paths and other visual cues to let people know they should slow down.

R. Clark: Asked if there are water table issues with depressed highway options at Benson and Northern Lights. Sean responded that yes, the road would be below the water table there and this has been understood since the MCR PEL. The difference between taking the Seward Highway under Benson and Northern Lights is that from Benson there's a way to design a gravity fed system for water drainage whereas going under at 36th would require the entire system be pumped as the design of the interchange would essentially create a "pool" at 36th Avenue without the ability for the water to drain.

R. Clark: Asked if there will be bike infrastructure included in the design for north-south or east-west travel? Sean noted there will be bicycle facilities east-west along 36th, and north-south along the frontage roads.

C. Ramsey: Asked if snow removal/maintenance would be done on nonmotorized facilities. Sean responded that DOT&PF maintenance is state funded and hard to forecast as there are many variables that determine what is done, where, and when. Wouldn't expect anything to change.

C. Ramsey: Asked about the proposed width of pedestrian facilities. Sean noted that the width depends on a number of things, like right-of-way constraints and where along the roadway you're measuring. It's too early in the design process to be able to give definitive answers. 5-10' is the range.

J. Grover: Asked about the date for the fall open house. Sean replied November, but the specific date is TBD.

C. Ramsey: Commented that a sidewalk or pathway needs to be accessible year round. A pathway that's clear in the summer and is used for snow storage in the winter is not acceptable, especially for people who have trouble getting around. She also questioned the minimum width required by Municipal code for pathways.

Campbell Park Community Council

Agenda

February 18, 2021 | 7:00 p.m.

via Zoom

<https://us02web.zoom.us/j/89083320398?pwd=WDZzSUdUMHRCCdVraU15RUhoYWdpZz09>

- I. Call to Order
- II. Approval of Agenda
- III. Approval of January 21st meeting minutes
- IV. Guest Speakers
 - A. Bill Falsey, candidate for Mayor
 - B. DOT&PF –Seward Highway: 36th Avenue Interchange Project. Renee Whitesell DOWL, with details of open house.
- V. Reports
 - A. Assembly (Felix Rivera / Meg Zaletel)
 - B. Legislature (Senator Elvi Gray-
 - C. Jackson / Rep. Andy Josephson)
 - D. Mayor's Office
 - E. School Board (Margo Bellamy)
 - F. FCC (Kevin Fimon)
 - G. Housing and Homeless Committee (Michael Burke)
 - H. CPCC Chair (Steve Johnson)
Capital Improvement Plan request -
Campbell Creek Trail Crossing at Lake
Otis Parkway
- VI. Unfinished Business
 - A. Resolution 2021 Opposing amendments to MC 21.050.040. (Becky Chambers)
- VII. New Business
- VIII. Next Meeting: March 18, 2021, 7 p.m.
- IX. Adjournment

From: [Sean Baski](#)
To: [Shannon McCarthy](#); [Reese, Jill \(DOT\)](#); [Linnell, John R \(DOT\)](#); [Rotkis, Diana L \(DOT\)](#); [david.post](#); [Gardner, Shawn C \(DOT\)](#); [Starzec, James A \(DOT\)](#); [Jim Amundsen](#)
Cc: [Farr, John D \(DOT\)](#)
Subject: [EXT] 36th Avenue Interchange: Campbell Park CC - 2/18/21
Date: Friday, February 19, 2021 8:17:24 AM

WARNING: External Sender - use caution when clicking links and opening attachments.

Notes follow:

From: Renee Whitesell <rwhitesell@dowl.com>
Sent: Friday, February 19, 2021 8:12 AM
To: Baski, Sean M (DOT) <sean.baski@alaska.gov>
Cc: Farr, John D (DOT) <john.farr@alaska.gov>; snoble <snoble@dowl.com>; Steer, Rachel (DOT Sponsored) <rsteer@dowl.com>; Katie Conway <kconway@dowl.com>
Subject: 36th Avenue Interchange: Campbell Park CC

Sean,

I presented the Seward Highway: 36th Avenue Interchange project at Campbell Park CC last night.

This was a Zoom meeting attended by 19 participants at its peak. Notable attendees included: Sen. Gray-Jackson, Rep. Josephson, and Assembly member Zaletel. In addition, Mayoral nominee Bill Falsey was in attendance. The structure of Campbell Park CC's meeting has presentations first so I was the second speaker following the start of the meeting, and elected representatives were present.

I provided an introduction to the project, using the screen-share function to show participants through the key features of the project website. I shared the project website detail through the chat-box. I also let participants know about the upcoming Open House and how to sign up to receive emailed project updates.

Questions included:

- When can residents expect construction start? Is that part of our current contract? I explained the structure of the current contract and steps prior to construction, which I said was approximately 3-5 years into the future.
- Does this project include the Tudor Interchange? I explained that was a separate project, which should be moving forward in 2021.
- Can we present this project to the Tudor Area Community Council? I confirmed we are on the agenda to present the project at the Tudor Area CC in early March.

Overall – this was a supportive crowd who thanked us for visiting.

Next up is Tudor Area Community Council, on March 4.

Thanks, Renee

Campbell Park Community Council

Agenda

Thursday September 16th, 2021 | 7:00 p.m.

via Zoom

Join Zoom Meeting

<https://us02web.zoom.us/j/85177018959?pwd=aXZsS3RYNkFvUzN4VDF6MW16cThmdz09>

Meeting ID: 851 7701 8959

Passcode: 588570

- I. Call to Order
- II. Approval of Agenda
- III. Approval of August meeting minutes
- IV. Presentation – AK Child & Family (Alexandra Peter)
- V. Reports
 - Assembly (Felix Rivera / Meg Zaletel)
 - Legislature (Senator Elvi Gray-Jackson / Rep. Andy Josephson)
 - Mayor's Office (Portia Noble)
 - School Board (Carl Jacobs)
 - FCC (Kevin Fimon)
 - Housing and Homelessness Committee (Michael Burke)
 - Joint Base Elmendorf
 - CPCC Chair (Steve Johnson)
- VI. Unfinished Business
- VII. New Business
- VIII. Next Meeting: October 21st, 2021, 7:00 p.m.
- IX. Adjournment

09.16.2021



36th avenue

INTERCHANGE

Seward Highway: 36th Avenue Interchange Campbell Park Community Council Meeting



PROJECT HISTORY AND BACKGROUND

- Midtown Congestion Relief Planning and Environmental Linkages (PEL) Study concluded in 2020 and recommended 5 projects
- 36th Avenue Interchange is the first project moving forward
- Interchange was studied in 2013-2015 but did not progress
- This is a fresh design and environmental permitting effort



WHY WAS THE 36TH INTERCHANGE SELECTED? — — — — —

- PEL Study recommended at-grade intersection improvements to construct frontage roads and implement two-phase signals
- Improved traffic flow on Seward Highway in short-term (modelled through 2028), but still had delays and safety issues associated with at-grade crossings (albeit much less than current)
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- Concern that full interchange would be needed immediately on completion of at-grade project, creating several years of consecutive construction and rework of only recently completed investments
- Statewide Planning chose to move to full interchange immediately to overcome this concern and responsibly program constrained funds in STIP



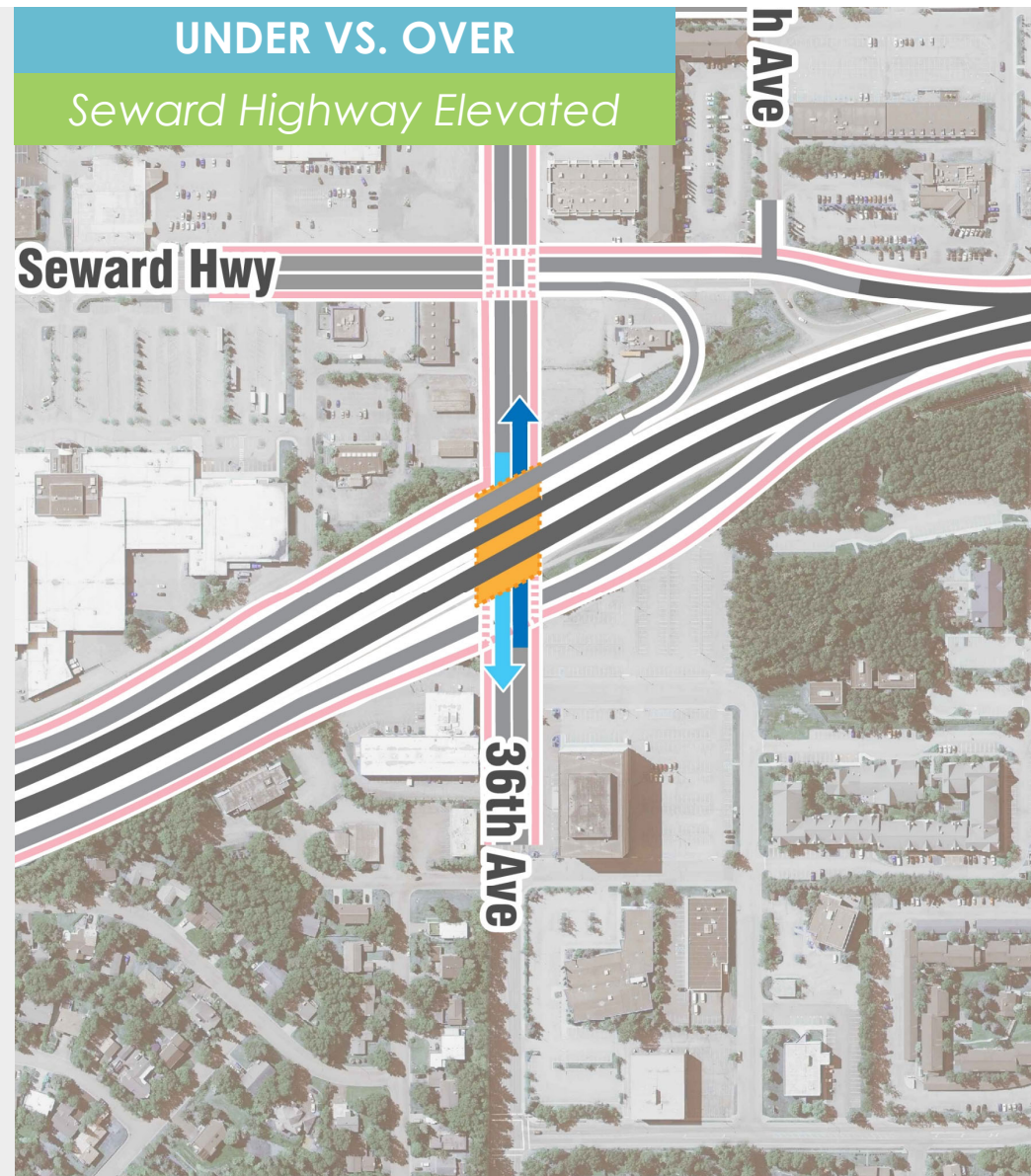
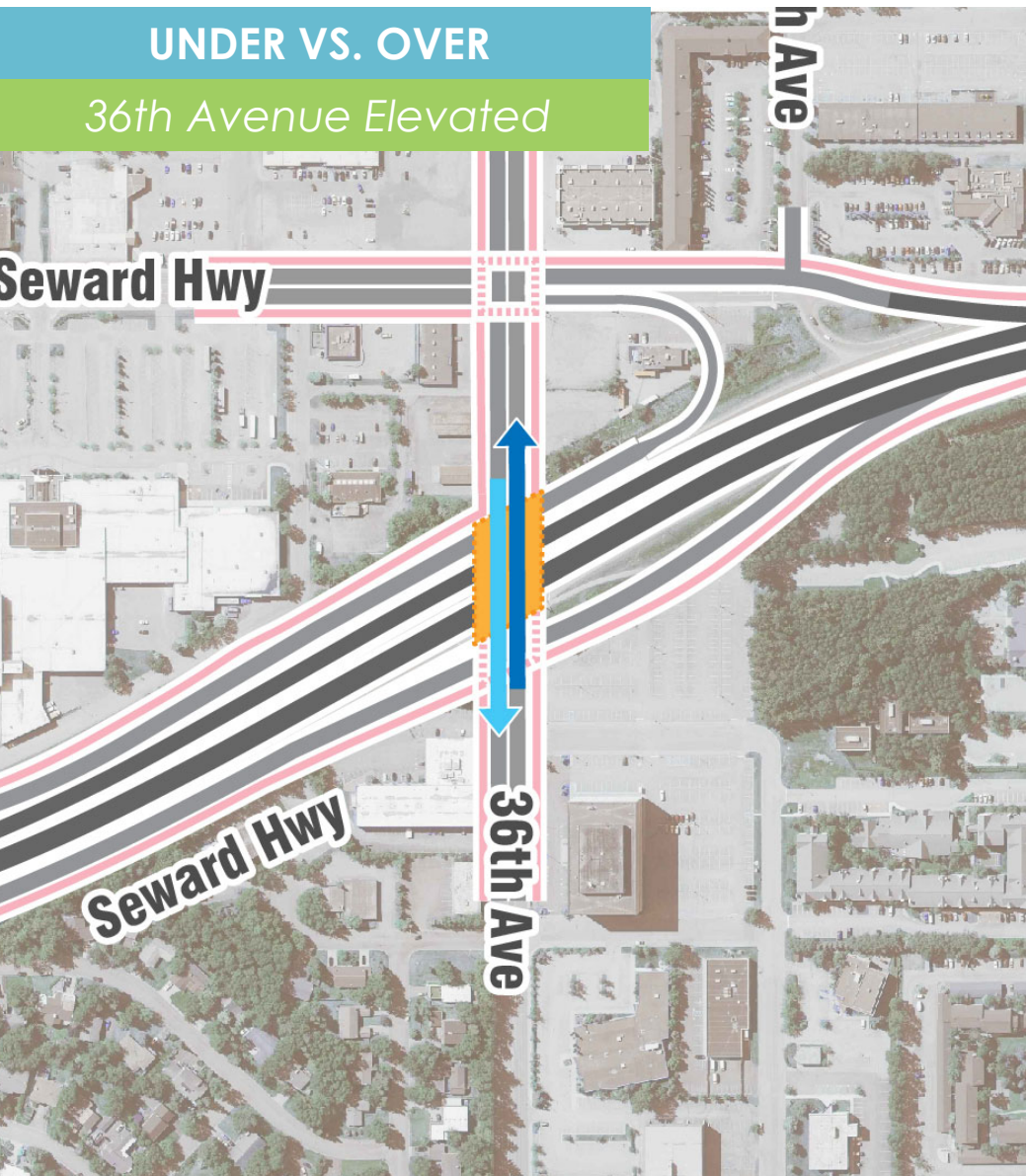
PROJECT STATUS — — — — —

- **Working on most promising alternatives:** Two loop ramp concepts
- **Refining vertical and horizontal geometry:** Minimizing vertical impacts
- **Initiating noise study:** 2022
- **Open House #2:** scheduled for late 2021

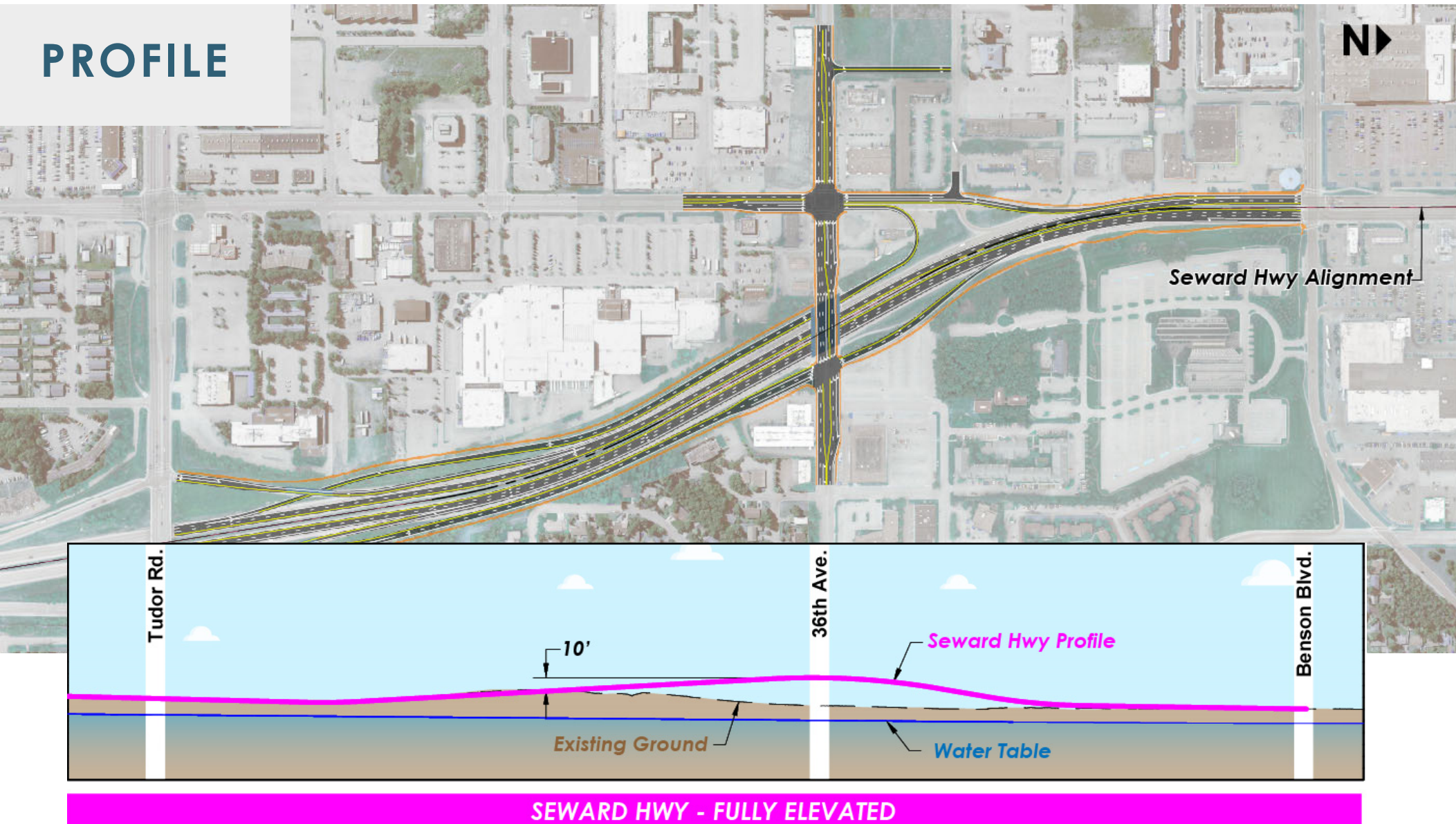
Everything at 36th Avenue is being designed to be forward compatible with a depressed freeway at Benson Boulevard, Northern Lights Boulevard, and Fireweed Lane, as recommended in the

PEL Study

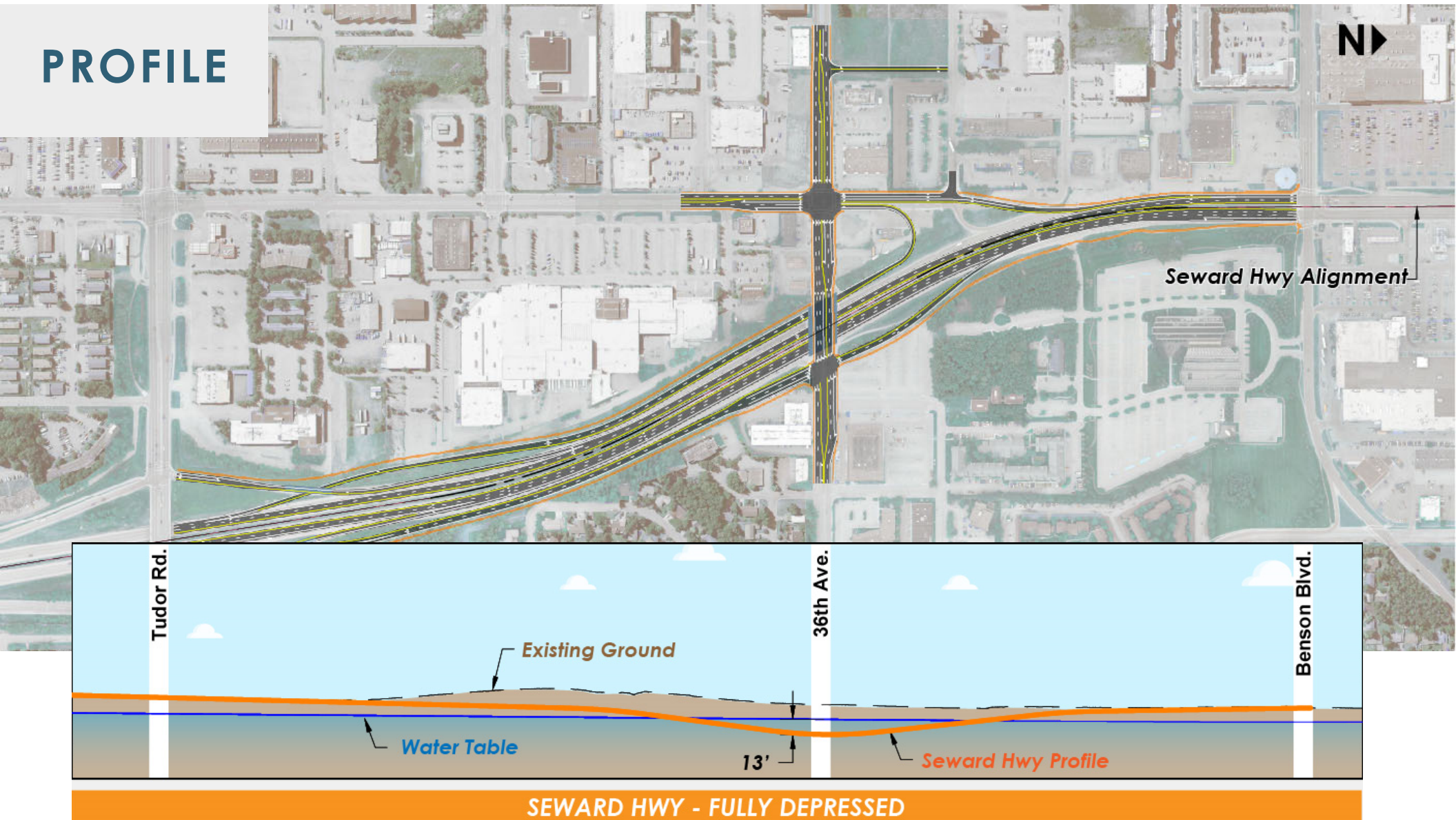




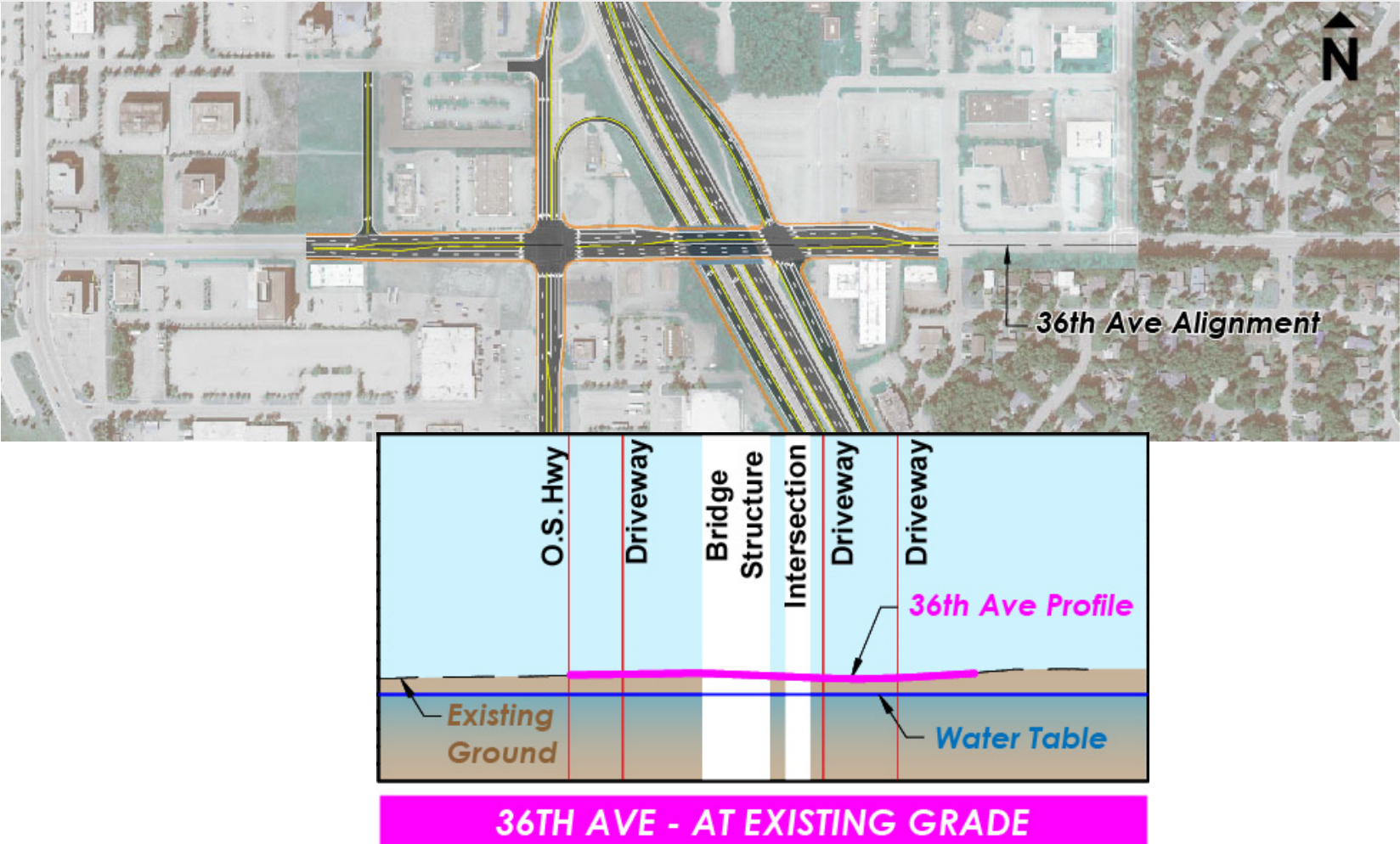
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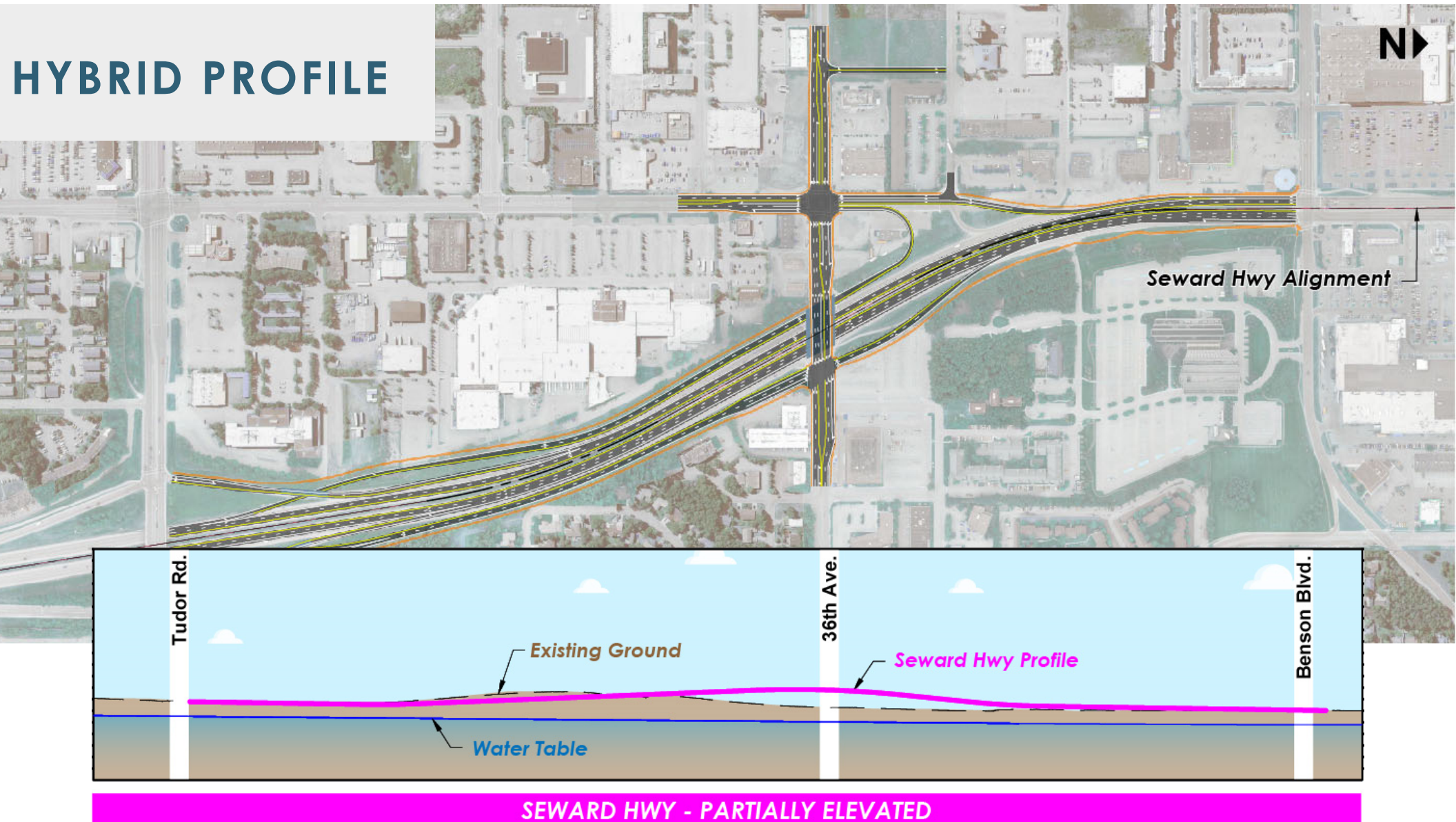
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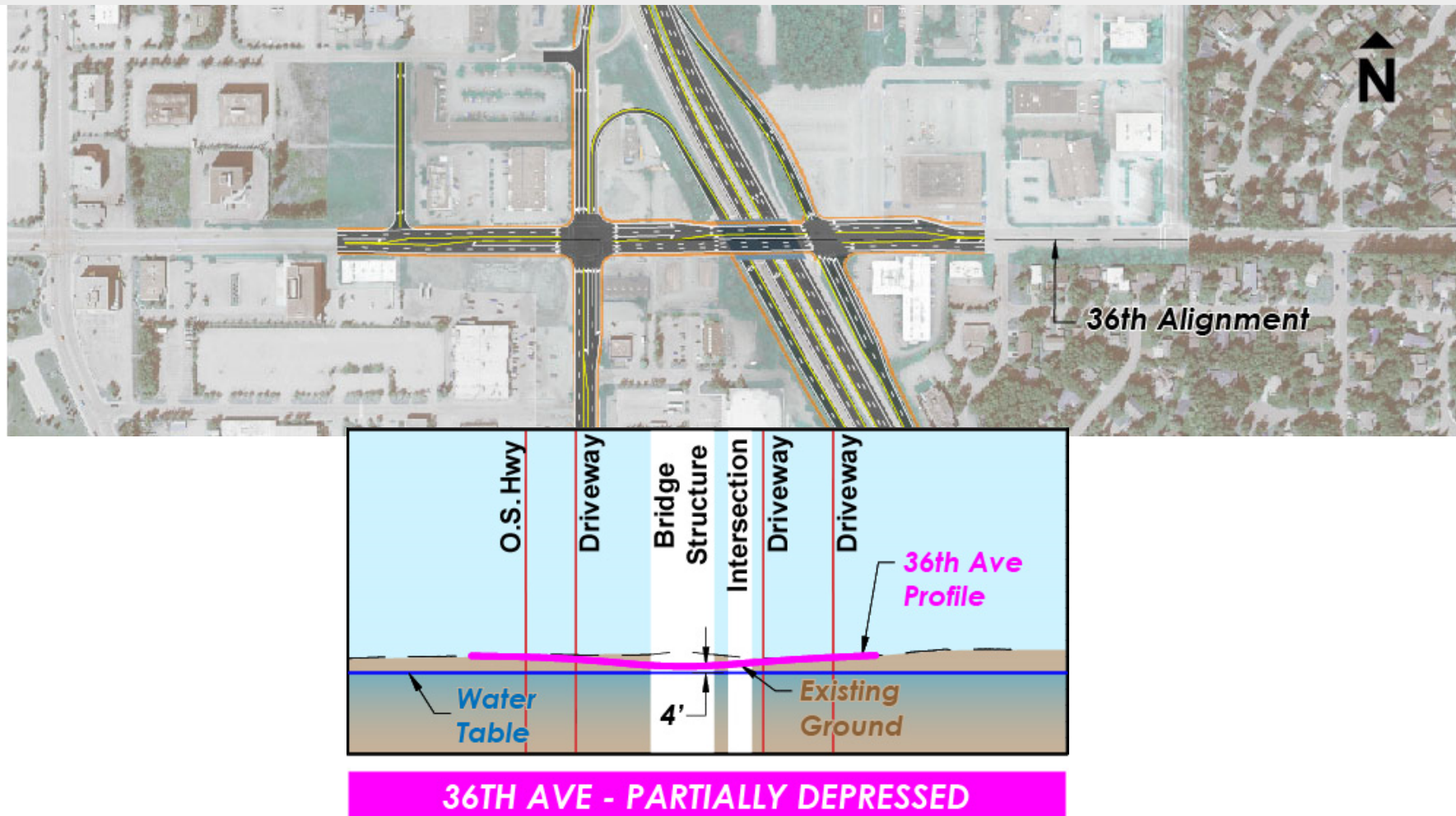
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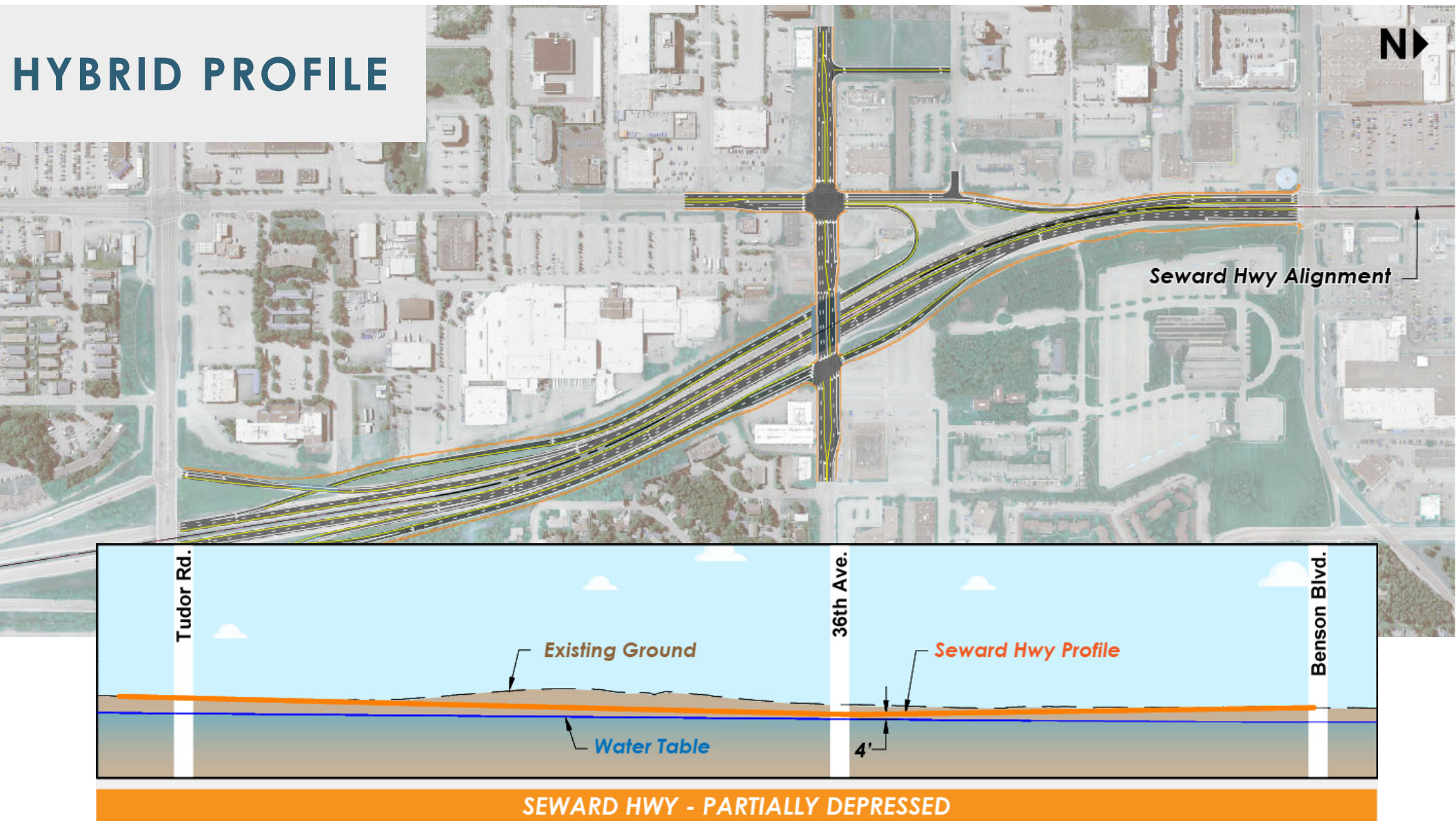
HYBRID PROFILE



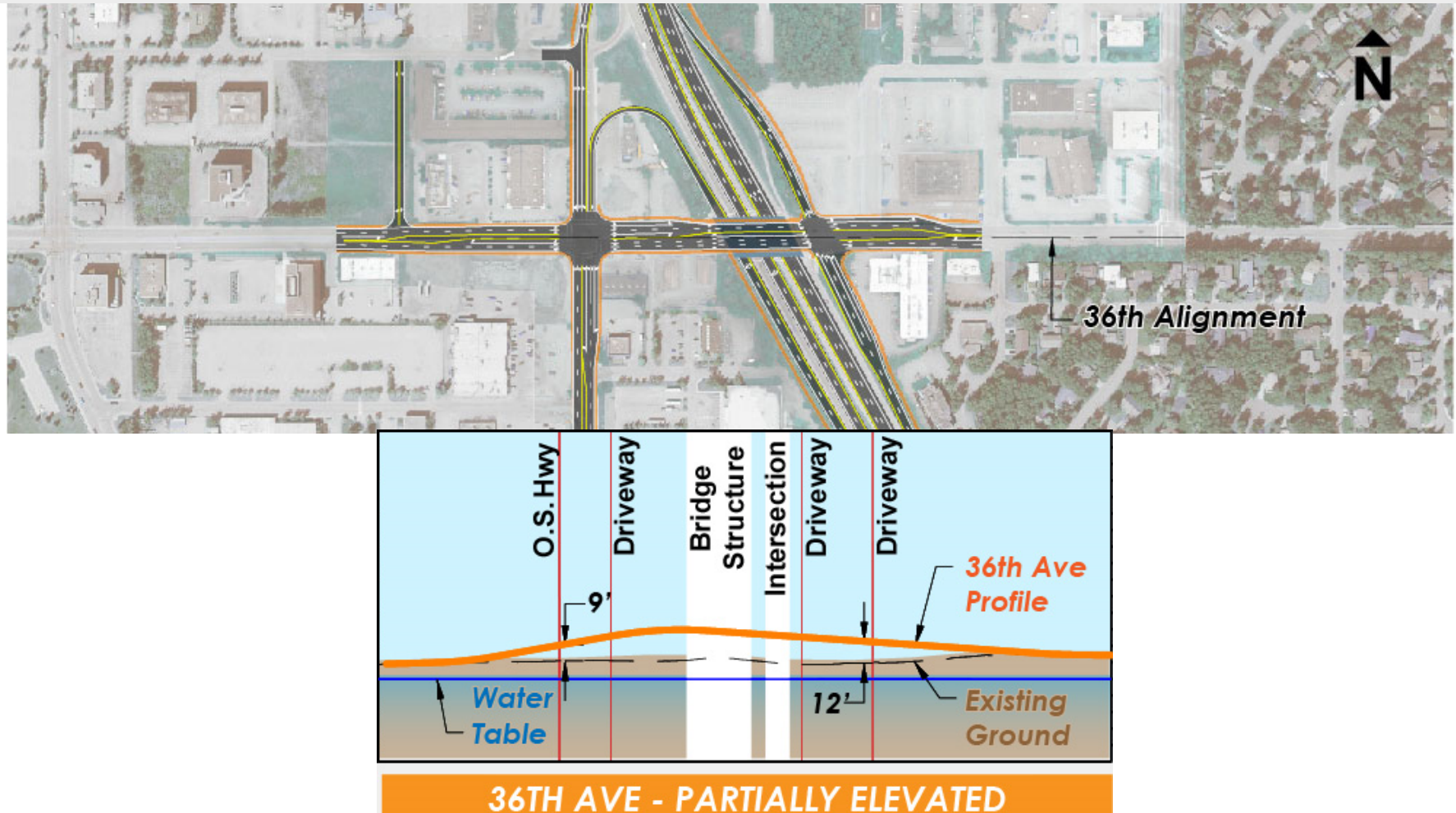
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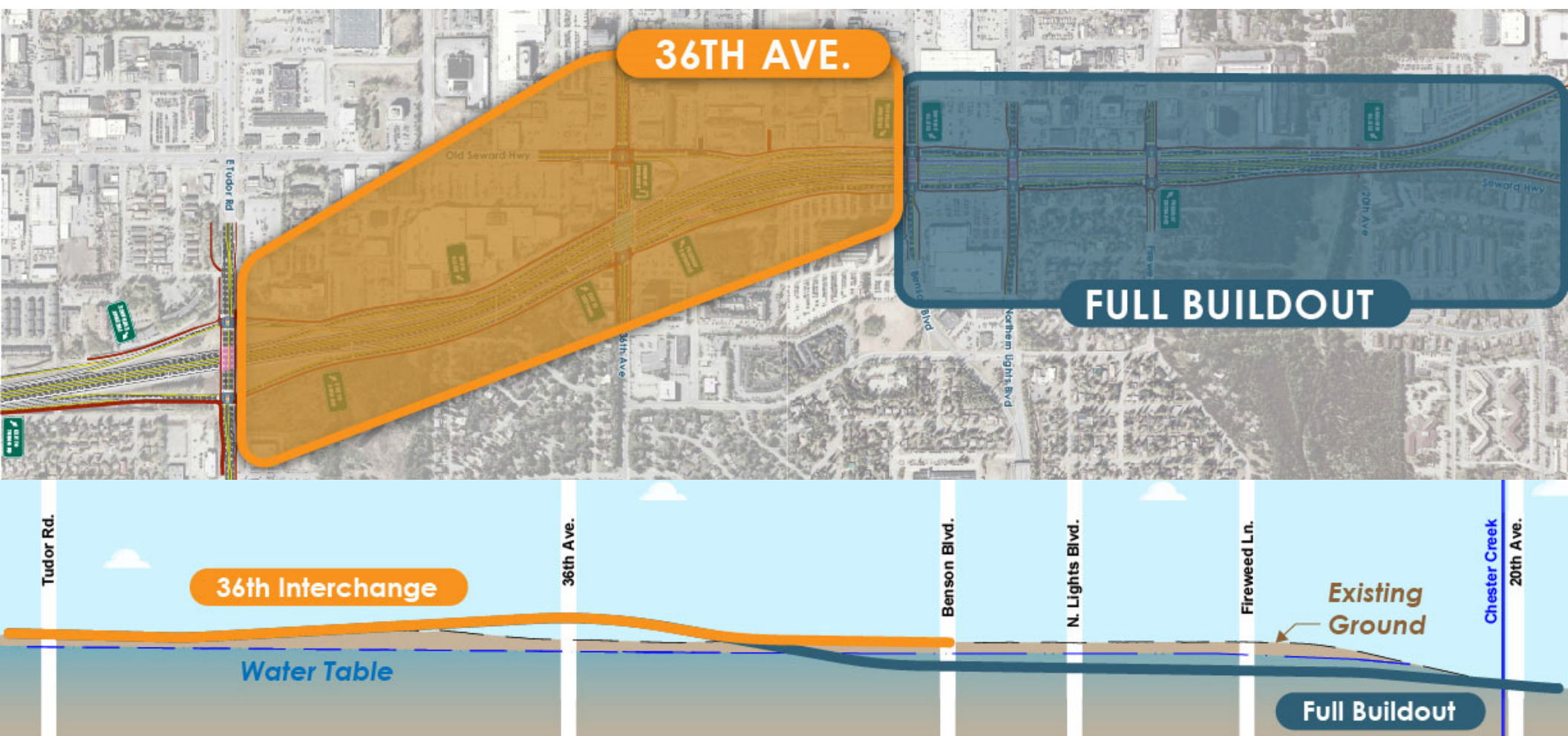
HYBRID PROFILE



HYBRID PROFILE



FULL BUILDOUT



OBSERVATIONS

- **Water Table:** high water table will require intersection to be pumped continuously (both groundwater and stormwater). Pumping volumes are significant and dewatering impacts could extend beyond the project area (currently being evaluated)
- **Contamination:** There is contamination on a property near the intersection, which will require treatment prior to discharge
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- **Noise:** Noise Study methodology has been developed and is currently under review. There are 7 residential properties that are “first row receivers” (immediately next to the project area)
- **Driveways:** Driveways on 36th Avenue are very close to the intersection, which create challenges with elevating or depressing 36th Avenue to reduce the height of an overpass



SCHEDULE

Task	Date
PEL Study	2017-2020
Public Involvement	Ongoing
Preliminary Environmental & Design	2020 - Ongoing
Final Design/ROW Acquisition	TBD
Construction	TBD





36th avenue INTERCHANGE

Thank You

Questions or Comments?

36thinterchange@dowl.com

www.36thinterchange.com



From: [Renee Whitesell](#)
To: [Sean Baski](#)
Cc: [Steven Noble](#); [Katie Conway](#); [Morgan McCammon](#); [Rachel Steer](#)
Subject: Campbell Park Community Council
Date: Thursday, September 16, 2021 8:12:06 PM

Sean,

Steve Noble and Renee Whitesell attended the Tudor Area Community Council meeting this evening, Thursday September 16. Steve provided an update presentation on the 36th Avenue interchange project, specifically focusing on early engineering to explore the profile of the interchange.

11 people were in attendance at the meeting. Notable attendees included Sen. Elvi Gray-Jackson, Rep. Andy Josephson, and Assembly Reps. Meg Zaletel and Felix Rivera.

Questions included:

- **Rep Josephson:** Enquired about the use of the term “discharge” when describing pumping the water table. Steve explained what pumping would be needed because of the “bowl” created by a depressed intersection. He also explained the implications of the contaminated site adjacent to the interchange, and that treatment would be needed to handle contaminated discharges.
- **Kevin Fimon:** asked what the noise study would address, and whether acoustic treatments were being considered for pavement. Steve recommended a future presentation specifically on noise. He explained that noise barriers tend to be used in Alaska, and pavement treatments tended to not last well in our Arctic climate. Steve noted that we will communicate with the public as part of the noise study, particularly the Geneva Woods Homeowners Association, Rogers Park Community Council, and Campbell Park Community Council.

Renee posted the project website in the chat box, a link to the Midtown Congestion Relief PEL Study website, and contact details to communicate with the project team.

Thanks,
Renee

Renee Whitesell, PTP
Senior Project Manager

DOWL

(907) 562-2000 | office
(907) 865-1161 | direct
(907) 602-6812 | cell

dowl.com

From: [Renee Whitesell](#)
To: [Sean Baski](#)
Cc: [Steven Noble](#); [Rachel Steer](#); [Katie Conway](#); [Farr, John D \(DOT\)](#)
Subject: 36th Avenue Interchange: Midtown CC and North Star CC - 2/10/21
Date: Thursday, February 11, 2021 9:16:00 AM

Sean,

I presented the Seward Highway: 36th Avenue Interchange project at Midtown CC yesterday lunchtime, and North Star CC last night.

Midtown CC

This was an in-person meeting attended by approximately 16 people, supplemented by a Zoom call-in with approximately 5 people on the line. The President is an active "Save Anchorage" group participant (he was wearing a large button promoting the group) and encouraged people to remove masks when speaking whilst sharing a microphone. Although I was verbally confirmed as being on the agenda it was overlooked, and when updating the agenda the President requested the addition by calling the project "The nightmare that's going to happen at the end of 36th and Seward".

The meeting was attended by Assembly member Rivera (via Zoom), who left immediately after providing his update. Sen. Gray-Jackson forwarded a written briefing but was not in attendance.

I provided an introduction to the project, and copies of the project fact sheet were distributed, which include the project website and email address. I also let participants know about the upcoming Open House, emphasized the website and how to sign up to receive emailed project updates.

Questions included:

- Is this project part of the larger project between Tudor and Fireweed? I explained how the PEL Study evaluated the area and identified several projects that together would implement the larger improvements, and this was two of the projects identified in the PEL Study.
- Be aware of construction impacts, especially disruption to businesses during the build.

The President clarified his earlier comment was concern about construction impacts. He also complimented the team on their communication to date and encouraged all members to reach out with questions as he felt we have done an excellent job of listening and responding throughout the PEL process. He further noted he was pleased the 36th Interchange was the first project moving forward.

North Star CC

This was a Zoom meeting attended by 13 participants at its peak. Notable attendees included: Sen. Gray-Jackson, Rep. Drummond, Assembly member Zaletel, Assembly member Rivera, Mayoral candidate Bill Falsey. All stayed for the duration of the meeting.

A similar presentation was given as detailed above for Midtown CC, using the screen-share function to show participants through the key features of the project website. No specific questions were raised, but the group expressed support for an online meeting supplemented by the ability to meet independently for specific questions.

This CC is particularly interested in the upcoming Fireweed Lane rehabilitation project, and there was a long discussion on the need for a road diet on the street. They are excited to be involved when it moves forward, and are particularly interested about how a road diet will tie in with the needed lane configuration at Seward Highway.

Tonight is Fairview CC.

Thanks, Renee

Renee Whitesell, PTP
Senior Project Manager

DOWL

(907) 562-2000 | office
(907) 865-1161 | direct
(907) 602-6812 | cell

4041 B Street
Anchorage, AK 99503

www.dowl.com

From: [Renee Whitesell](#)
To: [Sean Baski](#)
Cc: [Steven Noble](#); [Rachel Steer](#); [Katie Conway](#); [Farr, John D \(DOT\)](#)
Subject: 36th Avenue Interchange: Midtown Community Council
Date: Wednesday, May 12, 2021 2:47:07 PM

Sean,

I attended the Midtown Community Council at lunchtime today. Nothing major to report. Although we requested a project update on the agenda this was not provided, and we will request an update again at the Council's June meeting.

Notable attendees: Assemblyman Rivera (via Zoom), Assemblywoman Zaletel (in person).

Assemblywoman Zaletel noted during her update that there was progress with the 36th Avenue Interchange project, which has changed from what is recommended in the PEL Study. She recommended to the Council that they seek an update presentation from the project team so they could participate and comment on the work that was occurring.

Questions:

- Board Member Rosemary Borchardt – sought clarification on what was occurring with the project. She noted that when she looked at the project website, all she saw was a figure with a green line. Assemblyperson Zaletel noted the PEL Study was available online and offered to show Ms. Borchardt where it is. She noted the project had changed however and there were now three alternatives being considered, but she didn't think that information was available.

Following the discussion I raised my hand and introduced myself. I noted we had visited with Midtown Community Council in March and had asked to provide an update this month. I noted that we would like to visit with the Community Council in June to provide a presentation on the project. I let the Community Council know that there was information and a presentation video on the project website, 36thinterchange.com. No other questions were asked.

I followed up with Ms. Borchardt After the meeting and provided her with my business card with the project website noted on the back. She asked me to show her where the presentation was, and I directed her to both the presentation and the PDF of the presentation and script. She enquired about whether there would be turn movements in all directions, and I showed her the part of the presentation where that is shown. I encouraged her to watch the video, as I noted these are animated, which will make it easier to understand. Ms. Borchardt Also noted concerns about the impacts on the project on the former Golden Lion hotel, and noted she thought the project would take a lot of the parking. I explained that we are expecting there to be strip acquisitions, and that some of the parking was already located within DOT&PF ROW. She asked how many parking spaces were affected, and I said we did not have that information as yet, but we were working to minimize right-of-way impacts. Ms. Borchardt thanked me for the additional information.

We will follow up closer to next month's meeting to ensure we are on the agenda to provide a more formal presentation.

Renee

Renee Whitesell, PTP
Senior Project Manager

DOWL

(907) 562-2000 | office
(907) 865-1161 | direct
(907) 602-6812 | cell

dowl.com

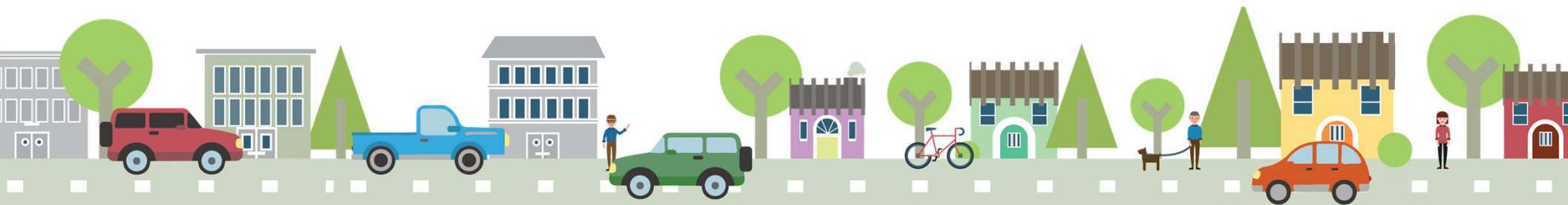
06.09.2021



36th avenue

INTERCHANGE

Seward Highway: 36th Avenue Interchange Midtown Community Council Meeting



PROJECT HISTORY AND BACKGROUND

- Midtown Congestion Relief Planning and Environmental Linkages (PEL) Study concluded in 2020 and recommended 5 projects
- 36th Avenue Interchange is the first project moving forward
- Interchange was studied in 2013-2015 but did not progress
- This is a fresh design and environmental permitting effort



WHY WAS THE 36TH INTERCHANGE SELECTED? — — — — —

- PEL Study recommended at-grade intersection improvements to construct frontage roads and implement two-phase signals
- Improved traffic flow on Seward Highway in short-term (modelled through 2028), but still had delays and safety issues associated with at-grade crossings (albeit much less than current)
- Full grade-separated interchange still needed to fully improve intersection past modelled 2028 volumes, improve safety, reduce delay above LOS F
- Concern that full interchange would be needed immediately on completion of at-grade project, creating several years of consecutive construction and rework of only recently completed investments
- Statewide Planning chose to move to full interchange immediately to overcome this concern and responsibly program constrained funds in STIP



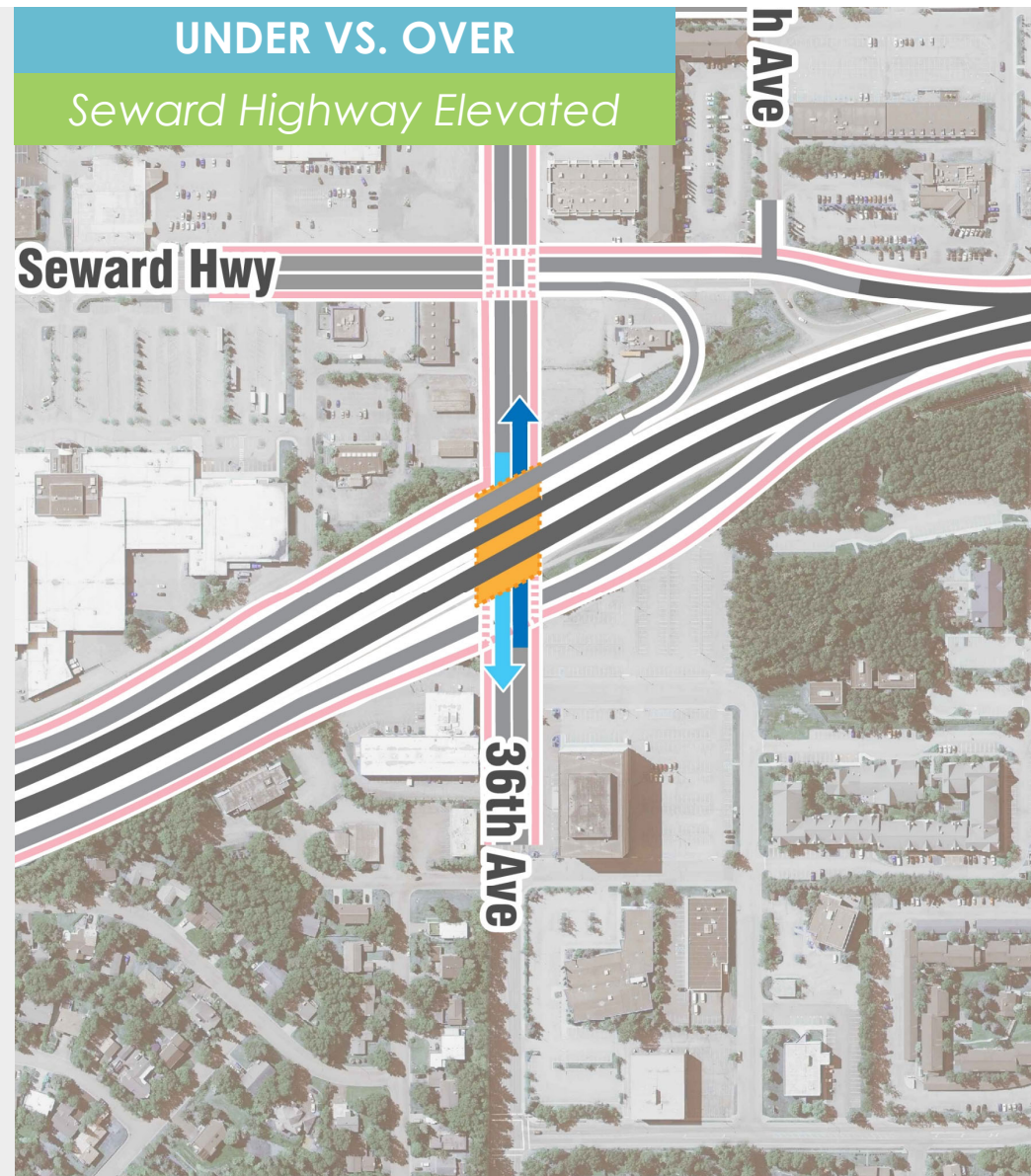
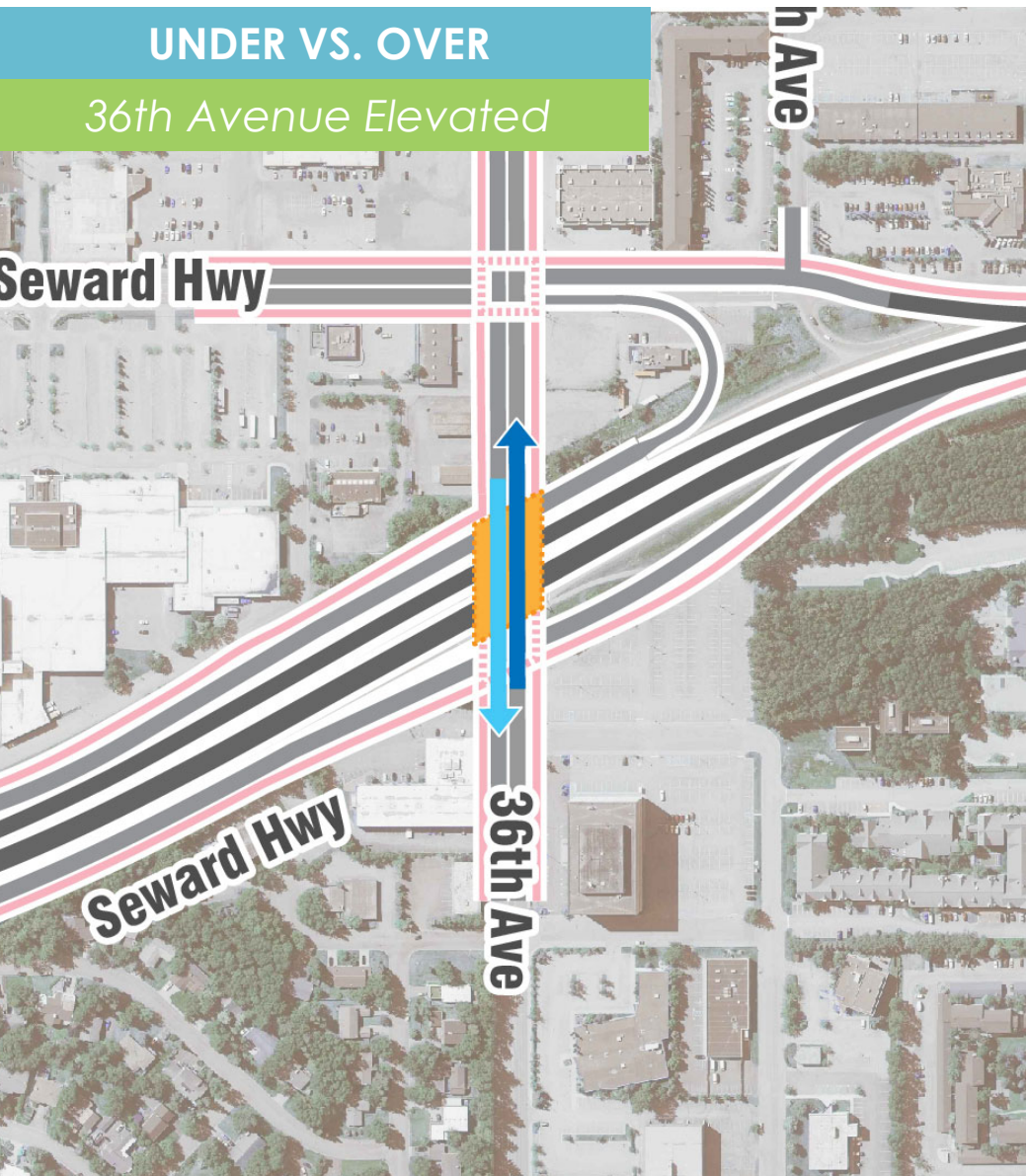
PROJECT STATUS — — — — —

- **Working on most promising alternatives:** Two loop ramp concepts
- **Refining vertical and horizontal geometry:** Minimizing vertical impacts
- **Initiating noise study:** Summer 2021
- **Open House #2:** scheduled for September 2021

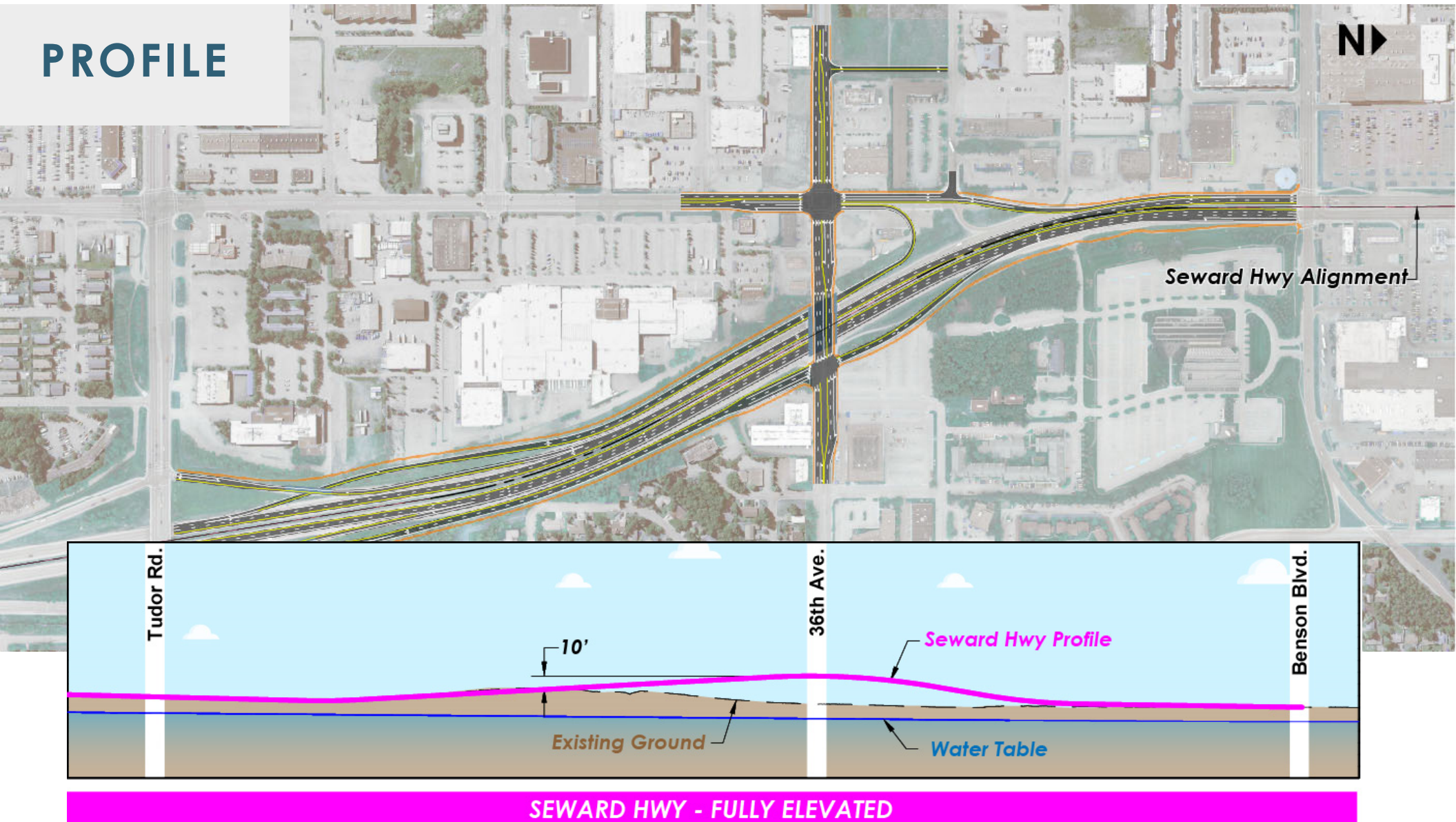
Everything at 36th Avenue is being designed to be forward compatible with a depressed freeway at Benson Boulevard, Northern Lights Boulevard, and Fireweed Lane, as recommended in the

PEL Study

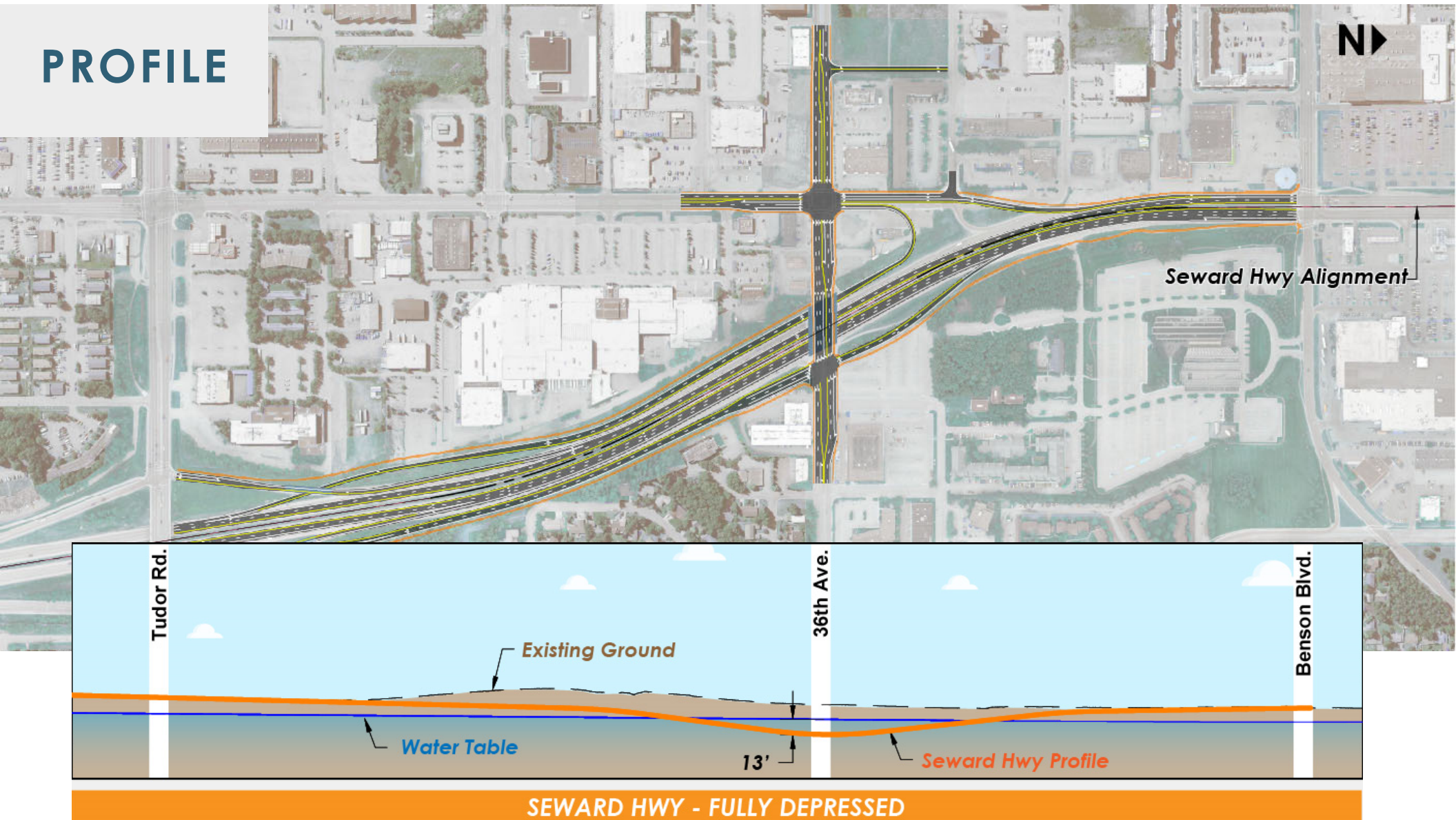




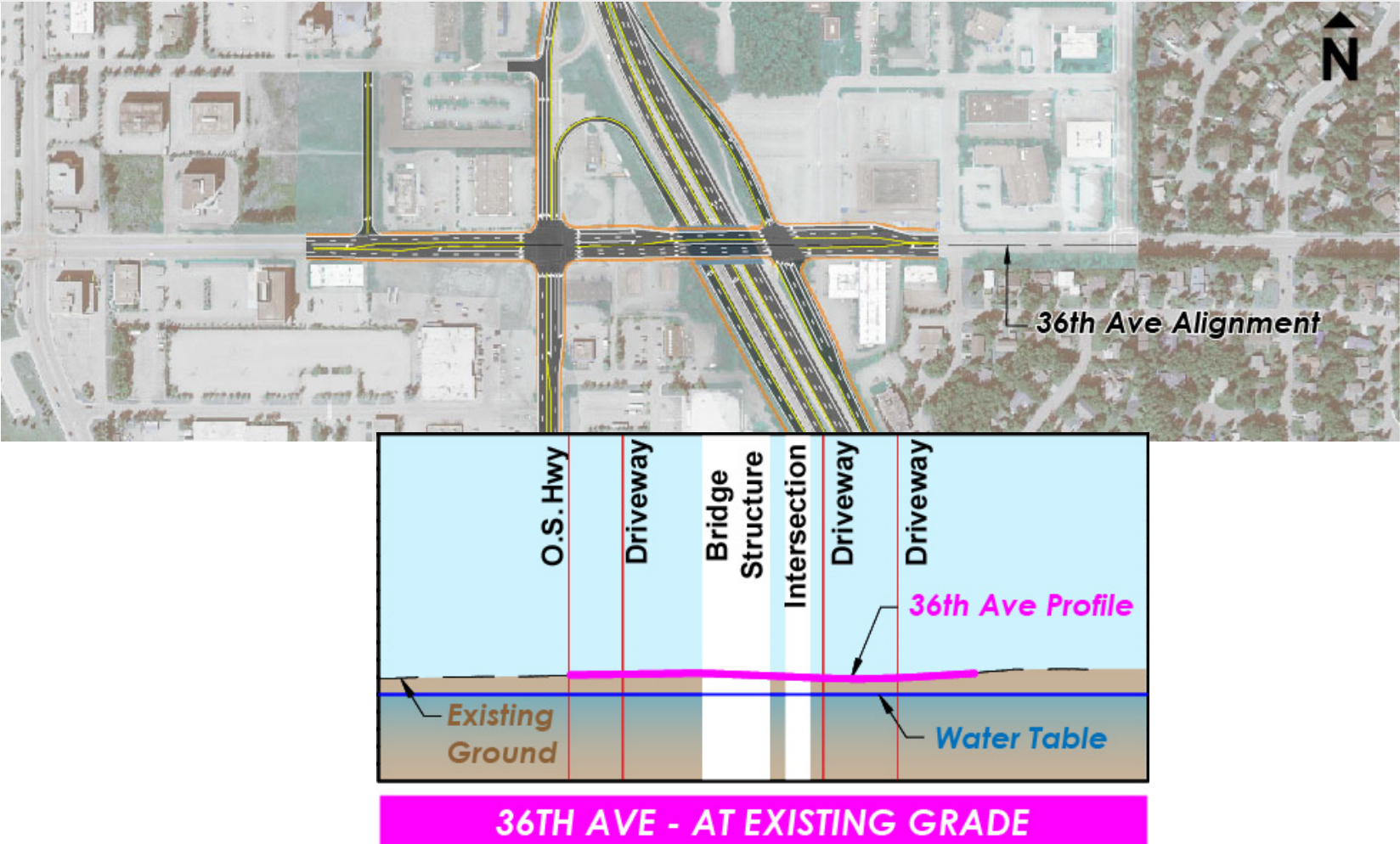
PROFILE



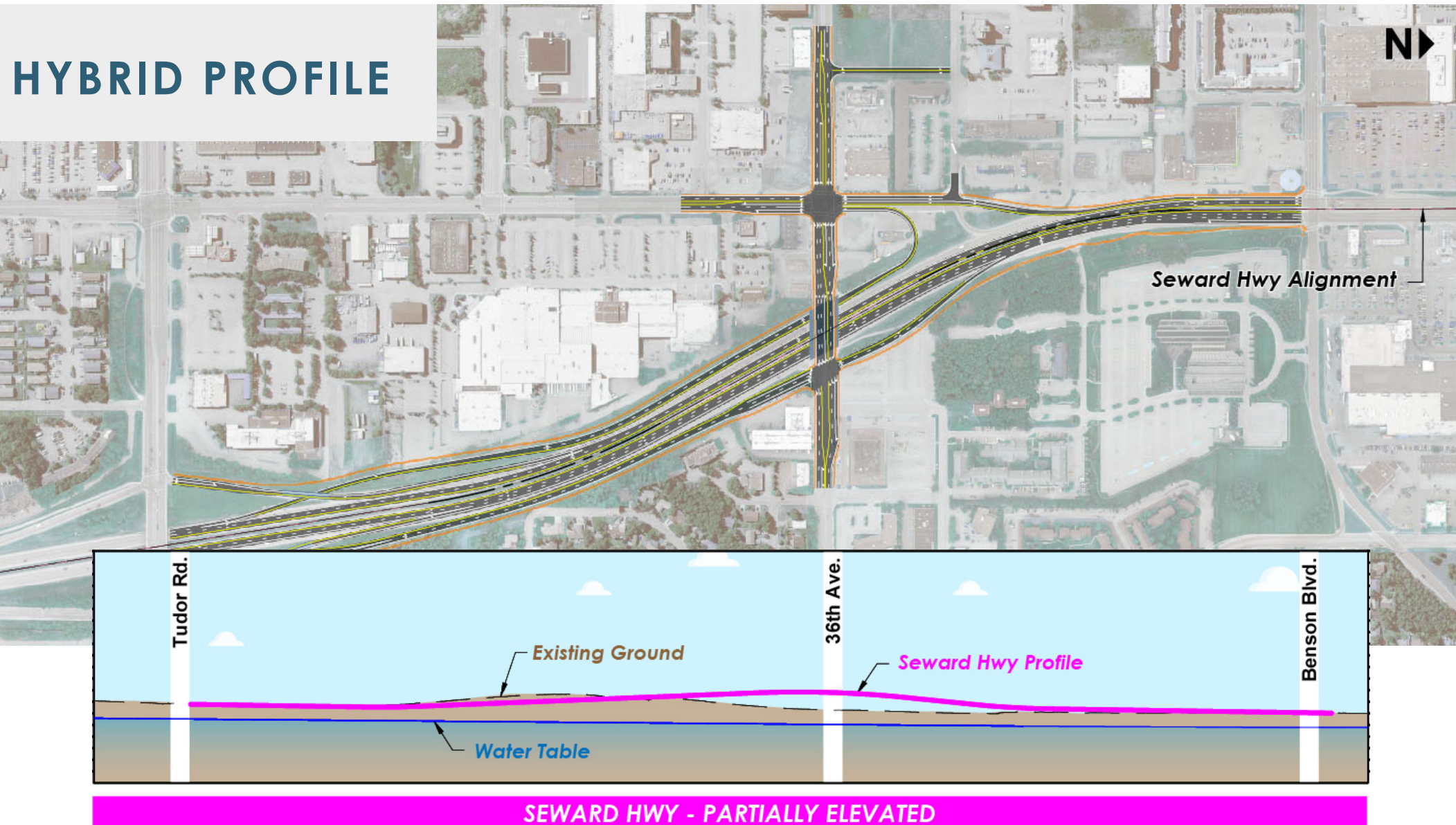
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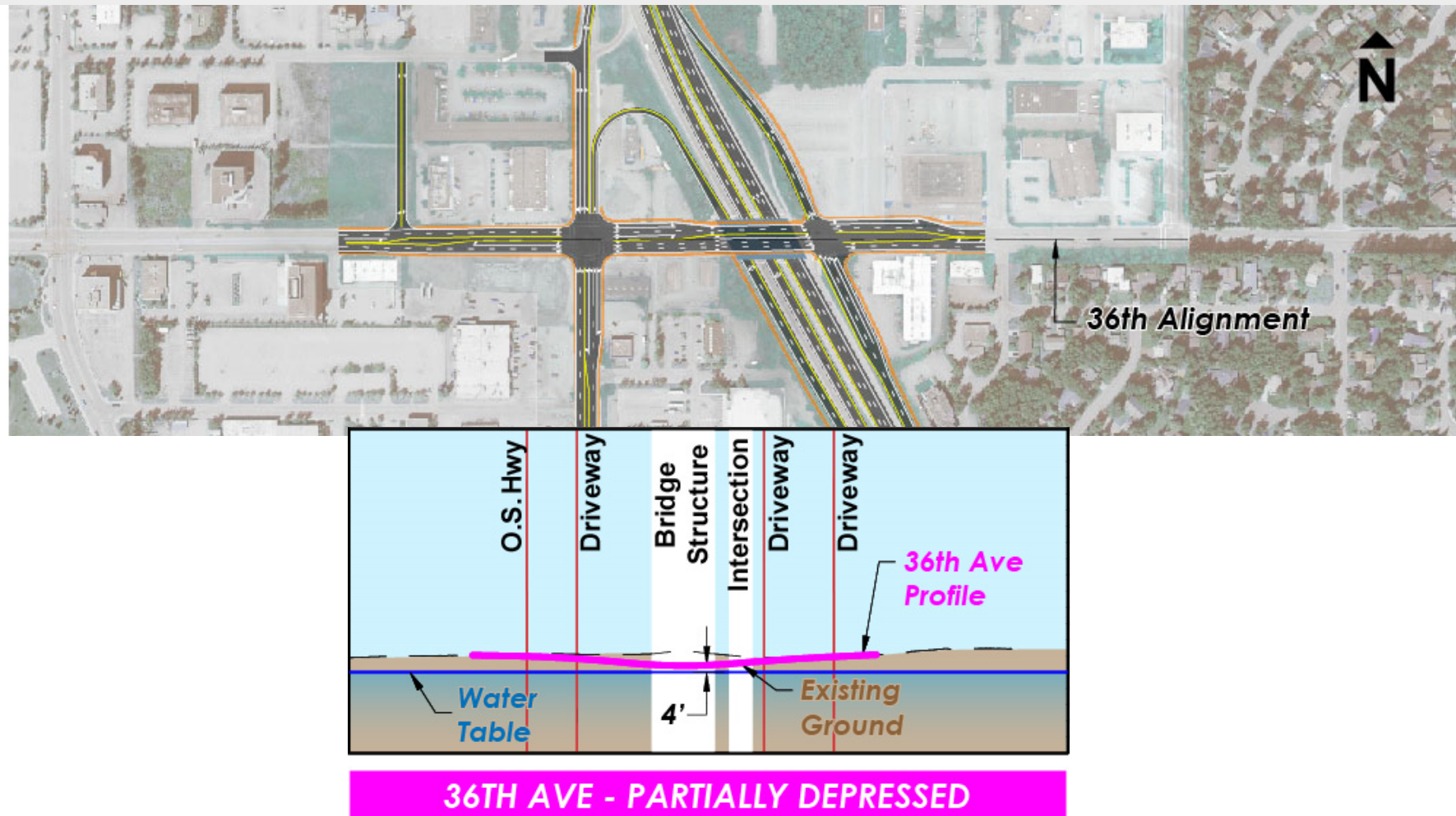
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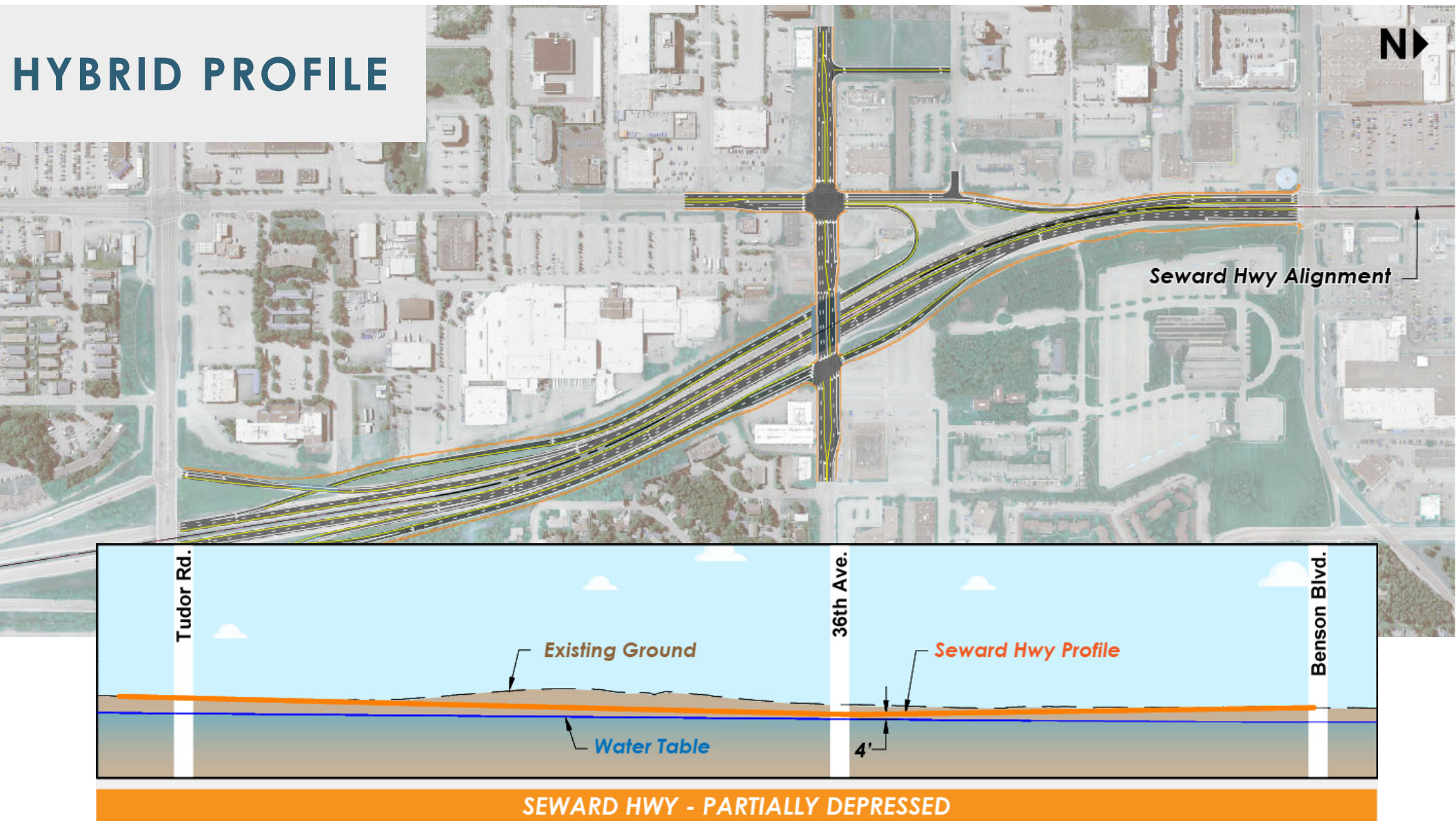
HYBRID PROFILE



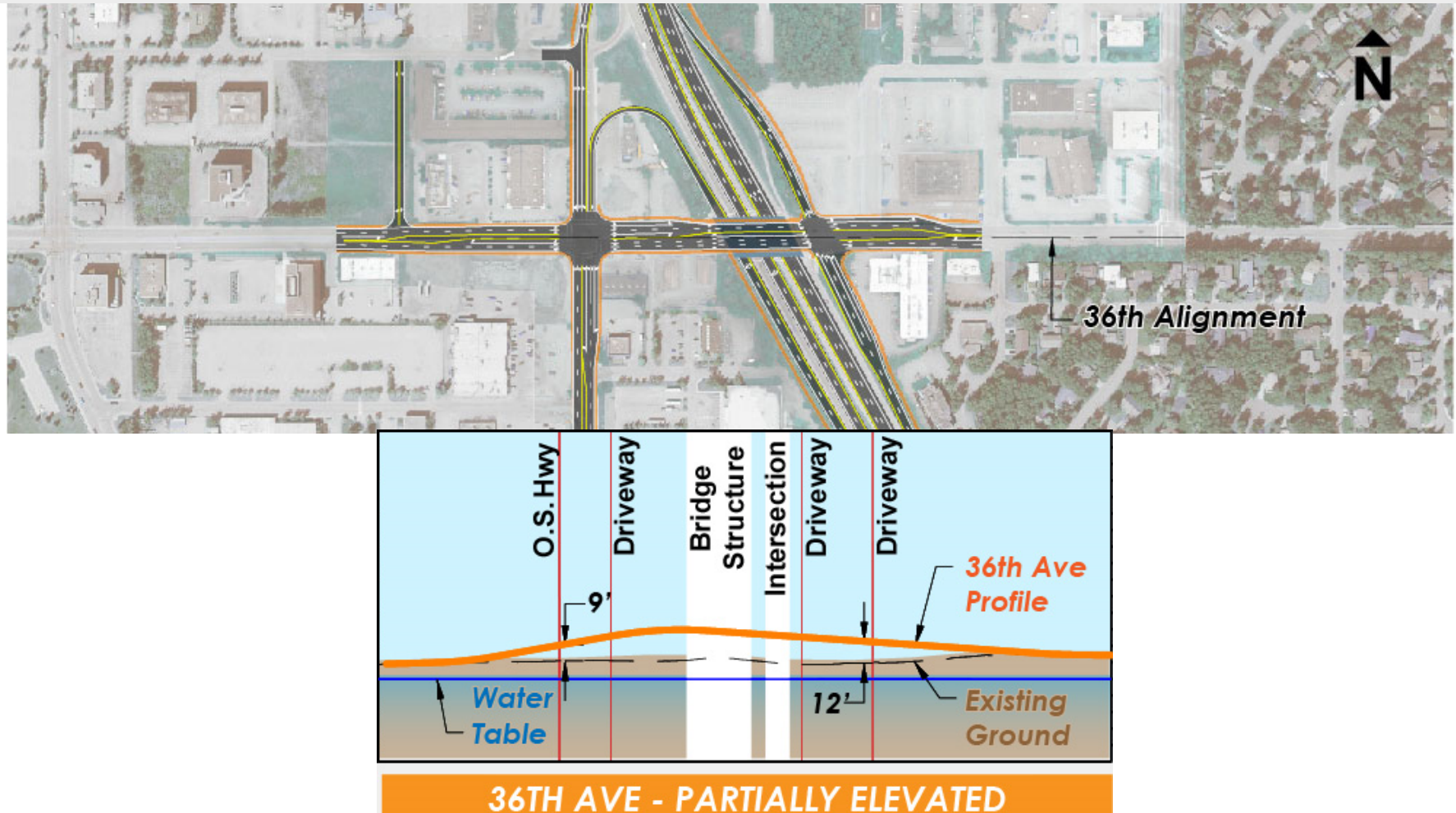
HYBRID PROFILE



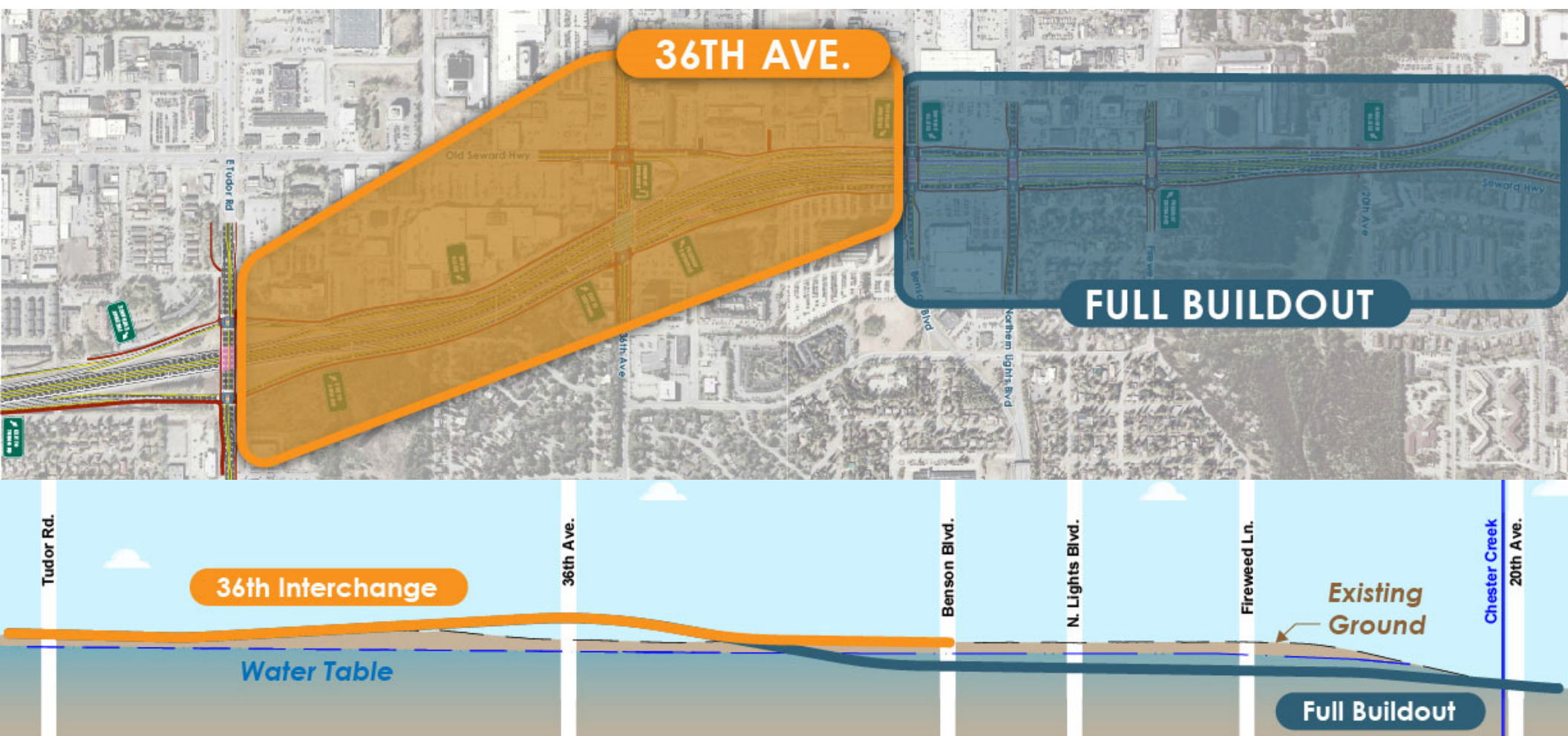
HYBRID PROFILE



HYBRID PROFILE



FULL BUILDOUT



OBSERVATIONS

- **Water Table:** high water table will require intersection to be pumped continuously (both groundwater and stormwater). Pumping volumes are significant and dewatering impacts could extend beyond the project area (currently being evaluated)
- **Contamination:** There is contamination on a property near the intersection, which will require treatment prior to discharge
- **Visual:** The entirely elevated highway is approximately 10 feet above the existing ground level between Tudor Road and 36th Avenue. Visual renderings are going to be prepared to show indicative views for upcoming open house
- **Noise:** Noise Study methodology has been developed and is currently under review. There are 7 residential properties that are “first row receivers” (immediately next to the project area)
- **Driveways:** Driveways on 36th Avenue are very close to the intersection, which create challenges with elevating or depressing 36th Avenue to reduce the height of an overpass



SCHEDULE

Task	Date
PEL Study	2017-2020
Public Involvement	Ongoing
Preliminary Environmental & Design	2020-2022
Final Design/ROW Acquisition	TBD
Construction	TBD





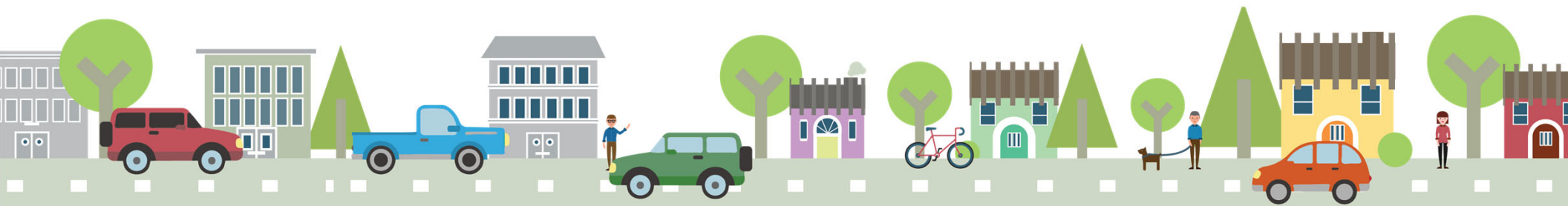
36th avenue INTERCHANGE

Thank You

Questions or Comments?

36thinterchange@dowl.com

www.36thinterchange.com



From: [Renee Whitesell](#)
To: [Sean Baski](#)
Cc: [Rachel Steer](#); [Katie Conway](#); [Steven Noble](#)
Subject: Midtown Community Council Debrief 6/9/21
Date: Thursday, June 10, 2021 3:30:03 PM

Sean,

We attended Midtown Community Council yesterday lunchtime and provided a presentation update on the 36th Avenue Interchange Project. Nothing major to report from this meeting.

Notable attendees: Assemblywoman Zaletel.

Sean provided a similar presentation update to the one provided at Rogers Park Community Council in early May, but this group was more interested in a general project update than the details of the profiles currently under consideration. Questions included:

- President Davidge – noted he would like to organize a briefing with the new Mayor. He noted that he thinks this should be DOT&PF's highest priority project in Anchorage, and notes that he currently avoids the intersection due to delay and congestion.
- Assemblywoman Zaletel – asked about whether the traffic slowing currently at 36th Avenue would be shifted to Benson Boulevard (and thereby the traffic noise will be shifted). She also asked whether there has been estimates of how much traffic will leave the highway at the new 36th interchange because of the improved traffic patterns. She would like to understand this as the project is likely to be in place for a while without the Benson/Northern Lights/Fireweed grade separations proceeding.
- President Davidge – asked whether anyone has considered an interchange/intersection at Seward Highway and International Airport Drive.
- President Davidge – asked that the project team keep Midtown Community Council informed about the project.

Sean noted that traffic movements at the intersection would be much freer flowing and more efficient than today's at-grade intersection. With regard to the International Airport intersection, he noted that DOT&PF could consider another intersection/interchange at this location in the future, but it would be one of many projects that DOT&PF could consider in the state and there is currently limited funding to address the many needs. Sean welcomed the provision of further updates to the Community Council, and let them know that the next public open house meeting on the project was planned for September 28, 2021. I introduced myself to the Community Council at the end of the presentation and let them know that I was a member and attended the meetings each month, and would be happy to answer questions.

Following the meeting, Assemblywoman Zaletel approached us and provided clarification on her question. She would like to know whether traffic modeling has looked at the turning volumes likely to be created at the 36th/Seward Highway intersection, and the ability for the surrounding street network to absorb the changed traffic patterns. Sean noted that we can get this information and provide it to her. She also outlined an interest in the height of the gradient increase in relation to the existing noise walls adjacent to the Geneva Woods neighborhood. She would like to understand the existing volumes, and whether these will increase or change as a result of the construction of the interchange. Residents of Geneva Woods are concerned that additional noise impacts will occur as a consequence of the elevated structure. Sean noted that noise analysis would consider this, and visual analysis is also being proposed to understand the implications of the interchange and braided ramps immediately north of the Tudor interchange.

Thanks,
Renee

Renee Whitesell, PTP
Senior Project Manager

DOWL

(907) 562-2000 | office

(907) 865-1161 | direct

(907) 602-6812 | cell

dowl.com

FAIRVIEW COMMUNITY COUNCIL
General Membership Meeting Agenda
February 11, 2021 — 7:00-8:45 PM

ZOOM: <https://us02web.zoom.us/j/84049110076> (see more details below)

**Please note that it is likely this meeting will be recorded and streamed on social media by entities
unaffiliated with the Fairview Community Council**

Call to Order

Welcome and Introductions

Approval of Agenda

Approval of January 2021 General Membership Meeting Minutes

Elected Officials

Senator Tom Begich

Representative Zack Fields

Anchorage Assembly member Christopher Constant

Anchorage School Board member Andy Holleman

Standing Reports

President Allen Kemplen

Treasurer Sharon Chamard

Presentations

Nick Davis, Senior Captain, Station 1, Anchorage Fire Department

Renee Whitesell, DOWL – 36th Ave. project update (Mid-Town Congestion Relief)

Eric Montoya – change of ownership of Retail Marijuana Establishment license for Alaska Tasty Cannabis

Dana Giliam and Eric Giliam – new Retail Marijuana Establishment license for Highpothemia

Representative from Municipal Street Maintenance – Snow removal

Unfinished Business

Fairview Community Council Resolution 2020-08 requesting the Municipality of Anchorage to modify its Snow Management Plan for the Karluk and Medfra Street hills south of 15th Avenue to resolve an unsafe pedestrian streetscape issue and to provide a safe snow-free sidewalk throughout the winter

Fairview Community Council Resolution 2021-01 regarding improvements along the Chester Creek Greenbelt

Fairview Community Council Resolution 2021-02 regarding public comments on the AMATS Non-motorized Plan

New Business

Report and Updates

Community Comments

Adjourn

Information about Zoom

At the March 2020 General Membership Meeting, there was discussion about moving to a different method for future meetings because of the importance of avoiding gatherings during this time of COVID-19, and an ad hoc committee was created to explore options. The Fairview Recreation Center was closed the next day. Subsequently, the ad hoc committee recommended that we use a virtual meeting interface application called Zoom.

Zoom is used quite a bit in business settings and is fairly user-friendly. It can be used on smart phones, tablets, laptops, and desktops. But it has its limitations with respect to bandwidth. In ordinary times, it is possible to see the faces of other meeting participants (assuming everyone has a webcam), but given the recent massive increase in use of the internet the guidance has changed, so we ask meeting participants to do the following:

- Do not use your video camera
- Make sure your microphone is muted

These will be the default settings when you join the meeting, so don't worry if you're not sure how to do this.

Also, we will be using the chat box feature in Zoom for asking questions and for making comments, although if anyone really want to speak, they can request this in the chat box and will then be "unmuted." This may sound a bit harsh, but it's the virtual meeting equivalent of raising your hand and being recognized by the chair.

How Do I Access Zoom?

- 1) If you want to connect using a smart phone or some other device connect to the internet, go to this URL: <https://us02web.zoom.us/j/84049110076>
- 2) If you are on a regular phone and want to connect using audio only, there are several dial-in numbers. After you're connected, enter 840 4911 0076# on your keypad.

+1 669 900 9128 US (San Jose)

+1 346 248 7799 US (Houston)

+1 301 715 8592 US

+1 312 626 6799 US (Chicago)

+1 646 558 8656 US (New York)

+1 253 215 8782 US
- 3) If the audio on your internet-connected device is not working properly (usually because of how your speakers are configured), you can try also dialing in on a regular phone to get the audio.

FAIRVIEW COMMUNITY COUNCIL
General Membership Meeting Minutes

February 11, 2021 — 7:00-9:04 PM

ZOOM: <https://us02web.zoom.us/j/84049110076> (see more details below)

Please note that it is likely this meeting will be recorded and streamed on social media by entities unaffiliated with the Fairview Community Council

Call to Order

Welcome and Introductions

Approval of Agenda

Moved by SJ Klein, seconded by Dave Rittenberg. Several changes, no objection to agenda with those changes.

Approval of January 2021 General Membership Meeting Minutes

Moved by Jerome Lopes, seconded by Leslie Kleinfeld. One change noted by Leslie Kleinfeld to the name of her business, Fit4Life changed to Fit for Health. No objection to minutes with that change.

Elected Officials

Brix Hahn on behalf of Senator Tom Begich

The House organized today. The Governor's current executive order will expire this week. When this happens, several protections will go away. The Governor is Bills Senator Begich is working on include Preparing Readers and Educating Kids Act (SB 8), and a bill providing free tuition to essential workers (SB 10). If you have any capital requests please send them to Senator Begich as soon as possible.

Comment from Allen Kemplen – Read in ADN that Governor Dunleavy is passing authority for emergency orders to legislature. Brix Hahn – before today the House wasn't organized and you need both houses to pass legislation to extend emergency orders.

Q from Leslie Kleinfeld – What do you mean "vaccines will go away"? Brix Hahn – I meant that vaccines will be limited. A by Tristan Walsh – Governor is putting this into legislatures lap. Would support an extension of the emergency orders. But the bill has been delayed on the Senate side. Clock runs out on Monday. Now that House is organized, it should be able to pass the bill.

Q from David Rittenberg – last month he requested that SB 10 include people who work in "safety net" services to be included as essential workers for purposes of free tuition. A from Brix Hahn – will check and get back to you.

Tristan Walsh on behalf of Representative Zack Fields

Rep. Fields nominated Rep. Stutes as chair, but the House is still not fully organized. The coalition might grow beyond those who voted for Stutes.

Bills to be introduced include one concerning a child care tax credit, and the Performance scholarship (changes to how much tuition the different levels will cover). Another bill is with Rep. Shaw to increase funding for the Port. Stimulus - State government of Alaska will receive about \$1B with about \$270M going directly to municipalities. There will be a capital budget; please let us know what your priorities are. We are helping people navigate PPP and unemployment. Q from Allen Kemplen – will there be an appropriation from the legislature for capital projects, or will there just be bonds put in front of the voters? A from Tristan Walsh – Right now it is mostly bonds. It is not a huge capital budget, but it's good to get your top two or three community council priorities in as soon as possible.

Senator Tom Begich – they absolutely need our votes to pass budget, will focus on capital budget, want to get meritorious projects into CAPSIS. The faster we get the list the better the chance we can get them

in the budget. It's also important for bonding, because they might be on the special election ballot in the summer.

Q from Leslie Kleinfeld – Alaska has done a phenomenal job getting people vaccinated. Why doesn't Governor want to continue the emergency order? Tom Begich – There is no reason for him to violate his constitutional obligation to protect the life, health and safety of Alaskans. He can't pass it to the legislature because the legislature can't vote on it because house isn't organized.

Q from Dave Rittenberg – will non-profit safety net services be included in SB 10? Tom Begich – yes, they will be included in the version that is coming before the Education Committee.

Anchorage Assembly member Christopher Constant

American Rescue Plan is moving quickly through Congress. It identifies about \$100M for Anchorage out of \$1B for Alaska; this is about 20% of municipal budget. These funds can be used for revenue replacement. We have appropriated money and opened a new program for residential rent relief. It will allow people who qualify to have up to 12 months of rent relief, broken up into three-month blocks. Checks will be written to landlords, not to tenants. Have spent \$20M on rent relief so far, and it has been successful in keeping people housed.

Funds from the American Resecure Plan can be used for revenue relief, but focus should be not so much on reducing property taxes but helping where there is genuine need. Will want to support tourism industries, owners and especially employees.

New employee being hired by the Municipality. It is the Chief Equity Officer – intended to create a new division of people to ensure that government investments are being made in a more equitable manner – to lift all boats. The position will report to mayor and Assembly. People should apply, even if they don't meet the published qualifications. Life experience can be taken into account.

Last Assembly meeting – proposal to convert emergency orders to ordinance. Chris did not support permanently encoding things like a mask mandate. After a long debate it was postponed indefinitely. There was also conversation about what is acceptable Assembly member behavior. Muni code has a process detailing how to remove an Assembly member. The intent was to not politicize Assembly process. The body agreed to postpone that item.

Muni code doesn't prohibit people living in their cars on private property (re: concern by Kristin Riall)

Q from Allen Kemplen – Congress and the federal administration are working on a fairly large infrastructure bill. Christopher Constant – The Secretary of Transportation has committed to visit Alaska. In initial conversations after his confirmation, he said his goals are to undo historic inequities and other damages caused by past transportation decisions that hurt communities of color. Christopher is planning to give him a good understanding of these issues in Fairview and Anchorage.

Anchorage School Board member Andy Holleman

Elementary school is open for face-to-face now. Some classes have been made remote again, but this is not affecting other classes in the same school. Positivity rate for COVID is declining in Anchorage. Middle school and high school will be back about the end of the first week of March for the start of the 4th quarter. There won't be any ASD bonds this year; last year we proposed a two-year bond package. Greuning School is going to be rebuilt and will be reopened. It has been closed since the earthquake.

Standing Reports

President Allen Kemplen

We may be back on track for our summer activities – improvements down at Eastchester Park. There will be opportunities to volunteer and meet your neighbors. We have put together a request for artist proposals for utility box wraps and expect to see proposals before the council by May. There will also be

planting of birch trees along 20th Avenue to help define the park a little better. Another part of the proposal is to build an information kiosk using logs, but may not happen this summer.

We have a request into DOT for a permit to use the vacant lot at 14th and Ingra for a community garden and are continuing to move forward with that process and to think about a design. If we can get the permit this summer, it will be another opportunity to volunteer and help build something of value for our neighborhood.

Treasurer Sharon Chamard

\$7 bank fee

Current balances:

General \$232; Comm. Dev. \$1013; Beautification \$274; A Better Fairview \$7930. Total \$9,449

Fairview Elementary School Bike Reading Program – currently \$1,550

Presentations

Renee Whitesell, DOWL – 36th Ave. project update (Mid-Town Congestion Relief)

Dowl is working with DOT on 36th interchange project. The Mid-Town Congestion Relief Planning and Environmental Linkages (MCR PEL) study looked at New Seward between Fireweed Lane and Tudor. The 36th interchange is a combination of 2 of the recommended 5 projects from the MCR PEL. It recommended surface transportation improvements to create a frontage road network for local traffic and a grade-separated interchange at 36th Avenue to separate the New Seward traffic from the local traffic. DOT is moving forward with that interchange project right away. We are in the early stages of considering alternative configurations for the intersection. There will be a virtual (on Zoom) open house on March 16. More information is available at <http://www.36thinterchange.com/>.

Q from SJ Klein – what is the status of the MCR PEL study? Has it been approved by the state/AMATS/and/or the Federal Highway Administration? If not what is holding it up? Renee Whitesell – the study is complete. It was delivered to the state and presented to AMATS. Don't know where it is in the formal ratification process.

Q from Leslie Kleinfeld – what is the conception to how to get pedestrians to safely cross the Seward Highway at Northern Lights? Renee Whitesell – that is not part of this particular project, it just is from Tudor to Benson. Q – does that section include a way for pedestrians to cross? A – will be tying into existing crossings. Suggest you make comments to the project team.

Sandra Millhouse – change of ownership of Retail Marijuana Establishment license for Alaska Tasty Cannabis – we are planning to upgrade 811 E. 6th. Will ask for guidance on landscaping. Also looking for a home in the neighborhood. SJ Klein – last year the guy who started Alaska Tasty Cannabis came to our meeting and promised some wonderful things. He said he would donate funds to support Fairview organizations, but he never did. There were expectations in the Neighborhood Responsibility Plan. The licensee failed to come to any council meetings since the initial meeting wherein we did not object to the license. SJ Klein moved that the council objects to transfer of the license because the current licensee did not comply with the Neighborhood Responsibility. Seconded by Leslie Kleinfeld. Comments by Leslie – thinks the new owner should have to put together a new Neighborhood Responsibility Plan, we should not just take the existing one. Colleen Howard – what is the goal of the motion? SJ – to clearly communicate to the Assembly that we oppose transfer of the license. It puts license holders on notice that they have obligations that we take seriously. Jerome Lopes – agrees that new potential owner

should get an opportunity to put together her own NRP. We need to give her a chance to present to our community. Eric Montoya – a license transfer is treated like a whole new license. He has put together new plans for the new licensee Canna Get Happy. The new owners are long-standing business owners with good relationships with other councils. Karen Gonne-Harrell – can we make the license transfer conditional if they have a new plan. We can look at their performance in other neighborhoods. Allen – perhaps the mover and seconder would agree to postpone the motion until next month to give the petitioner a chance to produce the NRP and share with council. SJ – Think Sandra has good intentions, want to welcome her to the neighborhood. SJ Klein moves to table the motion under consideration until March meeting. Sandra – I understand everyone's frustrations. Previous owner didn't do a lot of things. Eager to get the license changed to a new name. Leslie Kleinfeld seconds the motion. No objection.

Sharon Chamard moved, Dave Rittenberg seconded, to extend meeting until 9:00 pm. No objection.

Unfinished Business

Fairview Community Council Resolution 2020-08 requesting the Municipality of Anchorage to modify its Snow Management Plan for the Karluk and Medfra Street hills south of 15th Avenue to resolve an unsafe pedestrian streetscape issue and to provide a safe snow-free sidewalk throughout the winter

Allen Kemplen – this will be moved to next month because presenter from Street Maintenance was not arranged.

Fairview Community Council Resolution 2021-02 regarding public comments on the AMATS Non-motorized Plan

Moved by SJ Klein and seconded by Jerome Lopes to adopt the resolution. SJ Klein moves to amend the resolution to add additional items Appendix A. New item 6 would state "The pedestrian amenities along both sides of Ingraham and Gambell are inadequate to the non-motorized usage of those streets." New item 7 would state "The pedestrian and other non-motorized access to transit infrastructure is inadequate throughout the neighborhood, especially when compared to the primary transit corridors called out in the Land Use Plan Map." Leslie Kleinfeld seconds. No discussion, no opposition to motion to amend. Discussion on motion. Kristine Bunnell says item 3 should be amended to read "Anchorage **2040** Land Use Plan." Leslie Kleinfeld moves and SJ Klein seconds an amendment to that effect. No discussion, no opposition to motion to amend. No discussion on main motion, no opposition to approval.

New Business

None

Report and Updates

Dana Giliam – We'll be sending out a notice to neighbors in the area about our proposal to set up marijuana dispensary at 13th and Hyder Street. Our presentation at the council will be next month.

Kristine Bunnell – regarding the 2nd stage of the Downtown Comprehensive Plan. This is important for Fairview because the area includes part of Fairview. You contribute housing to Downtown, you contribute economically to Downtown. You have the Deteriorated Properties Investment Area which borders Downtown. We'll be taking about incentives and redevelopment in the entire study area. The

project kicked off in the last week of January. We are continuing with stakeholder meetings and will have a public meeting in April. See www.ourdowntownanchorage.com for presentations and recordings.

Maria Crawford – The Fairview Elementary School reading challenge ended on January 31. Winners will be announced tomorrow at the school. We'll get their measurements for helmets. We may also be able to get bike locks at wholesale price.

Dave Rittenberg – There was a vaccine clinic at Brother Francis Shelter and they vaccinated half of their guests.

Lindsey Hajduk – in chat box - NeighborWorks Alaska is co-hosting a Mayoral Candidate forum next week (with many other nonprofits). It'll be focused on trails, parks, climate, economy. Please join us!
<https://www.facebook.com/events/791520654780295> WEDNESDAY, FEBRUARY 17, 2021 AT 5 PM AKST
– 6:30 PM AKST

Community Comments

Elections are next month.

Adjourn 9:04 pm

FAIRVIEW COMMUNITY COUNCIL

General Membership Meeting Agenda

September 9, 2021 — 7:00 - 9:00 PM

ZOOM: <https://us02web.zoom.us/j/84049110076> (see more details below)

Please note: General Membership meetings are recorded and may be live-streamed on social media by entities unaffiliated with the Fairview Community Council

Call to Order

- Welcome and Introductions
- Approval of Agenda
- Approval of August 2021 General Membership Meeting Minutes

Elected Officials

- Senator Tom Begich
- Representative Zack Fields
- Anchorage Assembly member Christopher Constant
- Anchorage School Board member Dora Wilson

Standing Reports

- President Allen Kemplen
- Treasurer Sharon Chamard

Presentations

- DoT design and Build Interchange on Seward Highway Update – *Renee Whitesell*

Old Business

- Resolutions
 - Earl and Muriel King Park resolution

New Business

- Proposed expansion of innovation zone

Report and Updates

Community Comments/Concerns

Adjourn

Information about Zoom

At the March 2020 General Membership Meeting, there was discussion about moving to a different method for future meetings because of the importance of avoiding gatherings during this time of COVID-19, and an ad hoc committee was created to explore options. The Fairview Recreation Center was closed the next day. Subsequently, the ad hoc committee recommended that we use a virtual meeting interface application called Zoom.

Zoom is used quite a bit in business settings and is fairly user-friendly. It can be used on smart phones, tablets, laptops, and desktops. But it has its limitations with respect to bandwidth. In ordinary times, it is possible to see the faces of other meeting participants (assuming everyone has a webcam), but given the recent massive increase in use of the internet the guidance has changed, so we ask meeting participants to do the following:

- Do not use your video camera
- Make sure your microphone is muted

These will be the default settings when you join the meeting, so don't worry if you're not sure how to do this.

Also, we will be using the chat box feature in Zoom for asking questions and for making comments, although if anyone really want to speak, they can request this in the chat box and will then be "unmuted." This may sound a bit harsh, but it's the virtual meeting equivalent of raising your hand and being recognized by the chair.

How Do I Access Zoom?

- 1) If you want to connect using a smartphone or some other device connect to the internet, go to this URL:
<https://us02web.zoom.us/j/84049110076>
- 2) If you are on a regular phone and want to connect using audio only, there are several dial-in numbers. After you're connected, enter 840 4911 0076# on your keypad.

+1 669 900 9128 US (San Jose)

+1 346 248 7799 US (Houston)

+1 301 715 8592 US

+1 312 626 6799 US (Chicago)

+1 646 558 8656 US (New York)

+1 253 215 8782 US
- 3) If the audio on your internet-connected device is not working properly (usually because of how your speakers are configured), you can try also dialing in on a regular phone to get the audio.

09.09.2021



36th avenue

INTERCHANGE

Seward Highway: 36th Avenue Interchange Fairview Community Council Meeting



PROJECT HISTORY AND BACKGROUND

- Midtown Congestion Relief Planning and Environmental Linkages (PEL) Study concluded in 2020 and recommended 5 projects
- 36th Avenue Interchange is the first project moving forward
- Interchange was studied in 2013-2015 but did not progress
- This is a fresh design and environmental permitting effort



WHY WAS THE 36TH INTERCHANGE SELECTED? — — — — —

- PEL Study recommended at-grade intersection improvements to construct frontage roads and implement two-phase signals
- Improved traffic flow on Seward Highway in short-term (modelled through 2028), but still had delays and safety issues associated with at-grade crossings (albeit much less than current)
- Full grade-separated interchange still needed to fully improve intersection past modelled 2028 volumes, improve safety, reduce delay above LOS F
- Concern that full interchange would be needed immediately on completion of at-grade project, creating several years of consecutive construction and rework of only recently completed investments
- Statewide Planning chose to move to full interchange immediately to overcome this concern and responsibly program constrained funds in STIP



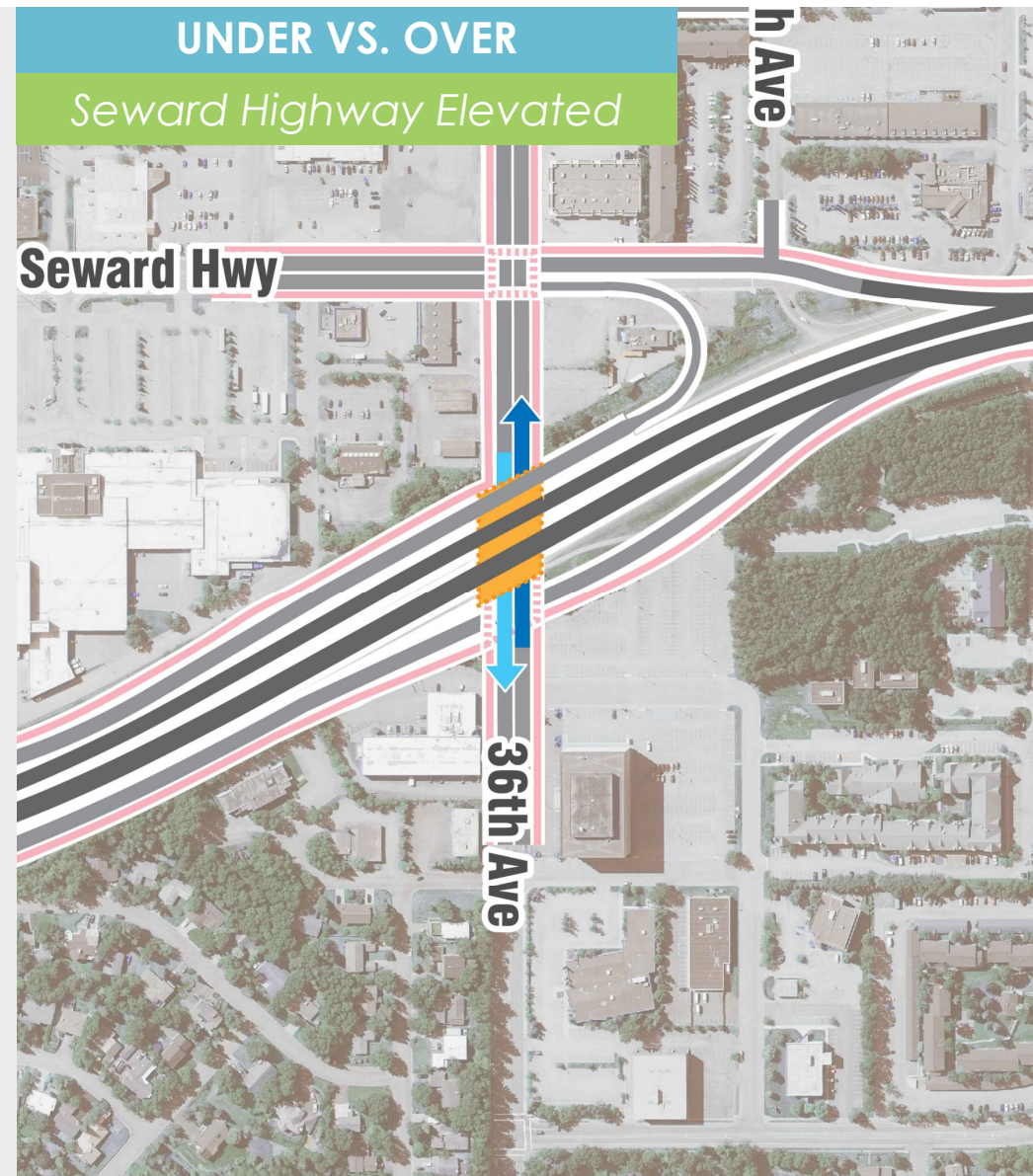
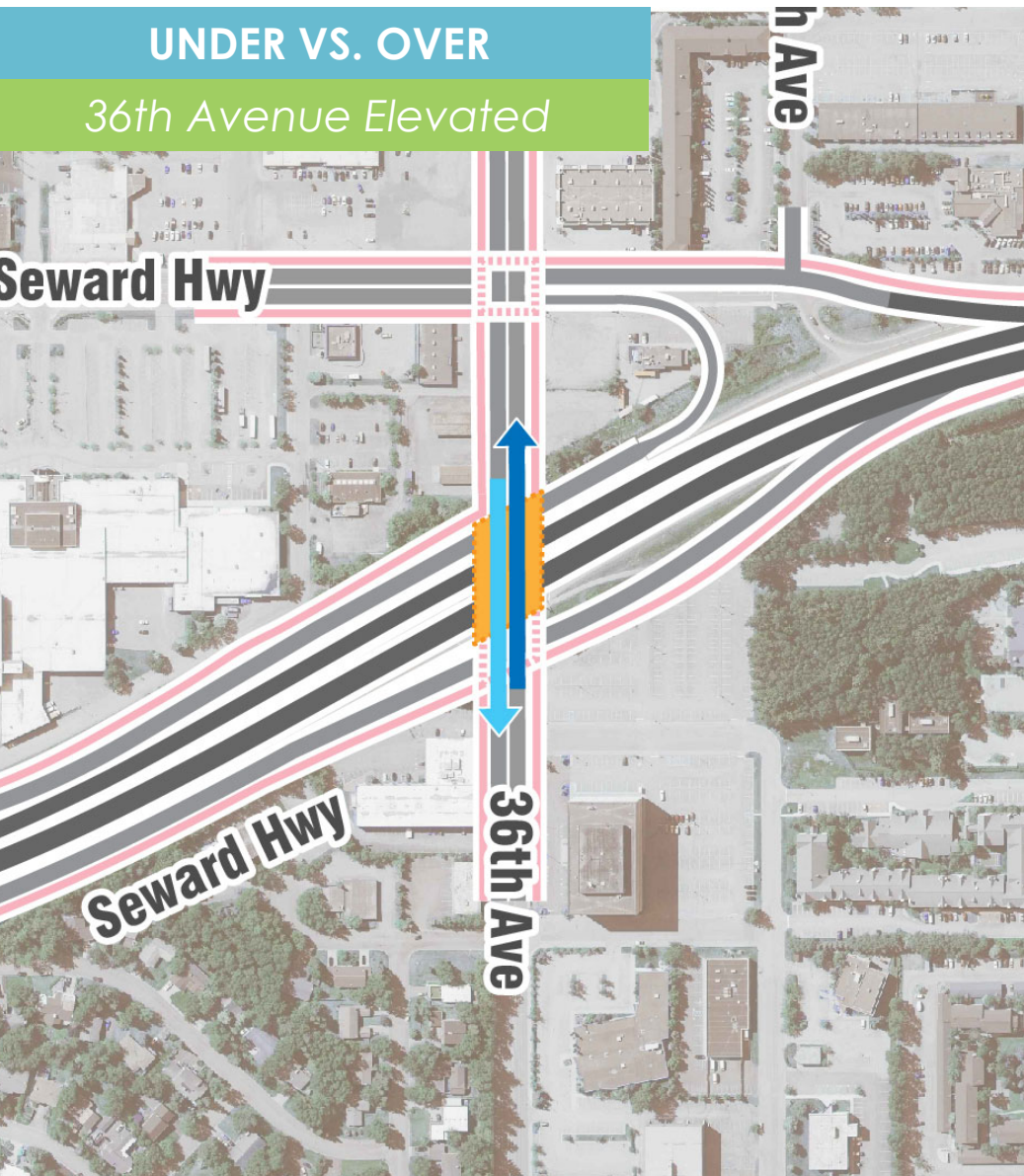
PROJECT STATUS — — — — —

- **Working on most promising alternatives:** Two loop ramp concepts
- **Refining vertical and horizontal geometry:** Minimizing vertical impacts
- **Initiating noise study:** 2022
- **Open House #2:** scheduled for late 2021

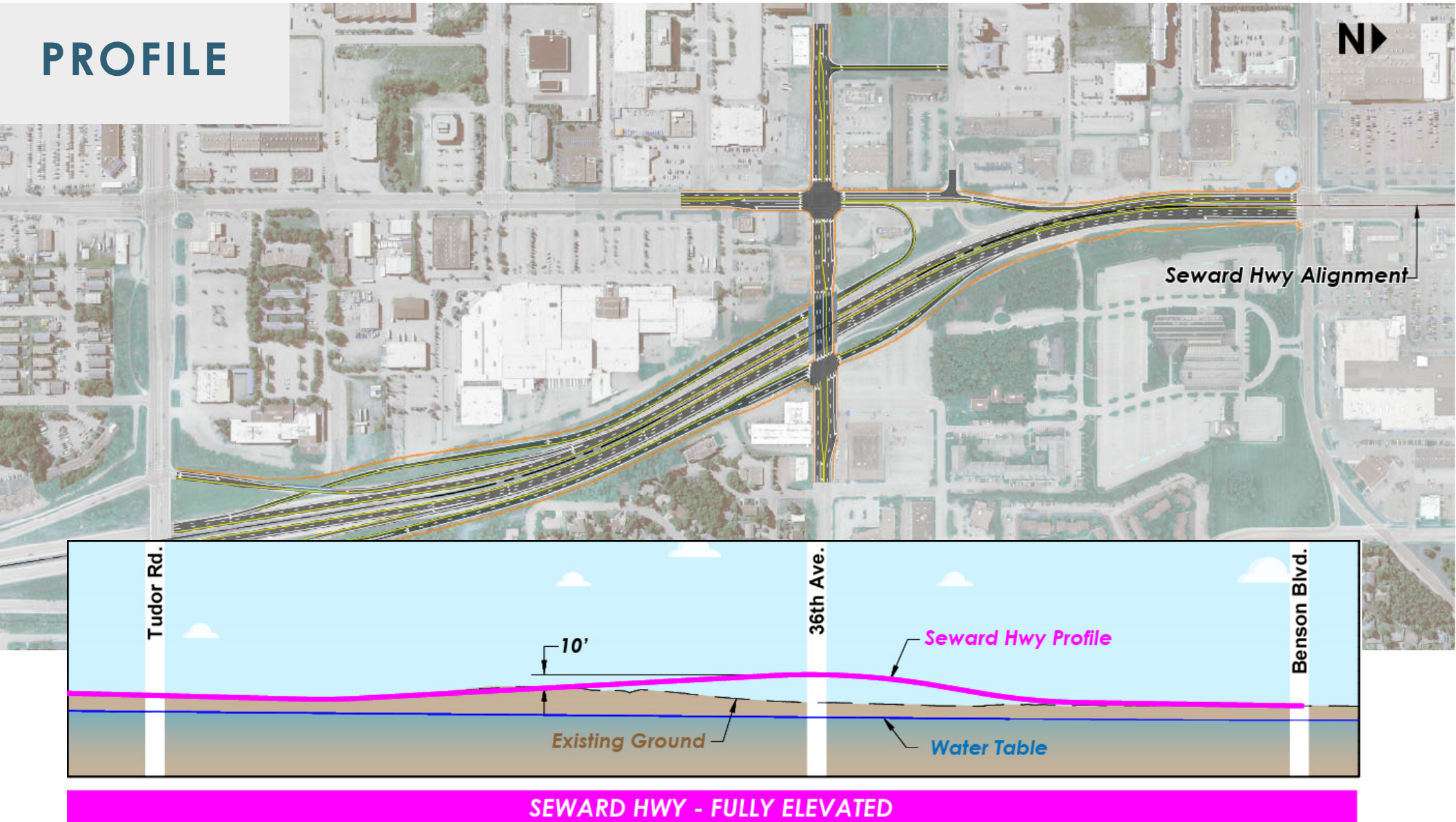
Everything at 36th Avenue is being designed to be forward compatible with a depressed freeway at Benson Boulevard, Northern Lights Boulevard, and Fireweed Lane, as recommended in the

PEL Study

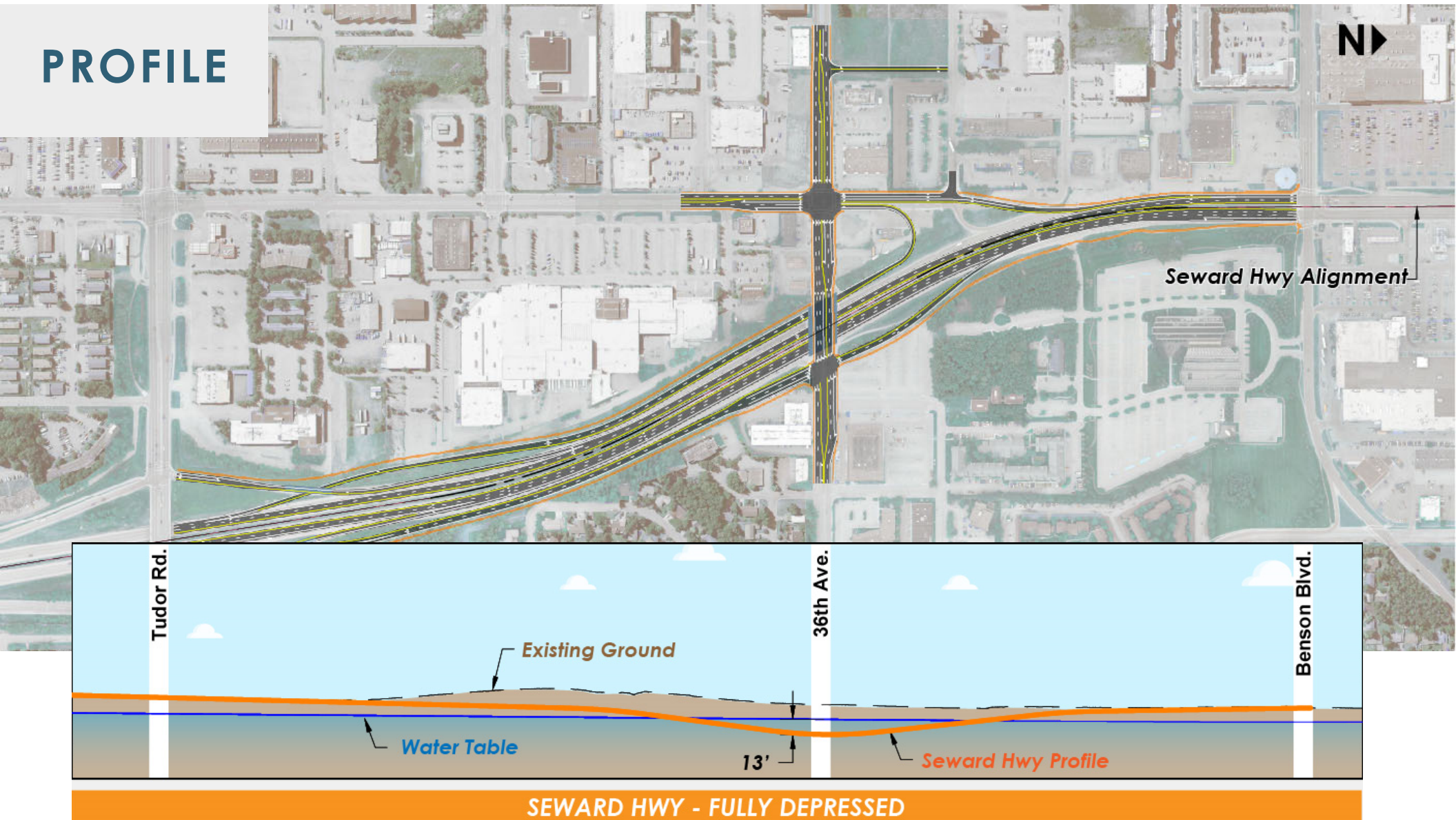




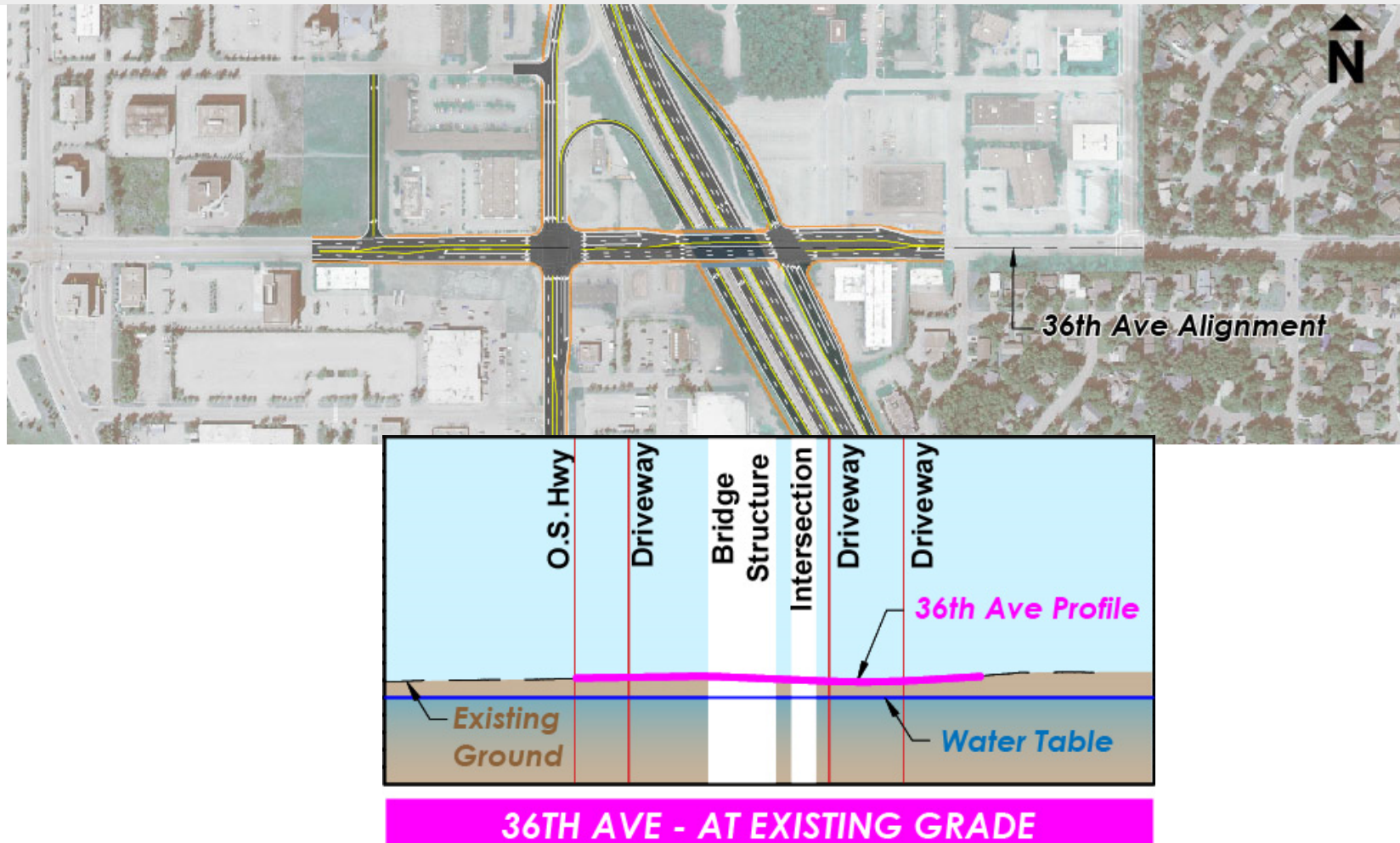
PROFILE



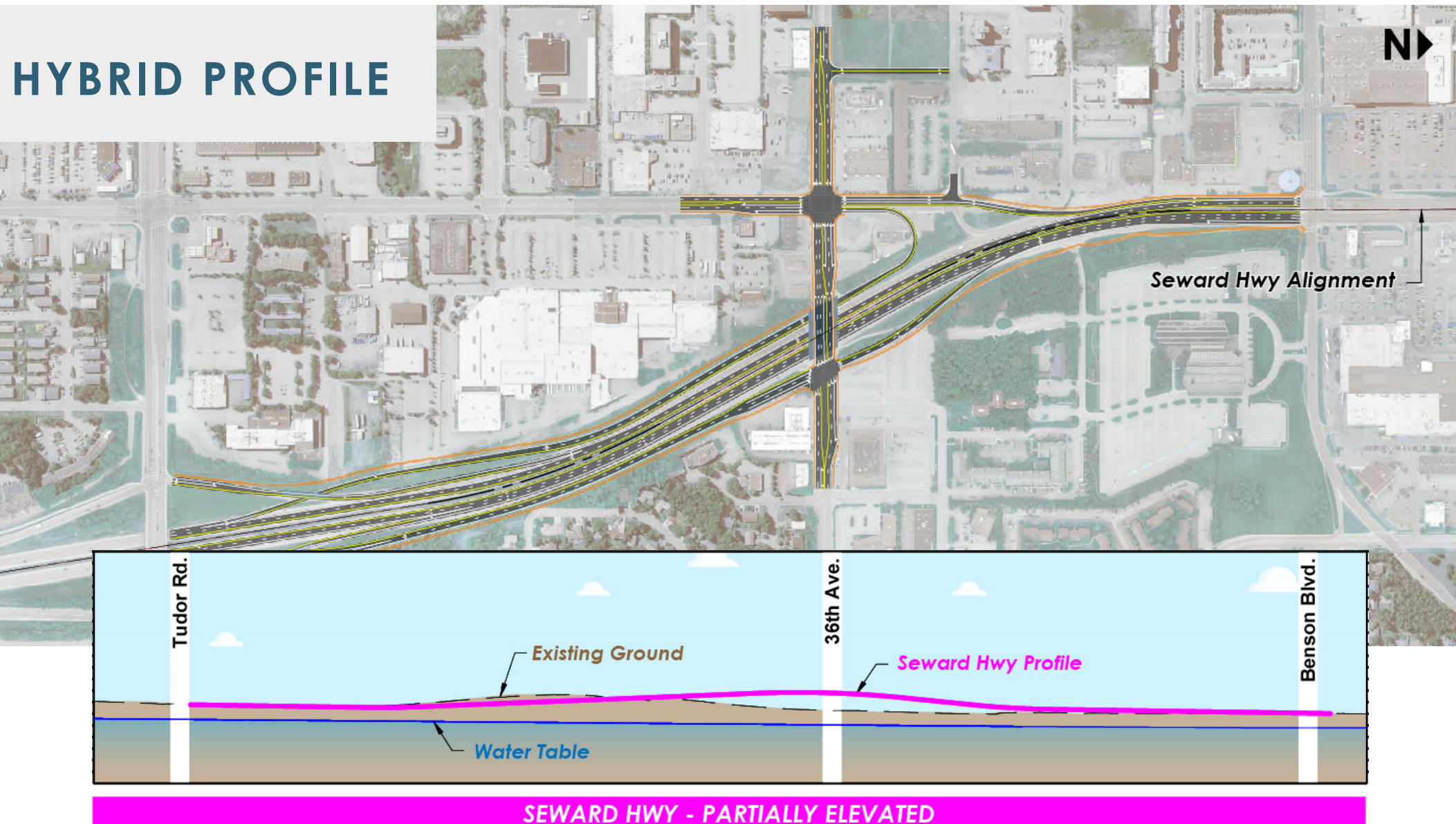
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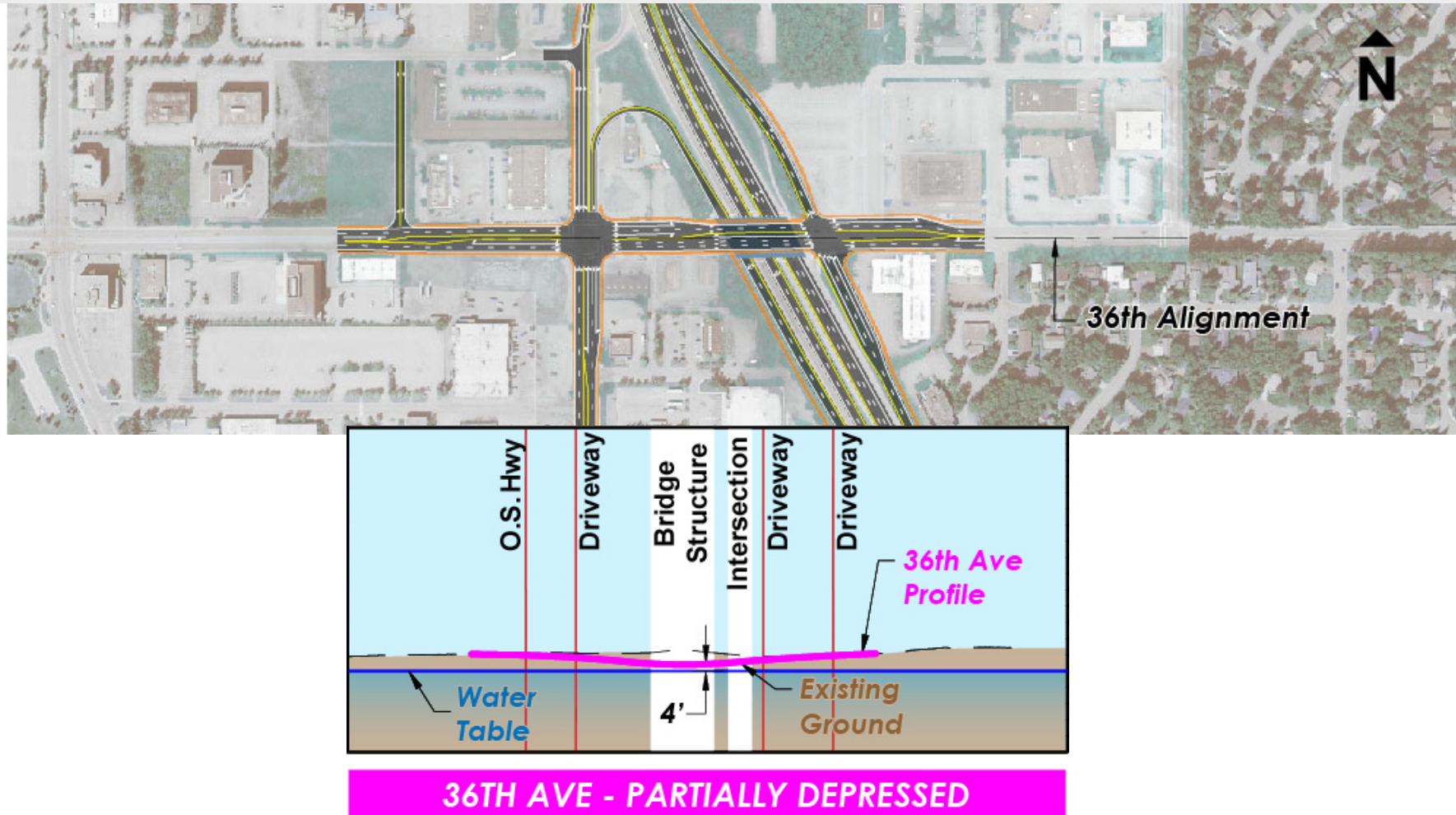
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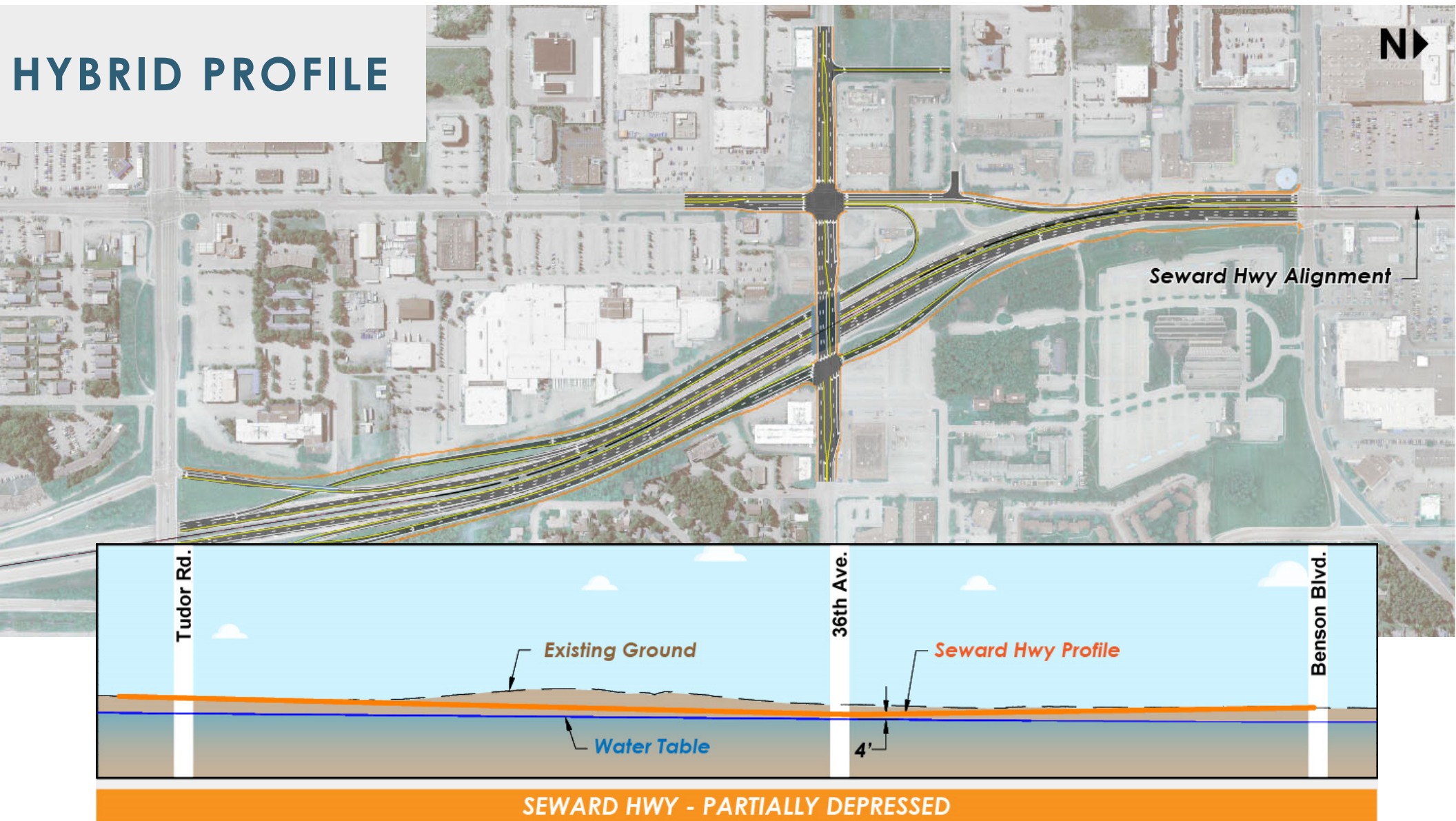
HYBRID PROFILE



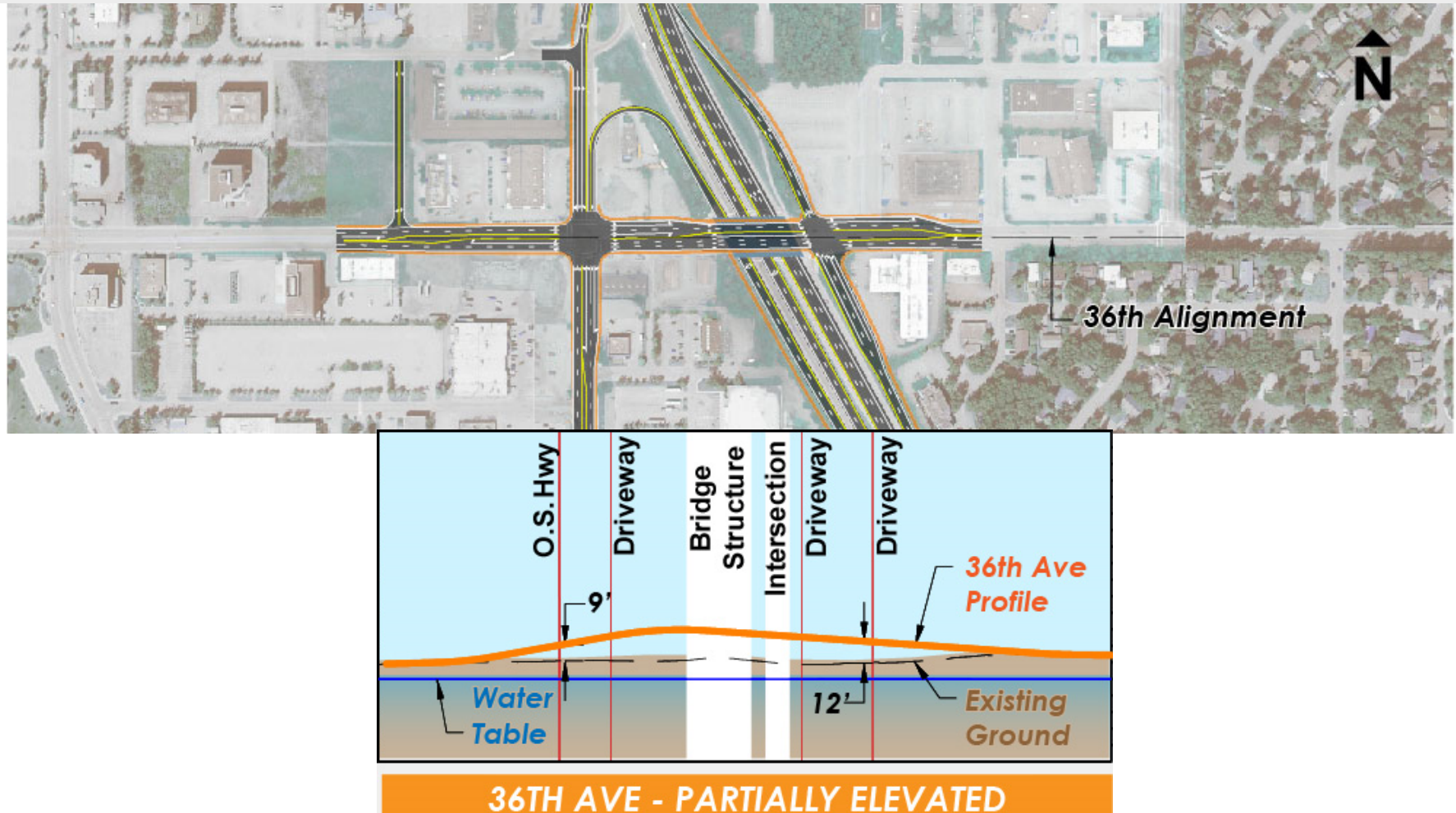
HYBRID PROFILE



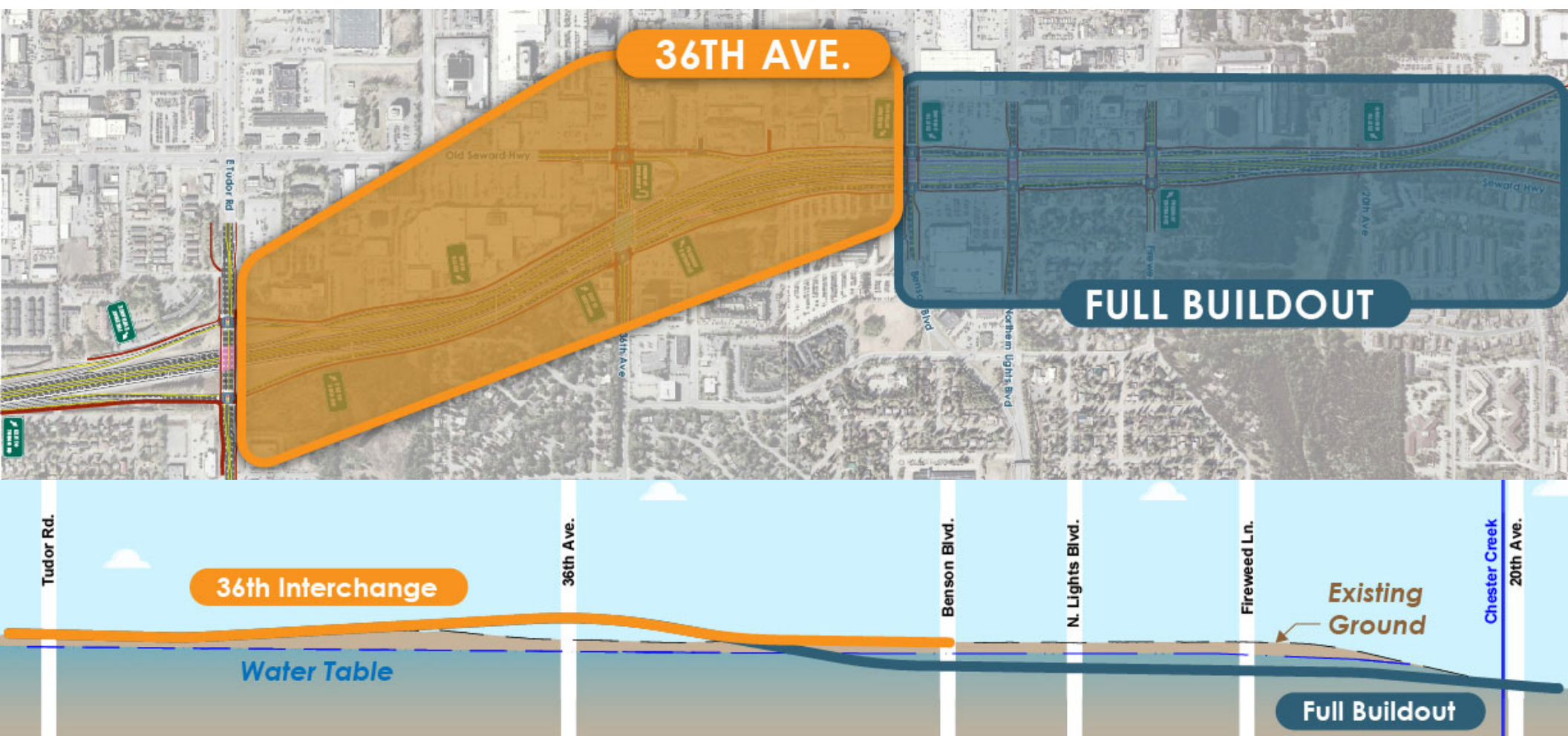
HYBRID PROFILE



HYBRID PROFILE



FULL BUILDOUT



OBSERVATIONS

- **Water Table:** high water table will require intersection to be pumped continuously (both groundwater and stormwater). Pumping volumes are significant and dewatering impacts could extend beyond the project area (currently being evaluated)
- **Contamination:** There is contamination on a property near the intersection, which will require treatment prior to discharge
- **Visual:** The entirely elevated highway is approximately 10 feet above the existing ground level between Tudor Road and 36th Avenue. Visual renderings are going to be prepared to show indicative views for upcoming open house
- **Noise:** Noise Study methodology has been developed and is currently under review. There are 7 residential properties that are “first row receivers” (immediately next to the project area)
- **Driveways:** Driveways on 36th Avenue are very close to the intersection, which create challenges with elevating or depressing 36th Avenue to reduce the height of an overpass



SCHEDULE

Task	Date
PEL Study	2017-2020
Public Involvement	Ongoing
Preliminary Environmental & Design	2020 - Ongoing
Final Design/ROW Acquisition	TBD
Construction	TBD





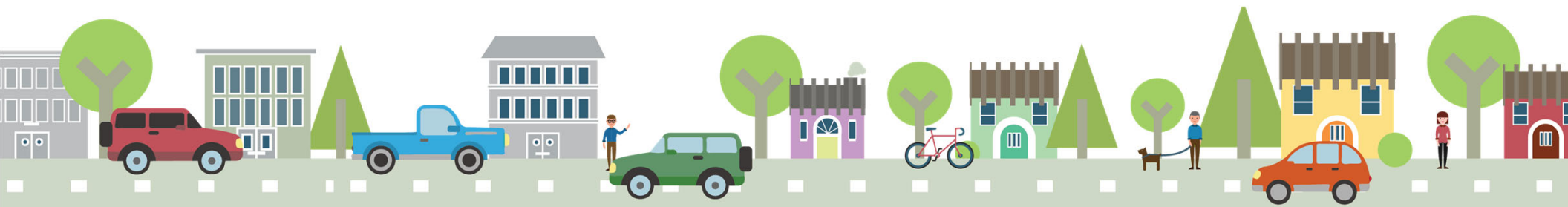
36th avenue INTERCHANGE

Thank You

Questions or Comments?

36thinterchange@dowl.com

www.36thinterchange.com



From: [Katie Conway](#)
To: [Sean Baski](#); [Farr, John D \(DOT\)](#)
Cc: [Steven Noble](#); [Renee Whitesell](#); [Rachel Steer](#); [Morgan McCammon](#)
Subject: 36th Ave Interchange Update at Fairview Community Council
Date: Thursday, September 9, 2021 9:14:58 PM

Sean Baski, Katie Conway, and Morgan McCammon attended the Fairview Community Council meeting this evening, Thursday, September 9. Sean provided an update on the 36th Avenue Interchange project, specifically focusing on early engineering to explore the profile of the interchange.

26 people attended the meeting. Notable attendees included Senator Tom Begich, Representative Zach Fields, Assemblyman Chris Constant, and Anchorage School Board member Dora Wilson.

General meeting notes:

- SJ Klein noted the Fairview Business Association is restarting out of frustration with the municipality's response to housing and homelessness needs. The group initially organized ~15 years ago when responding to issues or needs relating to DOT&PF.
- There was intense conversation and debate on COVID-19 mitigation (masking, vaccinations) and homelessness solutions. These two conversations took the lion's share of the meeting time and caused the meeting to be extended. Sean's presentation came after these conversations. There did not seem to be much appetite for discussion.

Questions/comments included:

S.J. Kline: Noted he'd sat on the business advisory group and this project was a less controversial one in the package.

L. Kleinfeld: asked about pedestrian crossings. Sean noted that the new design will separate Seward Highway traffic from 36th Avenue traffic. The project is about safety and connecting the community. The new design will make it safer and more comfortable for pedestrians.

L. Kleinfeld: asked about pedestrian crossings at Benson and Northern Lights. Sean replied this project does not include those intersections, those would be improved in a different project identified in the PEL.

S.J. Kline: asked about potential construction impact on traffic flows through Fairview, especially during rush hour. Sean responded that DOT&PF does their best to keep detours off neighborhood streets. The goal is to keep as many lanes open as possible to best serve the public. DOT&PF also attempts to avoid construction during rush hour.

Thank you,
Katie

Katie Conway
Public Involvement Manager

DOWL

(907) 562-2000 | office
(907) 865-1289 | direct

dowl.com

NORTH STAR COMMUNITY COUNCIL (NSCC)

NSCC ONLINE

- Federation of Community Councils - <http://communitycouncils.org/servlet/content/24.html>
- Facebook - <http://www.facebook.com/groups/northstarcommunitycouncil/>

GENERAL MEMBERSHIP MEETING AGENDA

**This Meeting will be held via ZOOM software.
Instructions to access the Zoom meeting are below.**

Wednesday February 10, 2021 at 7 p.m.

Call to order

- Round of introductions
- Approval of agenda
- Approval of minutes

Reports

Council Officers:

- **President** – Matt Johnson mattj@alaskan.com 907-223-3986
- **Vice-President** – Mark Butler
- **Secretary** – Mike Hondel
- **Treasurer** - Andre Camara
- **FCC Delegate** – Matt Johnson
- **Member at Large** - Susan Miller

Legislative Representatives – Please be **concise (3 minutes each!)**

- **State Senator** Elvi Gray-Jackson (SD-I) Sen.Elvi.Gray-Jackson@akleg.gov
- **State Representative** Harriet Drummond (HD-18) Representative.Harriet.Drummond@akleg.gov

Assembly Representatives - Please be **concise (3 minutes each!)**

- **Meg Zaletel** (District 4 Midtown Anchorage) Meg.Zaletel@AnchorageAK.Gov
- **Felix Rivera** (District 4 Midtown Anchorage) Felix.Rivera@AnchorageAK.gov
- **Kameron Perez-Verdia** (District 3 West Anchorage) Kameron.Perez-Verdia@AnchorageAK.Gov
- **Austin Quinn-Davidson** (District 3 West Anchorage) Austin.Quinn-Davidson@AnchorageAK.gov

New Business

Seward Highway: 36th Avenue Interchange Project - Department of Transportation and Public Facilities – Seward Highway: 36th Avenue Interchange Project Renee Whitesell from DOWL will provide an update, including details about an upcoming open house.

Eradication of Invasive species *Prunus Padus* (known as bird cherry, hackberry, hagberry, or Mayday) tree in the Chester Creek Greenbelt

Candidates for Mayor – Opportunity to Speak

- **Bill Falsey for Mayor**
- **Others**

Candidates for School Board – Opportunity to Speak

NSCC general membership meetings are the 2nd Wednesday of each month (Sept – May) at 7pm at North Star Elementary School, 605 West Fireweed Lane, in the school Library. The Municipality of Anchorage complies with Title I of the Americans with Disabilities Act of 1990. Reasonable accommodations will be made available to qualified individuals upon request. Contact the Community Councils Center at (907) 277-1977 at least 10 days in advance of need.

Old Business (there may be no updates on items below)

- Anchorage Reads Program
<http://www.anchoragelibrary.org/about/about-apl/library-news/anchorage-reads-2021-launches-february-1st/>
- Spenard Road: Minnesota to Benson
- Other

Announcements

Adjourn at or before 8:59 p.m.

Join Zoom Meeting

<https://zoom.us/j/95683486767?pwd=WUVZSHZhOXdGRG9NQWhQZ1ZTQy93QT09>

Meeting ID: 956 8348 6767

Passcode: 046224 Dial by your location

+1 253 215 8782

This is a recurring meeting on the second Wednesday of every month. Please download and import the following iCalendar (.ics) files to your calendar system.

Monthly: <https://zoom.us/meeting/tjErf-qspj0tHNMugGGNv-4PNBZIFCkblnLz/ics?icsToken=98tyKuCtqDMpHtyTtx-ARowMBIqgb-jxiCFago1wiRjCJA5FL1fGGMB4G5RcG9rc>

NORTH STAR COMMUNITY COUNCIL (NSCC)

NSCC ONLINE

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GENERAL MEMBERSHIP MEETING AGENDA

**This Meeting will be held via ZOOM software.
Instructions to access the Zoom meeting are below.**

Wednesday September 8, 2021 at 7 p.m.

Call to order

- Round of introductions
- Approval of agenda
- Approval of minutes

Reports

Council Officers:

- **President** – Matt Johnson mattj@alaskan.com 907-223-3986
- **Vice-President** – Mark Butler
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New Business

1. 36th Ave. Interchange Project Update

Sean Baski, DOT&PF Project Manager, and Renee Whitesell, DOWL Senior Project Manager, will provide an update on the project, focusing on preliminary engineering work to support recommendations around the interchange profile.

2.

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Old Business *(there may be no updates on items below)*

1. Status of Electrical Undergrounding Project – Chugach Electric
2. Elimination of Mayday (Prunus Padus) trees from the Chester Creek Greenbelt
3. Status of Fireweed Lane Improvement Project

Announcements

The Spenard Road Rehabilitation project is hosting an Open House (outdoor and in person) on Monday September 27th from 5 to 7 p.m. at the Nave (Church of Love) parking lot at 3502 Spenard Road. Meeting materials (including a prerecorded presentation) will be posted to the project website (www.spenardroad.com) by September 24th.

Adjourn at or before 8:59 p.m.

Join Zoom Meeting

<https://us06web.zoom.us/j/89782545798?pwd=MU1yMnhOYk52amZiZTRJTWdxT21NQ09>

Meeting ID: 897 8254 5798

Passcode: 746946

Dial by your location

+1 253 215 8782 US (Tacoma)

Meeting ID: 897 8254 5798

Passcode: 746946

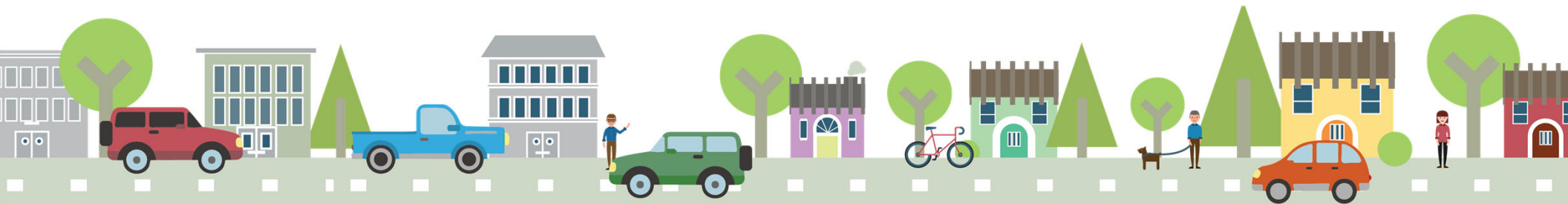
09.08.2021



36th avenue

INTERCHANGE

Seward Highway: 36th Avenue Interchange North Star Community Council Meeting



PROJECT HISTORY AND BACKGROUND

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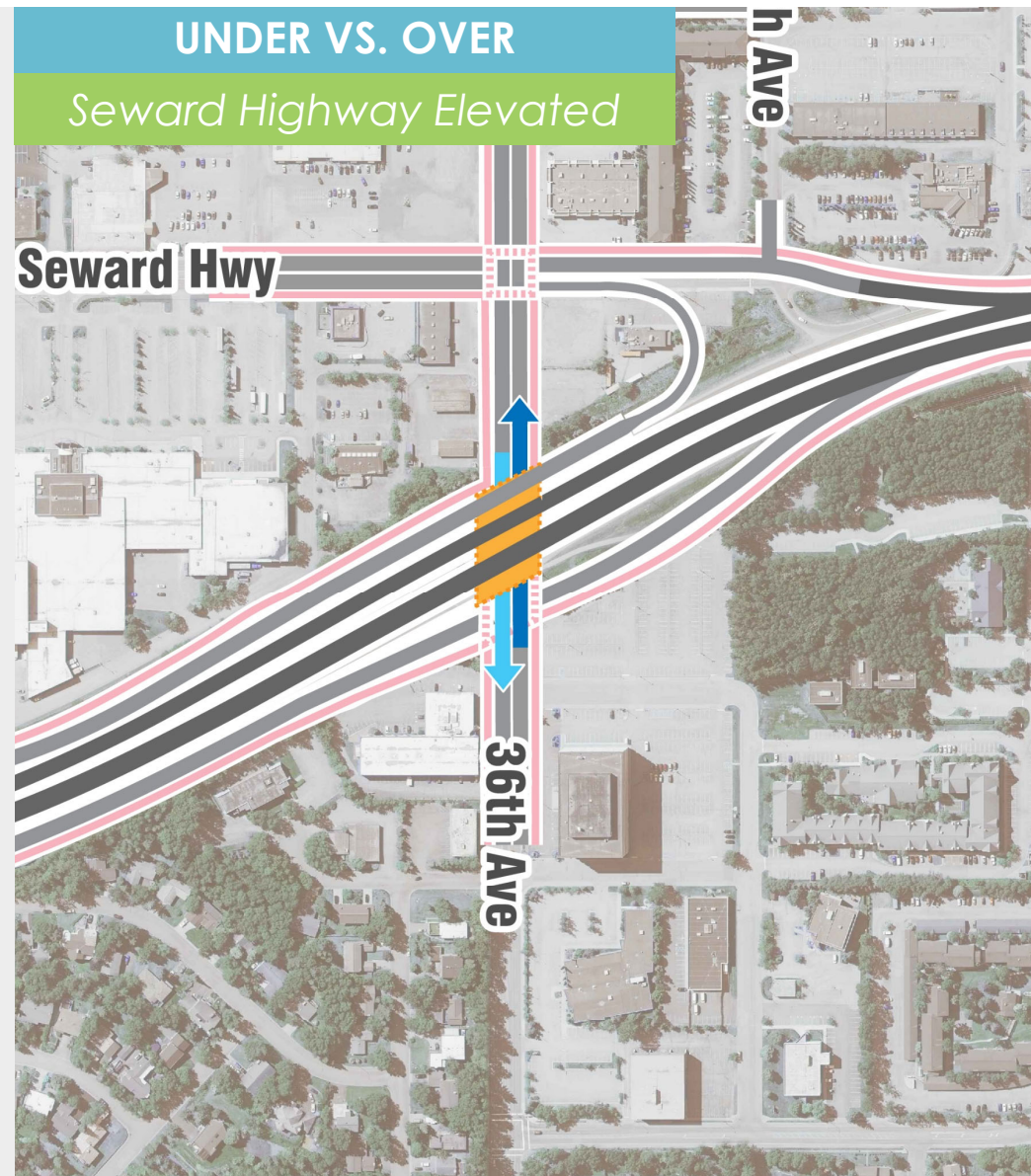
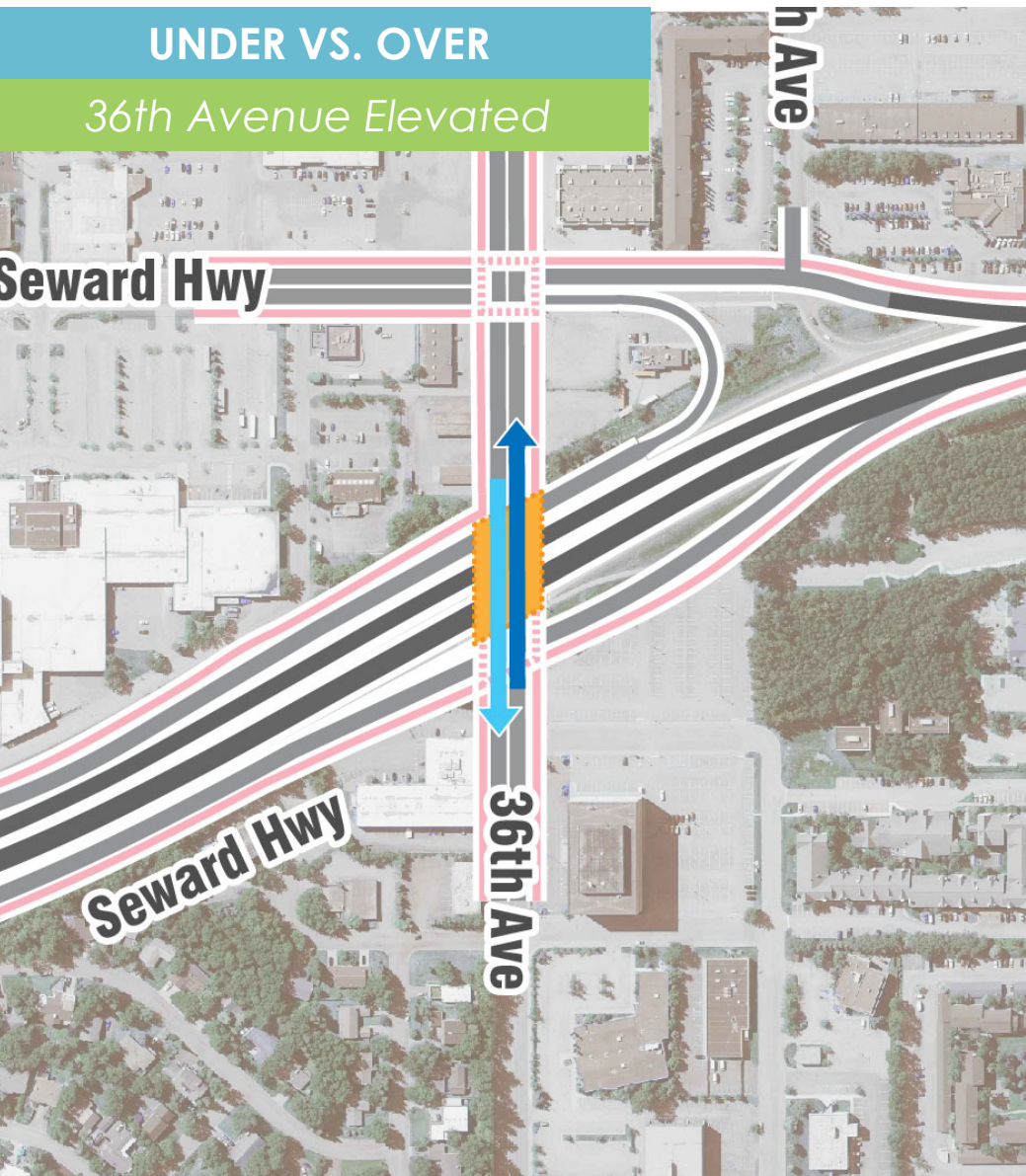
PROJECT STATUS — — — — —

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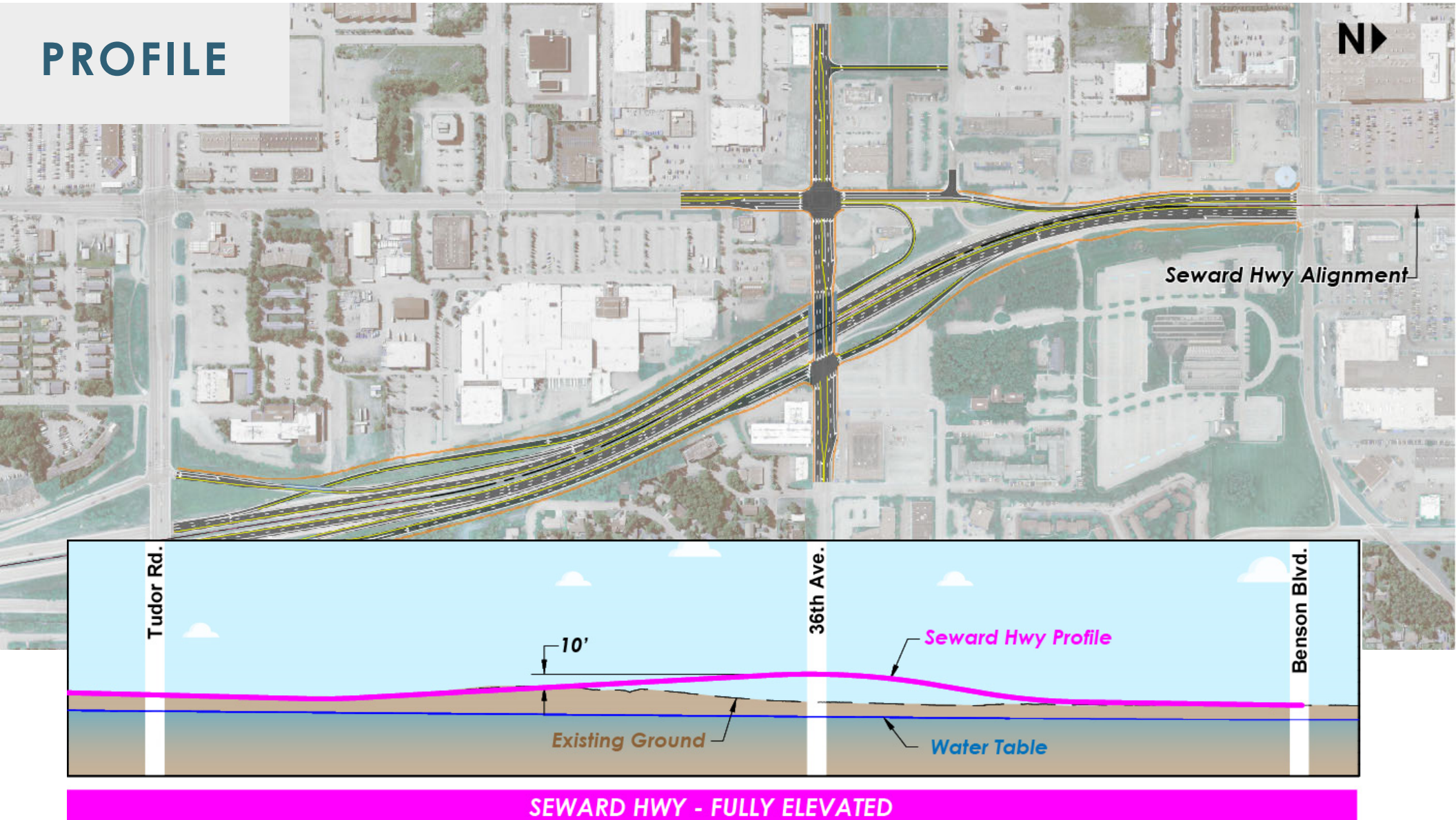
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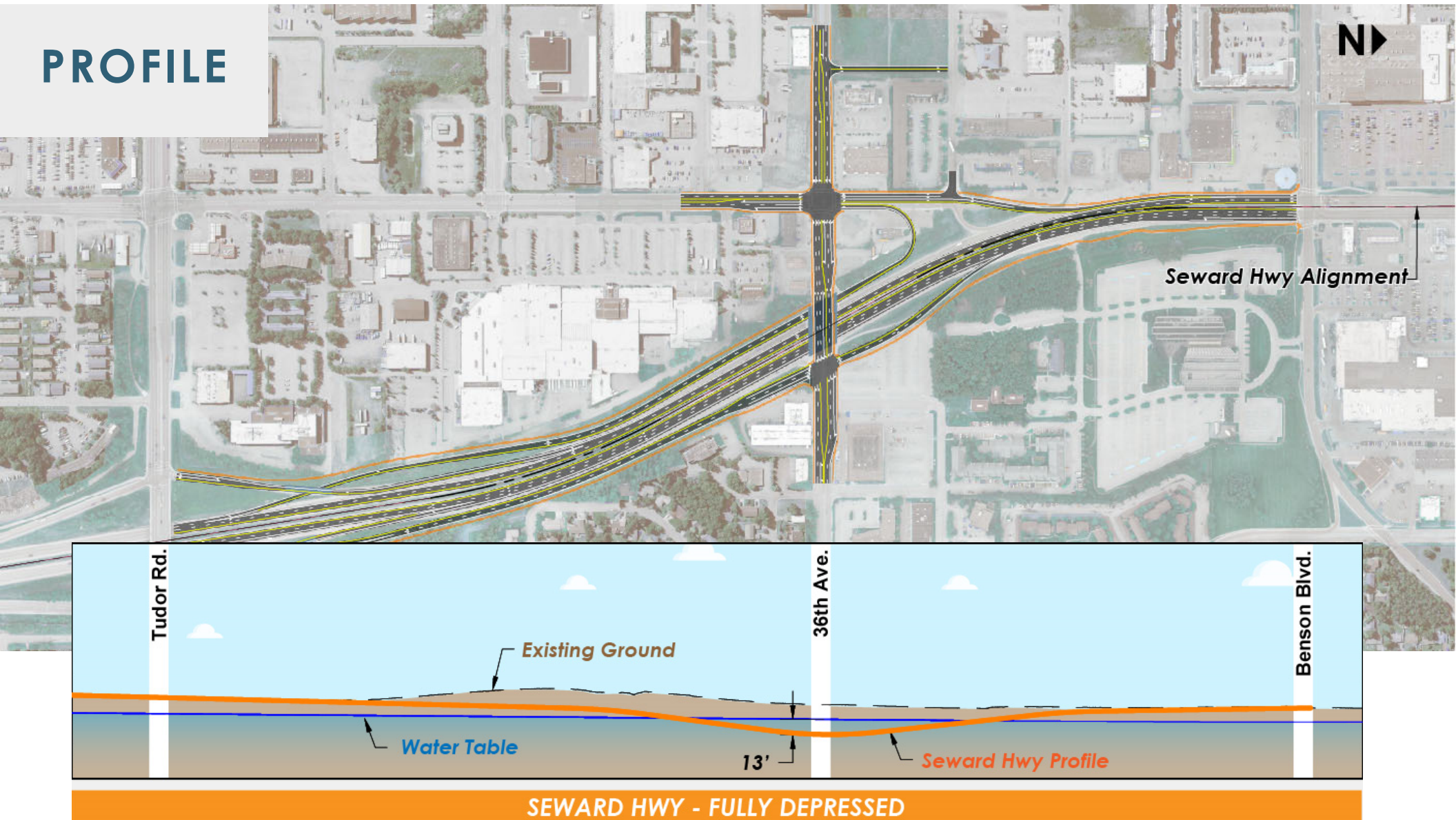




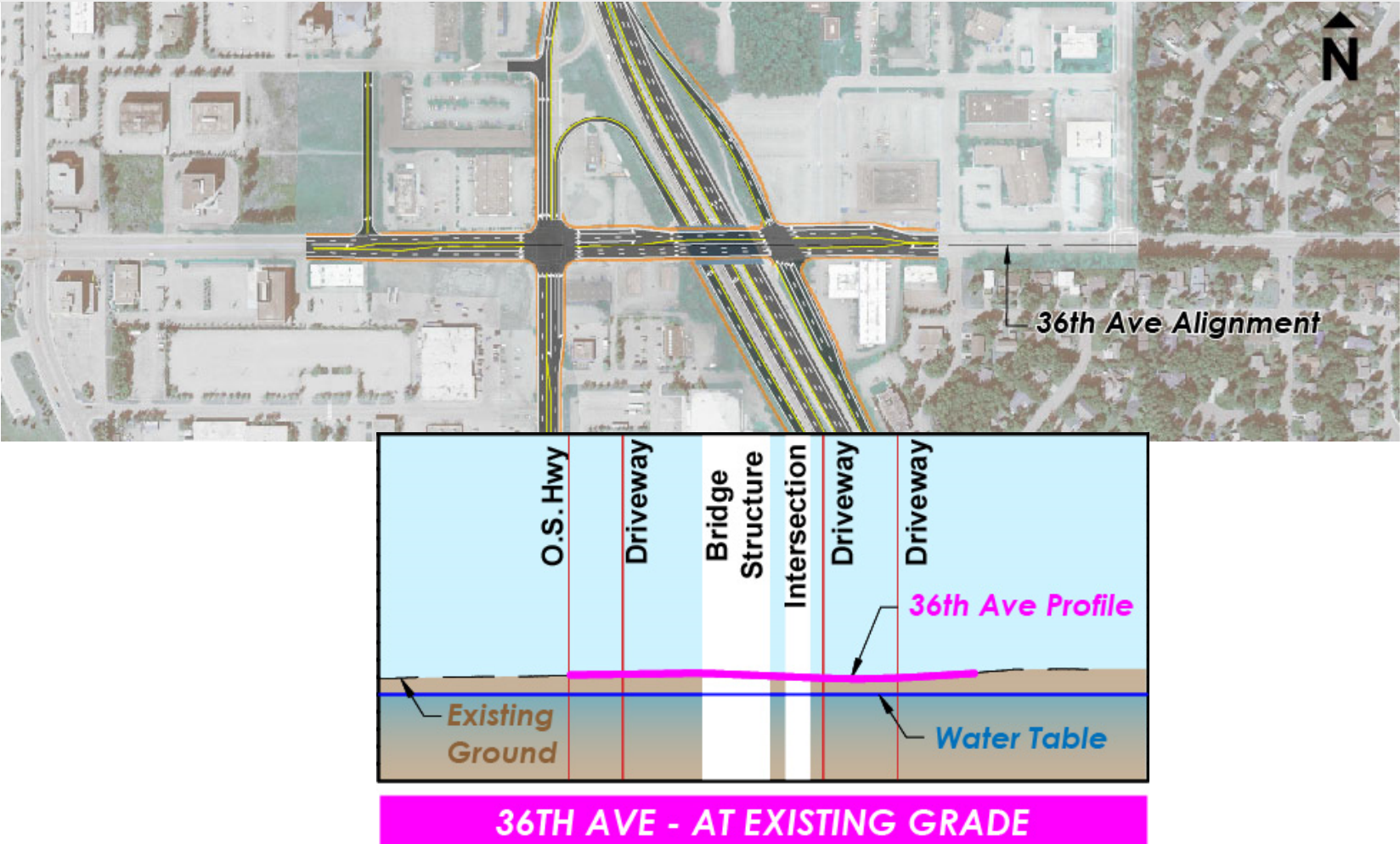
PROFILE



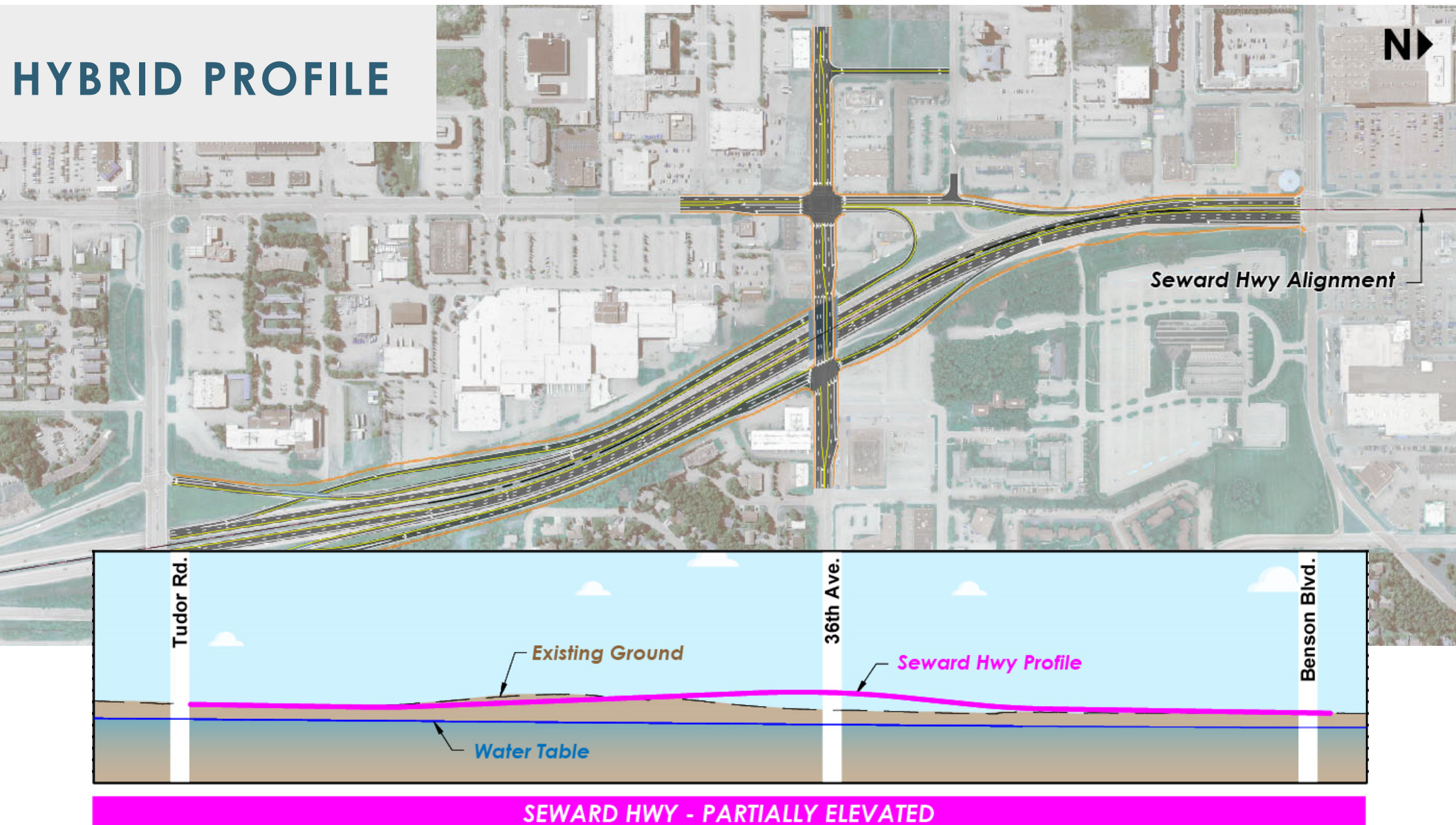
PROFILE



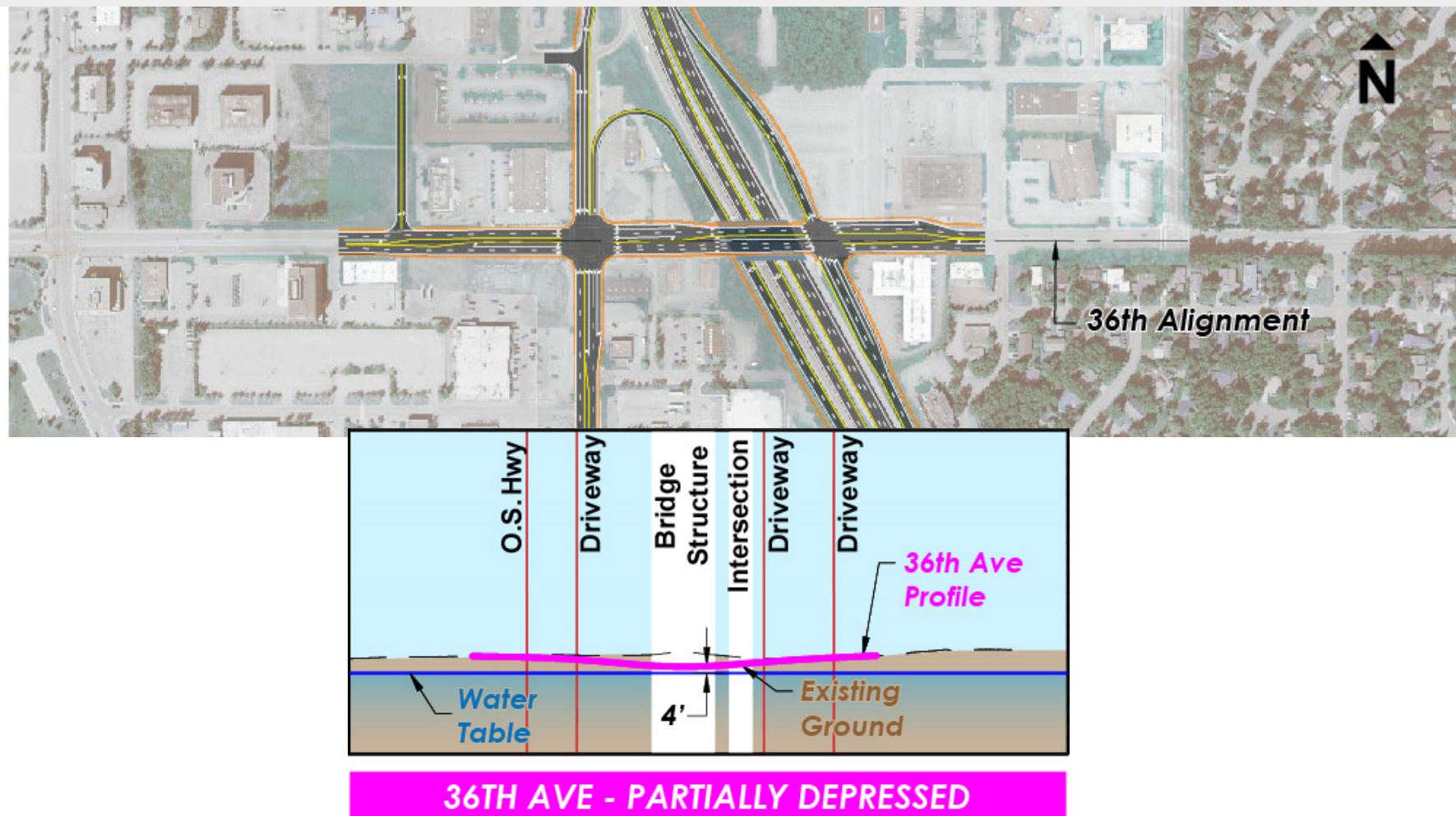
PROFILE



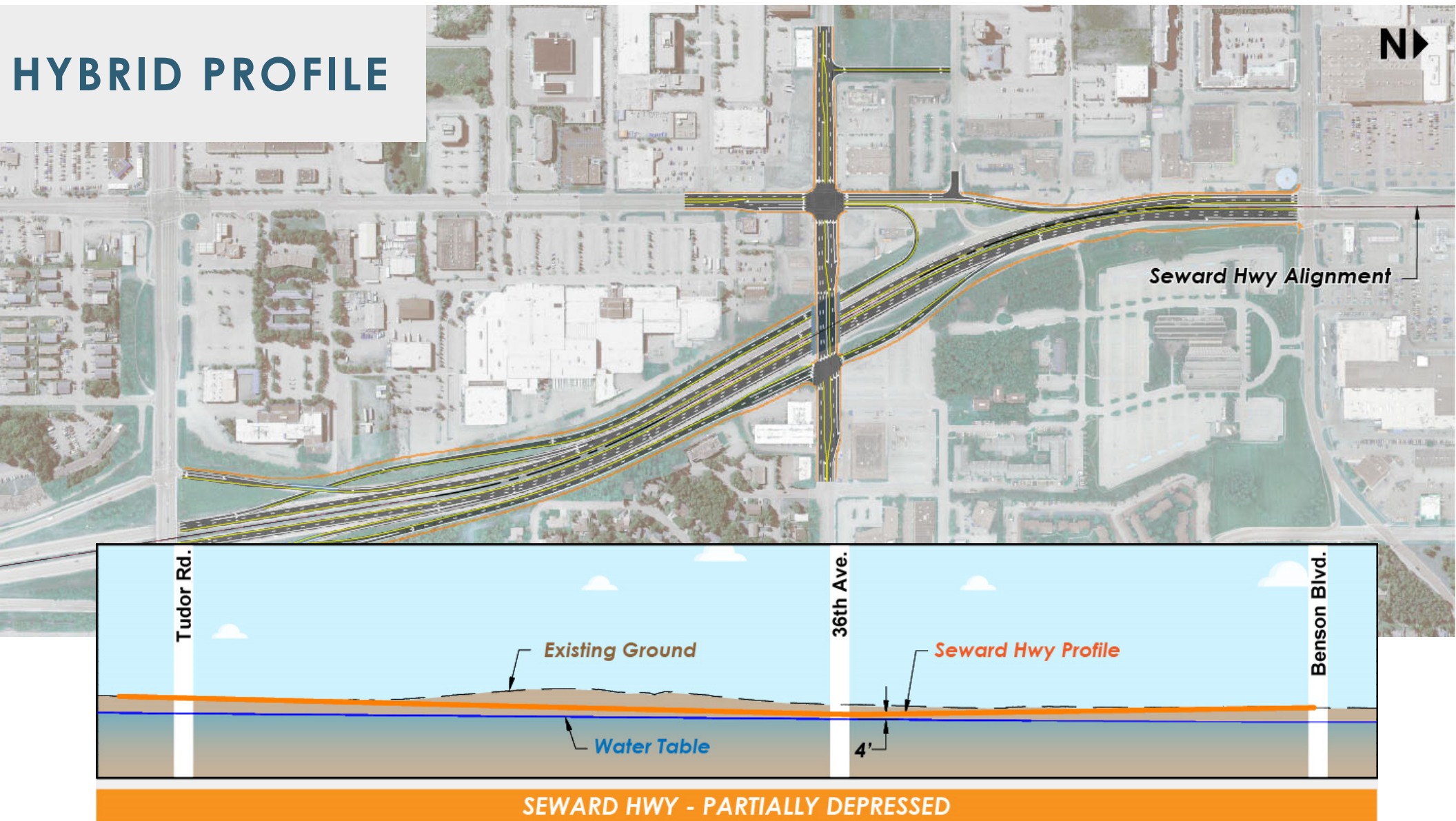
HYBRID PROFILE



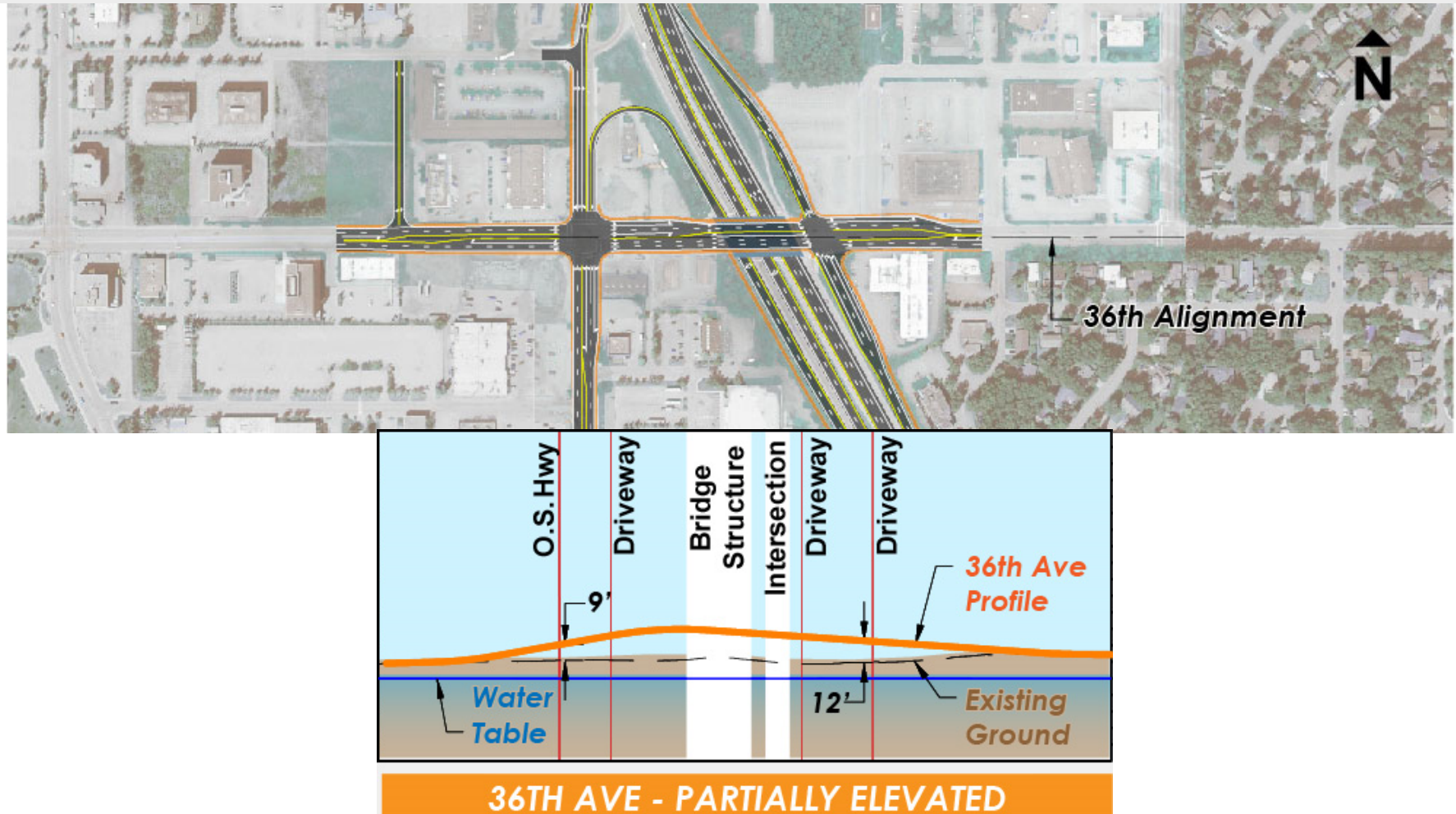
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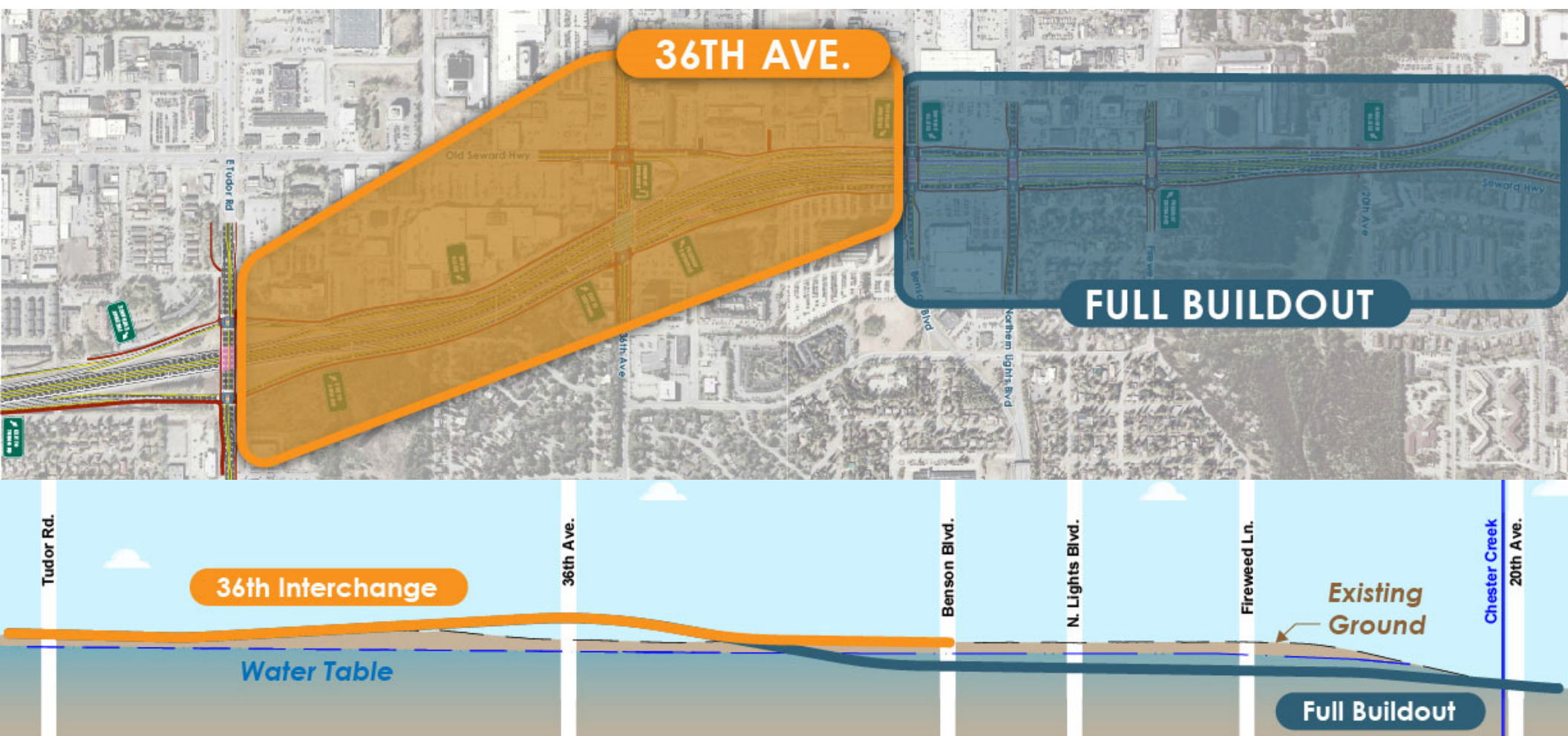
HYBRID PROFILE



HYBRID PROFILE



FULL BUILDOUT



OBSERVATIONS

- **Water Table:** high water table will require intersection to be pumped continuously (both groundwater and stormwater). Pumping volumes are significant and dewatering impacts could extend beyond the project area (currently being evaluated)
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SCHEDULE

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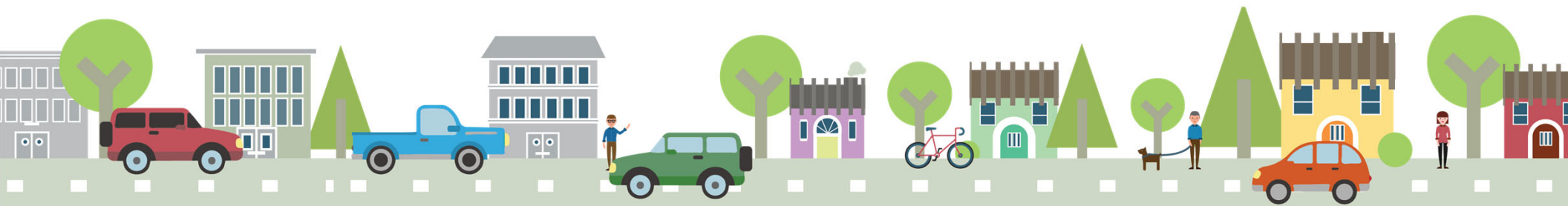
36th avenue INTERCHANGE

Thank You

Questions or Comments?

36thinterchange@dowl.com

www.36thinterchange.com



From: [Rachel Steer](#)
To: [Sean Baski](#); [Farr, John D \(DOT\)](#)
Cc: [Steven Noble](#); [Katie Conway](#); [Morgan McCammon](#); [Renee Whitesell](#)
Subject: North Star Community Council - 9/8/21
Date: Wednesday, September 8, 2021 8:28:24 PM

Sean Baski and Rachel Steer attended the North Star Community Council (NSCC) meeting on Wednesday September 8 to give an update on the 36th Avenue interchange project. Seven members of the NSCC were in attendance at the meeting.

In addition, the following state and local representatives were present:

- Sen. Elvi Gray-Jackson
- Assembly Rep. Felix Rivera
- Assembly Rep. Meg Zalatel
- Assembly Rep. Austin Quinn-Davidson
- Margo Bellamy, Anchorage School District Board President

Sean focused his presentation on engineering and environmental work that has been completed over the past year.

He reviewed some of the factors under consideration as the project team works to select a profile for the interchange including:

- High water table
- Contaminated soils
- Visual impacts
- Noise impacts
- Driveways in close proximity to the interchange

Sean reviewed the project schedule and indicated the next open house would be held in late-2021. There were no questions from the council.

Rachel

Rachel Steer
Public Involvement

DOWL

(907) 562-2000 | office
(907) 865-1125 | direct
(907) 301-5526 | cell

dowl.com

Rogers Park Community Council

7pm Monday, July 13, 2020

Zoom Meeting

Updates and Notices

Anchor Park, College Village, Rogers
Park and Woodside East

I. Committee Updates:

A. Crime and Safety Committee:

1. Proposed Treatment Center/Homeless Shelter at Golden Lion Hotel on 36th
2. Homeless Camp Abatement - Update
3. Looking for someone to Chair the Committee
4. Continue to report Homeless Camps at [ANCWORKS CAMP REPORTING](#)

B. Gardening and beautification of RPCC- Planting Date TBA

RPCC Board approved \$500 for plants and community summer event

C. Midtown Congestion Relief (MCR) Bob Butera, Dave Evans, Jim Wright and/or Steve Noble (DOWL)[PEL Study is available on the RPCC website or click this link](#)**

D. Nominating Committee: Peter Mjos, Chair

Nominees to stand for Election at our September 14th 2020 Council meeting:

Steve Lindbeck, President

Linda Chase, Vice President

Treasurer, Ric Wilson

Secretary, Mark Foster

E. ADU Committee: Dan Rosenberg, Chair – update

II. Representative Reports:

A. Assembly Report(s): Meg Zaletel & Felix Rivera

Zoom meeting July 9th – see next page for a statement & Zoom link

Regarding proposed Treatment Center at the Golden Lion on 36th Avenue

B. Legislative Report(s): Representative Andy Josephson (HD-17) Representative Harriet Drummond (HD-18) Senator Elvi Gray-Jackson (SD-I)

Rogers Park Community Council: <http://www.communitycouncils.org/servlet/content/28.html>

Federation of Community Councils: <http://www.communitycouncils.org/servlet/content/home.html>

Golden Lion Statement and meeting announcement:

Anchorage has been exploring how to bring a substance misuse treatment center online for many years. Substance misuse has been an underlying and under addressed issue that has strained our social safety net and that has been further exacerbated by the COVID-19 pandemic. At this point, we are in discussions with the administration and have taken questions and concerns to them for response and so that we may move the community conversation forward on Thursday, July 9th. We invite you to join us on Thursday to learn more. Once we all know more, we can better assess the administration's plan for a treatment center and determine if more information is needed, or Golden Lion is an appropriate location, or whether we need to look elsewhere. We hope you will join us in that conversation.

Topic: Midtown Treatment Center Community Meeting

Time: Jul 9, 2020 06:00 PM Alaska

Join Zoom Meeting

<https://us02web.zoom.us/j/82931399731?pwd=OU85REJDVC9mRVZsRFBxTzllVkVSZz09>

Meeting ID: 829 3139 9731

Password: 682433

One tap mobile

+16699006833,,82931399731#,,,,0#,,682433# US (San Jose)

+12532158782,,82931399731#,,,,0#,,682433# US (Tacoma)

Dial by your location

+1 669 900 6833 US (San Jose)

+1 253 215 8782 US (Tacoma)

+1 346 248 7799 US (Houston)

+1 929 205 6099 US (New York)

+1 301 715 8592 US (Germantown)

+1 312 626 6799 US (Chicago)

Meeting ID: 829 3139 9731

Password: 682433

Find your local number: <https://us02web.zoom.us/j/82931399731?pwd=OU85REJDVC9mRVZsRFBxTzllVkVSZz09>

Thanks,

Meg Zaletel, Midtown Assembly Member

meg.zaletel@anchorageak.gov, 907-343-4117(voicemail)

She/her (pronouns)

Dena'inaq elnen'aq' gheshtnu ch'q'u yeshdu. (Dena'ina)

I live and work on Dena'ina land. (English)

From: [Katie Conway](#)
To: [Katie Conway](#)
Cc: [Katie Conway](#)
Subject: MCR: Rogers Park Community Council 7/13/20 meeting recap
Date: Wednesday, November 17, 2021 11:15:03 AM

From: Lemons, Regina E (DOT) <regina.lemons@alaska.gov>

Sent: Tuesday, July 14, 2020 3:56 PM

To: Amundsen, James (DOT) <jim.amundsen@alaska.gov>; Linnell, John R (DOT) <john.linnell@alaska.gov>; Post, David E (DOT) <david.post@alaska.gov>; Vanhove, Todd E (DOT) <todd.vanhove@alaska.gov>; McCarthy, Shannon K (DOT) <shannon.mccarthy@alaska.gov>; Rotkis, Diana L (DOT) <diana.rotkis@alaska.gov>

Cc: Baski, Sean M (DOT) <sean.baski@alaska.gov>; Farr, John D (DOT) <john.farr@alaska.gov>; Gardner, Shawn C (DOT) <shawn.gardner@alaska.gov>; Jackson, Tory <Tory.Jackson@hdrinc.com>; Reese, Jill (DOT) <jill.reese@alaska.gov>; Starzec, James A (DOT) <james.starzec@alaska.gov>

Subject: MCR: Rogers Park Community Council 7/13/20 meeting recap

All,

See below for a meeting recap from Rogers Park Community Council virtual cc meeting held on Monday, July 13th from 7:00-9:00pm via Zoom. Both Sean Baski and Steve Noble from DOWL were in attendance. There were about 60 people in attendance and representatives for the neighborhood include Assemblypersons Rivera and Zelatel and Representative Josephson and Senator Gray-Jackson. The meeting lasted from 7 pm to shortly before 9 pm.

- The vast majority of the discussion was in regard to the Municipality of Anchorage's proposed acquisition of the Golden Lion Hotel for use as a Substance Misuse and Homelessness Treatment Facility.
- Jason Bockenstedt, the Mayor's chief of staff, led the discussion on the proposal and answered most of the questions. Most of those in attendance were against the proposal but the MOA seems that they are intent on moving ahead and will be putting out an RFP soon to find a proposed operator for the facilities. The timing of the acquisitions was not clear from the discussion last night, but they plan to solicit proposals from potential operators before they complete the acquisitions.
- The MOA is aware of the MCR project and the potential parking, access, and ROW impacts that could affect the Golden Lion property. Prior communication from the MCR team to Chris Schutte let them know that MCR is likely to lead the DOT&PF not renew the 3-year revocable encroachment permit for Golden Lion's parking that is in the ROW and that there could be narrow strip takes on the Seward Highway and 36th Avenue sides. They understand that more information on potential impacts will become known as DOT&PF works through the environmental document for the 36th Avenue project.
- Attendees wanted to know if there had been coordination between the MOA and the DOT&PF regarding the treatment center proposal and if there were conflicts with MCR. Jason answered this question explaining that they had coordinated and that all indications were that they MOA's plan for this property would be able to proceed, though they understand that there could be some parking and access impacts that would be negotiated with the MOA as the new owner of the property, if it were to be acquired.

- Following the lengthy discussion on Golden Lion, Steve Noble gave a brief update on the MCR project and introduced Sean Baski as the new DOT&PF project manager. We let them know that the Final PEL has been posted to the website for public access, that the topographic and ROW survey tasks have been proceeding in May/June, and that we are moving ahead into the environmental and preliminary engineering phases of the 36th Ave. We let them know that the advisory group meetings would be starting again soon and that an invitation to participate in the advisory group will soon be sent out to those who were previously involved during the PEL study and we hope to have our first meeting by mid-August. There were no follow up questions from the public.

Regina Lemons, P.E. *

State of Alaska DOT&PF, Central Region Highway Design Section

P.O. Box 196900. Anchorage, AK 99519-6900

907.269.0753

Regina.lemons@alaska.gov

*Licensed in the State of Texas

Rogers Park Community Council

Monday, February 8, 2021 – 7 PM

Meeting via ZOOM

AGENDA

- I. CALL TO ORDER AND WELCOME:** Steve Lindbeck, President
- II. ZOOM MEETING PROCESSES:** Al Milspaugh, Federation of Community Councils
- III. APPROVAL OF MINUTES:** December 14, 2020, and January 11, 2021 (action item)
- IV. FINANCIAL REPORT:** Ric Wilson, Treasurer
- V. FCC REPORT:** Jim Wright, FCC Representative

- VI. COMMITTEE REPORTS**
 - a. Garden & Beautification Committee – Zach Zaletel, Chair
 - b. Midtown Congestion Relief – Steve Noble, DOWL

- VII. REPRESENTATIVE REPORTS**
 - a. Alaska Legislature Reports: 5 minutes each
 - i. House District 17 – Representative Andy Josephson
 - ii. House District 18 – Representative Harriet Drummond
 - iii. Senate District I – Senator Elvi Gray-Jackson
 - b. Anchorage School Board Report: 5 minutes
 - i. Board Member Margo Bellamy
 - c. Anchorage Assembly Reports: 5 minutes each
 - i. Seat F – Meg Zaletel
 - ii. Seat G – Felix Rivera, Assembly Chair

- VIII. NEW BUSINESS**
 - a. Resolution in Support of the Development of ASAM Level 3.3 High Intensity Residential Treatment Services in Anchorage and Continued Community Council Collaboration

- IX. ANNOUNCEMENTS AND MEMBER COMMENTS**

- X. ADJOURNMENT**

TO JOIN THE ZOOM MEETING: <https://zoom.us/j/94514203881> Meeting ID: 945 1420 3881

Upon entering please log in with your (1st name & last name) then “Member” or “Guest” with your affiliation. Individuals calling in via telephone must provide their name and affiliation. The host will assist you. To mute/unmute, tap *6. The meeting is open to the public and may be recorded.

Next Meeting of the RPCC Executive Board (via Zoom): February 25, 2021, 7 PM

Next Meeting of the RPCC (via Zoom): March 8, 2021, 7 PM

Rogers Park Community Council: <http://www.communitycouncils.org/servlet/content/28.html>

Federation of Community Councils: <http://www.communitycouncils.org/servlet/content/home.html>

Municipality of Anchorage: <http://www.muni.org/pages/default.aspx>

Rogers Park Community Council Meeting Minutes

Meeting: Monday, 8 February 2021, 7:00pm – 9:30pm

Zoom meeting membership was confirmed by Al Milspaugh, FCC Outreach, from 6:55pm-7:13pm.

Mark Foster, Secretary, noted that there were 60 zoom meeting participants of which at least 30 RPCC members were in attendance. (Quorum requirement = 15 members).

President Steve Lindbeck called the meeting to order at 7:13pm.

President Lindbeck presented the minutes of January 11th for consideration. They were moved by Linda Chase, Seconded by Pete Mjos. President Lindbeck asked if there were any objections to approving the minutes of January 11. Hearing no objections, they were approved by unanimous consent.

President Lindbeck presented the minutes of December 14th for consideration. They were moved by Linda Chase, Seconded by Susan Levy. President Lindbeck asked if there were any objections to approving the minutes of December 14th. Hearing no objections, they were approved by unanimous consent.

Ric Wilson, Treasurer, reported a balance of \$763.77 at January 31, 2021, which included 1¢ of interest in January.

Jim Wright had submitted a Federation of Community Council Report that will be posted to the website.

Elected Representative Reports

Senator Elvi Gray-Jackson (Senate District I) highlighted:

- February was Black History Month.
- She had introduced SB40 in support of celebrating the achievements and inventions of members of the Black Community in U.S. History.
- She had given a floor speech the previous Wednesday and expected to do so again on the achievements of prominent members of the Black Community in U.S. History.
- She had, in collaboration with Senator Tom Begich, had introduced several bills related to public safety, including SB 1,2,3,4,7 and 46. In addition she had introduced SB5 which established Juneteenth as a state holiday.
- She was busy on several committees and actively working with her colleagues from both sides of the aisle.
- Linda Chase asked if the police had been consulted on the proposed public safety legislation that had been introduced.
- Senator Gray-Jackson thanked Ms. Chase for the question. Senator Gray-Jackson, consistent with her practice while on the Assembly, had worked hard to bring stakeholders to the table. She had been working the bills all summer, including multiple meetings with public safety officials to review draft bills and redrafted bills to meet the concerns expressed by public safety. Her staff had put together a power point presentation that described the process and the bills. Senator Shower from the majority had asked Senator Gray-Jackson to send him the bill files on SB1 and SB4 for the purpose of review and scheduling in his committee. Senator Gray-Jackson was looking forward to the bills receiving a hearing in his committee.

- She encouraged people to tune into Gavel to Gavel and read her monthly Newsletter.
- Her office number is 465.4930
- Additional information is available via Elvi's Updates, available at:
<https://www.facebook.com/SenatorElviGrayJackson/>

Committee Reports

Zach Zaletel reported that the Garden/Beautification Committee met and was looking for projects to recommend to the Municipality for inclusion on their capital projects list. Zach had developed and submitted a map of potential projects. The next meeting of the Garden/Beautification Committee was scheduled for 7pm on February 17. Zach also reported that Ron Alleva had donated 60 planters to the Committee. Ron showed a stack of large planters on his zoom screen.

Steve Noble, DOWL Engineering and Sean Baski, DOT Project Manager for the 36th Avenue Interexchange talked about the status and next steps for the project.

The project was in the early conceptual design phase. An open house to describe the project, project plan and current conceptual designs was slated for Thursday, March 16th from 5 to 7pm. It would consist of a 15-minute pre-recorded presentation followed by a 45-minute question and answer period which would be repeated again at the top of the hour.

Buck Walsky asked about the plan to raise the highway and its potential impact on noise. Mr. Baski indicated that they were considering which of the two roads to raise and that one of the considerations was noise and noise abatement and were planning on conducting a formal noise contour study consistent with federal planning requirements.

Mike Savitt asked whether the Municipal investment in the Golden Lion Inn for a treatment center was a mistake in light of the project to redo and expand the intersection.

Mr. Baski reiterated that the Municipality had consulted with DOT prior to their selection of the site for potential acquisition to assess whether and how much of an impact the intersection redesign might have on the property. DOT had shared with the Municipality that they expected that they would take about three parking spaces that were previously available by a permit within the right of way and that the safety of the intersection would be greatly improved – safety was the cornerstone of the project.

Buck Walsky asked whether the project advocates had any examples of safe intersection designs that they could site. Mr. Baski offered examples of traffic separation and pathways at Lore and Huffman.

Kurt Currey asked whether the right of way had been secured. Mr. Baski indicated they were just finalizing project definition and expected to refine the design option prior to finalizing right of way procurement plan. The project still needed to get in the Statewide Plan after which it would begin to become eligible for federal funds. He did not expect any acquisitions of right of way until after federal funds were available. Kurt Currey said “he didn’t see how you could make it work.” Mr. Baski offered that his team saw “a lot of room” in which to make a number of traffic and safety improvements while only taking “slivers” and narrow “strips” which they had shared with the Municipality prior to the Municipalities consideration of the Golden Lion Hotel property for a treatment center.

Beth of “Beth/Barry Q” asked have you looked at the roundabouts that were removed in New Jersey?

Sean replied that modern roundabouts were one of the options to consider especially since the data indicated that modern roundabouts were “far safer” than signals. And they would endeavor to take into account community input.

Mike Savitt reiterated his concerns that the project would severely affect the viability of the Golden Lion Hotel site as a treatment center and be a “disaster to the community” and that the Municipality should hold off on the Golden Lion site until after the DOT design was completed.

The DOT team reiterated that they saw numerous concepts in the preliminary design that would preserve the viability of the Golden Lion site and had reported those findings to the Municipality prior to their identification of the Golden Lion as a viable site.

Susan Levi asked about the “old Dairy Queen” site on the West Side of the Highway. Mr. Baski indicated that DOT had acquired those properties some time ago and they afforded the project more room.

Representative Andy Josephson (House District 17) highlighted:

- The House remained locked in a stalemate at 20:20.
- He had prefiled bills on:
 - Oil Spill Penalties & Fees which had not been updated in the 40 years
 - End of Fiscal Year Fiscal Administrative and Appropriations Procedures, e.g., “reverse sweep” and end of year management of budget reserve procedures
 - Defined Benefits Pension Plans for Police, Fire and State Troopers; picking up a bill that Chuck Kopp had been sponsoring.
 - Workers Compensation issues related to Permanent Partial Impairments, e.g., loss of a finger at work, which had not been updated since 2000
 - Disallowing discrimination in employment, housing or credit based on gender issues
 - Hate Crimes, responding to a knife attack in Stirling that may well have merited consideration as a hate crime
 - Workers Compensation issues related to what constitutes a presumptive illness
- In light of the failure of the House to organize, he was attending Senate Committee hearings to monitor legislative developments.

Representative Harriet Drummond (House District 18) highlighted:

- She was frustrated by the 20/20 stalemate in the House.
- She shared that Alaska was the number one state in the country at number of vaccines that had been administered per capita.
- She was looking forward to the House organizing so they could start on their budget committee work. In the meantime, she encouraged people to go to the Commonwealth North website AKBudget.com for a review of the budget and to use that as a guide to recommend any changes in the budget.
- Dave Morgan expressed concern that the House had not yet organized and offered what he knew to be a spurious comparison, given his long history of involvement in Kentucky politics and knowledge of Senator McConnell’s practice as a legislative leader, to the U.S. Senate where the Vice President had authority to act as a tie-breaker which clearly helped focus both sides of the power sharing agreement in the U.S. Senate.

- Sara Dykstra was pleased that kids were getting back to school but remained concerned that teachers were being further subdivided as a group and put further back in the vaccine queue. Why were teachers being sent into the classrooms without a vaccine?
- Representative Drummond shared her concerns. She highlighted that ANTHC was doing a great job of vaccinating their population, all the way down to 18 year olds and covering households with vulnerable members. They were doing it right.
- Dave Evans offered that he had contacted the State for information on the size of the tiers and when the next tiers might be expected to become eligible.
- Representative Drummond indicated she would send a request for information to the state.

School Board Member Margo Bellamy highlighted:

- The next school board meeting was scheduled for next Tuesday and they were in the middle of budget deliberations.
- In-person schooling now included K-6.
- “Beth & Barry” asked about COVID illness rates in the schools.
- Board Member Bellamy indicated that the district was posting daily updates of which classrooms were closed due to positive tests by 5pm on weekdays.

Assembly Chair Felix Rivera highlighted:

- The Assembly was scheduled to meet on Tuesday the 9th of February and was slated to hear the introduction of measures to move the Emergency Orders to Assembly Ordinances – effectively moving the process from the Executive Branch to the Legislative Branch with a more open deliberative process.
- The Municipality was extending/expanding the federally supported rental relief program and anticipated a total of \$75-76 million would be available.
- In addition, VISIT ANCHORAGE was expected to distribute additional relief to the tourist industry.
- The Assembly met in January and moved a capital project bond and ballot proposition on the IT needs of the Police forward to the April ballot. They had allocated funds from the operating budget to dig into body worn camera issues, including privacy questions.
- Dustin Darden asked whether the video footage would be held by private corporations or available to foreign countries.
- Assembly Chair Rivera responded that the Assembly would consider where the vendor would house the servers and the privacy issues raised.

New Business – Resolution in Support of A Treatment Center in Anchorage + Timely and Meaningful Opportunities for the Community Council to Engage in the Treatment Center Development Process:

The resolution was introduced by Steve Lindbeck and Mark Foster.

Linda Chase spoke in favor of it as a reasonable compromise – supporting the development of a treatment center in Anchorage while emphasizing the need for timely public input from the local community at each stage in the development process.

Mike Savitt, Jami George, Dave Morgan, Beth & Barry, Carrie Currey, Ron Alleva, Chip/Geogianna spoke against the resolution. In summary Mike Savitt offered that it was not “strong enough” at condemning the Assembly’s actions to explore the development of a treatment center at the Golden Lion Hotel Site.

Pete Mjos spoke in favor of the resolution. Ric Wilson spoke in favor of the resolution.

Dave Evans raised a point of order – don’t we need a motion before we can begin the discussion.

The Chair concurred.

The resolution was moved by Ric Wilson and seconded by Michael Savitt.

A motion to table was made by Kirk Currey; seconded by Jami George.

The motion to table passed 16-15.

Candidates for local office made a brief pitch for their candidacies:

- Alisha Hilde school board
- Carl Jacobs school board
- Kelly Lessons school board
- Patrick Higgins school board
- Dustin Darden Assembly

Without objection Steve Lindbeck adjourned the meeting at 9:30pm.

Rogers Park Community Council

Monday, March 8, 2021 – 7 PM

Meeting via ZOOM

AGENDA

- I. CALL TO ORDER AND WELCOME:** Steve Lindbeck, President
- II. APPROVAL OF MINUTES:** February 8, 2021 (action item)
- III. FINANCIAL REPORT:** Ric Wilson, Treasurer
- IV. FCC REPORT:** Jim Wright, FCC Representative
- V. PRESENTATION:** State DHSS on Covid-19 Vaccinations
- VI. COMMITTEE REPORTS: 4 minutes each**
 - a. Garden & Beautification Committee – Zack Zaletel, Chair
 - b. Midtown Congestion Relief Committee – Bob Butera, Chair
 - c. Accessory Dwelling Units – Dan Rosenberg, Chair
 - d. Public Safety Committee – Linda Chase, Chair
 - e. Nominating Committee – Linda Chase, Chair
- VII. REPRESENTATIVE REPORTS: 4 minutes each**
 - a. Alaska Legislature Reports:
 - i. House District 17 – Representative Andy Josephson
 - ii. House District 18 – Representative Harriet Drummond
 - iii. Senate District I – Senator Elvi Gray-Jackson
 - b. Anchorage School Board Report:
 - i. Board Member Margo Bellamy
 - c. Anchorage Assembly Reports:
 - i. Seat F – Meg Zaletel
 - ii. Seat G – Felix Rivera, Assembly Chair
- VIII. OLD BUSINESS:** Resolution tabled during February 8 meeting (available on RPCC website)
- IX. ANNOUNCEMENTS AND MEMBER COMMENTS**
- X. ADJOURNMENT**

TO JOIN THE ZOOM MEETING: <https://zoom.us/j/94514203881>

Meeting ID: 945 1420 3881

Call in: +1 253 215 8782

Upon entering please log in with your (first and last) name and then note “Member” or “Guest” with your affiliation. Individuals calling in via telephone must provide their name and affiliation. The host will assist you. To mute/unmute, tap *6. The meeting is open to the public and may be recorded.

Next Meeting of the RPCC Executive Board (via Zoom): April 1, 2021, 7 PM

Next Meeting of the RPCC (via Zoom): April 12, 2021, 7 PM

Rogers Park Community Council: <http://www.communitycouncils.org/servlet/content/28.html>

Federation of Community Councils: <http://www.communitycouncils.org/servlet/content/home.html>

Municipality of Anchorage: <http://www.muni.org/pages/default.aspx>

From: [Katie Conway](#)
To: [Sean Baski](#)
Cc: [Farr, John D. \(DOT\)](#); [Steven Noble](#); [Rachel Steer](#); [Renee Whitesell](#)
Subject: Rogers Park Community Council meeting 03/08/21
Date: Monday, March 8, 2021 8:27:00 PM

Hi Sean,

I attended the Rogers Park Community Council meeting tonight to give a quick reminder about the open house next week.

This was a Zoom meeting attended by approximately 50 participants. Notable attendees included: Sen. Elvi Gray-Jackson and her staff, staff to Rep. Josephson, Assembly members Meg Zaletel and Felix Rivera, and Anchorage School Board Member Margo Bellamy.

Bob Butera, the chair of their Midtown Congestion Relief committee, was absent from the meeting and there was no one else from that committee to give an update. I took the opportunity to remind folks about the midday and evening open houses next week, saying that meeting materials would be available on the project website by the end of this week and that we're encouraging folks to send us any questions in advance of the meeting. There were no questions from participants.

Peter Mjos asked for a quick update on the Community Building Working Group. I said we met a few times in the fall, did an asset mapping exercise in January, and are planning for the next meeting – a Strategy Lab – this spring, likely the second half of April. He had no follow up questions, but said he would reach out if he had questions later.

Participants did not express tremendous interest in our project.

Thank you,
Katie

Katie Conway
Public Involvement Manager

DOWL

(907) 562-2000 | office
(907) 865-1289 | direct

4041 B Street
Anchorage, AK 99503

www.dowl.com

Rogers Park Community Council Minutes
7 PM Monday April 12, 2021

CALL TO ORDER AND WELCOME: Steve Lindbeck, President

Board Members:

Steve Lindbeck, President

Linda Chase, Vice-President

Secretary (Vacant)

Ric Wilson, Treasurer

Sara Dykstra, RPCC Member Acting Secretary

I. Welcome

The meeting was called to order at 7:11 pm with 39 participants. Members were asked to rename themselves with RPCC for voting purposes

II. APPROVAL OF MINUTES: March 8, 2021 (action item)

Linda Chase moved to approve them. There was no second. Minutes from March will be resubmitted next month for approval.

III. FINANCIAL REPORT: Ric Wilson, Treasurer

Ric Wilson reported present balance is \$763.78. Interest payment of \$.01.

IV. COMMITTEE REPORTS

a. Garden & Beautification Committee – Zach Zaletel, Chair

Plan: Materials assembled to start planting. Request from members — If anyone is planting at home and can plant extra hearty plants, the committee could use extras for beautification. Extra assistance in College Village needed. There is extra seeds/soil available if anyone wants to plant. FCC will reimburse up to \$75 in expenses for beautification - receipts needed.

b. Midtown Congestion Relief Committee – Bob Butera, Chair

Steve Noble and Dave Evans were present. Steve Noble update: Continuing to work on environmental processes and look at traffic/noise issues. Early stages of project planning as well as alternatives (36th Ave). Another public Meeting will occur late summer/early fall. There is a resolution that will be discussed and possibly voted on later tonight. Dave Evans described resolution: March 16 DOT and DOWL had a meeting to describe alternatives on 36th interchange. They asked for feedback and this resolution provides that feedback. This is consistent with the January 2020 resolution that was passed. Key points: RPCC continues to support a future depressed freeway - under 36th and under N lights, advocated for non-motorized passages, maximum 35mph on frontage road and 45 mph on highway. Also advocated for Environmental Assessment.

c. Public Safety Committee – Linda Chase

No updates at this time.

d. Nominating Committee – Linda Chase

Elections will occur in May for new officers. Candidates to date: President: Steve Lindbeck, VP: Pete Mjos Secretary: no name currently, Treasurer: Ric Wilson, FCC Rep: Jim Wright (not

an elected position - chosen by President). Additional nominations can be submitted to Linda Chase.

V. REPRESENTATIVE REPORTS

a. Alaska Legislature Reports: 5 minutes each

i. House District 17 – Representative Andy Josephson

Day 84 of Session. Once adjourned there may not be authority to spend additional funding.

House organized around 18th of Feb. Department of Law: Not much to report, except additional funding for therapeutic courts and additional funding for prosecutors of certain crimes (sexual offenses). Dept of Law also wants to shift muni prosecutions to home rule cities (usually only Anchorage and Juneau) - this was declined and Dept of Law will continue to pay. DHSS: 51% turnover at Office of Children's Services — they have increased \$3000 bonus.

200+ Social Workers have also been given a clinical therapist to support them - also a \$500,000 scholarship fund. Upcoming Bills: Defined pension for first responders, Workmans comp to essential workers during a disaster, Update personal partial impairment formulas, Bill on Reverse Sweep, Sponsor of House bill 104 - double motor fuel tax (current tax was set in 1970) - move from \$.08 to \$.16/gallon. Wrapping up operating budget in Finance Committee. Question: Can you dedicate those funds directly for roads? A: Rep J - No - you can designate them but you cannot dedicate them. Can be taken for other purposes with enough support. Q: On the \$1.1 Billion, would a special session be needed? Is that Feasible? A: Could not do special session for \$5 Billion. Concern is that the Governor will use a process to spend money in the interim.

ii. House District 18 – Representative Harriet Drummond

Not Present

iii. Senate District I – Senator Elvi Gray-Jackson

Confident that they will be in session for full session and also that they will extend for 10 days to appropriate funds. HB76 is now in the Senate - to extend COVID emergency. Anticipating it will be voted on soon. All committees she serves on awaiting final budget. Bills she has proposed: Choke hold, law enforcement use of force, Juneteenth paid holiday, Vehicle registration for persons with disabilities, all awaiting hearings or in process with hearings. SB 80 - Public Schools mental health - will move out of committee soon. First Virtual constituent meeting on Saturday - 20 folks attending as well as live-streamed on Facebook. Question: SB 39 - status A: Has been taken but Senator Showers has another bill waiting - not sure what is in there. Q: What percentage of legislators has been vaccinated? A: Has not been updated lately — believes last update was about 40% — there are 400 people in the Capitol including staff. Rep Josephson says the numbers are increasing. Sen. Gray -Jackson reports her whole staff is vaccinated.

b. Anchorage School Board Report: 5 minutes

i. Board Member Margo Bellamy

Next Meeting April 20, 2021 6pm - Executive Session prior (closed to public) - Agenda posted online. Planning graduations now. Kindergarten roundup in April (virtual info sessions).

Planned a robust summer school to mitigate learning loss due to COVID — registration started today. Open to students who have failed or had incomplete first — then will be open to others. Fall Lottery is open for lottery programs. Budget was passed by Assembly. Q: Is Virtual Option for summer school only available to virtual students or those that attended in person? A:

Currently it is only open to virtual students, but if there are open slots, then any student can

enroll. Q: Website was unclear about who could register for summer school. Q: How is hiring going? A: Limitation is teachers right now — it is hard to hire teachers (and more money needed) for more students. Some teachers will be teaching both sessions Q: When do new terms for school board start? A: April 26th is when new board will be restructured. Sworn on Monday May 3rd (Not May 4th).

c. Anchorage Assembly Reports: 5 minutes each

i. Seat F – Meg Zaletel

3 topics around COVID response

1. Mayor's request to extend emergency declaration to be extended through July 16th. Cases remain high, need measures in order to keep economy open and to be able to respond quickly. Expecting a robust debate tomorrow
2. EO 20: Do it for Summer - goes into effect Friday PM. Loosens restrictions for some areas - Mask Mandate still in effect, but allows community to take advantage of increasing vaccination rate. Graduation attendance was a concern - removed restrictions on graduation crowd size.
3. Health Policy Committee briefing - Transition plan to demobilize over time. This is typically done in stages — they will utilize emergency operations staff but normalize those into our regular staff (vaccines and testing). Contracts and logistics will remain with emergency operations. Shelters and congregate settings - at time of briefing there was 950 individuals with capacity of 1036 — 814 are currently in EOC - FEMA is reimbursing until end of September. They are looking for options to transition out of this emergency shelter into something more permanent. Q: Mayor has set 70% goal for vaccinations — all measures will be “advisory” at that point. Member believes we already have herd immunity. A: Please email the paper and info to pass along. Felix and Meg will be holding a conversation with the community about this very topic and would welcome that. The emergency order does not just cover covid response but also the vaccination roll out. There should be a presentation at Assembly meeting about the current response. Q: Where did the mayor get the 70% vaccination rate goal? A: Figure came from Dr. Johnston, our Muni Epidemiologist
Follow up: Member asked about Dr. Johnson's medical background.

ii. Seat G – Felix Rivera, Assembly Chair

1. COVID 19 relief funds - Muni will be getting \$101 million (estimate). Assembly has had town halls/work sessions (March 19 and 26, April 9) to get feedback. No “next meeting” scheduled yet. Awaiting federal guidance from various departments and also wants to know exactly how much the funds will be. Also waiting to find out what state legislature will do with \$1.1 Billion — Assembly wants to do a gap analysis to find holes and gaps in funding between State and Muni. A website will be available soon — currently go to Muni website and go to COVID 19 link. Feedback form is active currently.
2. Golden Lion RFP was published today on muni.org - Purchasing - Current Bidding opportunities. 3 different bids -RFP - 2021P012. Synopsis and 18 page RFP. Important sections: Page 6 of RFP -Willingness of participants to participate in public outreach (requirement who will be that person), Page 10 - Good neighborhood guidelines (GNG) (attachment C outlines GNGs) Page 11 - Breakdown of points (up to 100), Delivery of Similar Services is a requirement and Participant pathway from entry to discharge, Building Modifications, GNGs (10 points). Evaluation process - committee of individuals that perform the evaluation - Community Councils will be able to be involved but how is TBD. Good Neighbor Guidelines: Advisory Committee (neighborhood and provider representatives - thorough communication to mitigate negative impacts), also discusses

participant and community safety. The RFP is final but Good Neighbor Guidelines are in draft. Due date for proposals is May 14th — there may be opportunity to add an addendum to RFP if there are glaring holes. Jim Wright has been working on how we as a council can influence security and safety around this facility. Jim was disappointed the RFP went out today but it is important that there is an opportunity for addenda. Felix said any additional criteria could be added within the current scoring components. Those would need to be added quickly — end of the month at the latest. Important Dates: Facility Walkthrough April 26, Pre-proposal meeting April 27th, Questions due to April 28th. Final proposals due May 14th. April 23rd is probably an internal deadline for providing additional info to be included.

Questions:

-Q. Are vendors required to provide community impact statements (Mike)? A: Vendors commitment to mitigate community concerns are included in the GNGs.

-Q: What will the muni's responsibility be towards communities and patients once a vendor is operating? Will it just be up to the vendor? A: Ongoing muni commitment will be memorialized in the start up contract. It is a public private partnership and there are start up dollars as well as municipal oversight. Sample contract was provided in the RFP.

-Q: Will mitigation and testing for TB be included?(Dave Morgan) A: Yes, we will look at that

-Q: Since this is voluntary, many patients will be self pay or insurance — how will that defray the overall cost to the muni (residential 30 days inpatient)(Pete Mjos). A: The RFP is meant to target the 1115 medicaid waiver was brought on — when there is a gap. The provider can create multiple categories for funding - private, medicaid, etc as well as muni contribution for start up funds. Part of RFP is a financial plan as well.

Suggestion to include insurance requirements to protect from liability (risk management).

Government has some immunity with policy making but it does not cover for implementation.

Meg asked Jim Wright to review sample contract and flag any concerns. Concern that the implementation will not go well and will have negative impacts on the neighborhood (Ron Alleva). Felix provided clarification - Golden Lion is not a shelter, never was a shelter, it is an inpatient substance treatment center. Pete Mjos provided clarification — it is a closed center, not open - they are under strict supervision for the 30 day stay. People will not be able to come and go at will, it will be fenced. Clients will be people who qualify for the program. One major gap we have is the time a client identifies this need and the time when they can get into this facility.

-One member offered up support for Meg and Felix and thanked them for incorporating council feedback into the RFP.

Q; Does this proposal still include step down housing? A: Vendors can provide what services that would like to offer (including step down housing) in the proposal. If that is an option then identify it would need to be addressed in all the areas of the Proposal (security, GNG, etc).

Q: Are the GNGs just strongly worded suggestions or is it something they need to hold to or they will lose their contract? (Beth A) A: Currently in draft form — now is the time to give feedback — would be helpful to have them give updates to community council, have a designee that works with the council - increase feedback. There is some more time with the GNGs as it is currently in draft.

Q: What is the timing on this? (Mike Savitt) A: RFP is out - Meg and Felix did try to get it delayed but were not successful. Feedback should be provided ASAP. Administration chose to release it now rather than delay/wait.

-Linda Chase thinks an additional committee should be formed to provide more input. Jim Wright agreed— there is an opportunity to provide feedback to alter the outcomes.

Steve Lindbeck: There have been 3 resolutions produced by members about this very topic. Jim was asked to do some legal work on what our authority is and how we can be constructive. There have been lots of good faith efforts to try to find common ground.

Question: Should we take a week or two now that we have the new RFP to provide more feedback and modify the resolution? Should we have an additional meeting?

Beth Abisror - would be in favor of pulling their resolution in support of Jim's resolution. Mike Savitt also agreed to pull his resolution in favor of Jim Wright's resolution. Suggestion is to adopt the resolution then formulate amendments for RFP based on our resolution and their RFP. Dave Evans - point of order, Bylaws would require us to have a special meeting to approve feedback to administration.

Suggested Way to Proceed:

1. Vote on resolution from a few months ago
2. Beth has pulled 2nd resolution
3. Can vote on Jim Wright's's resolution tonight
4. Committee to create additional amendments and a special meeting will need to be held to approve amendments.

VI. NEW BUSINESS:

- a. Summer meetings - Request to continue meeting over summer over Zoom. There is significant business that should be addressed. Mixed input from members - might need to take a break, not everyone is around/available. A lot of work from executive members. Concern about what will happen over the summer. Either as needed or limited agenda items (old business and new business). Decision deferred until next month's meeting.

- b. Municipal Capital Improvements Process CIP) – Heather Ireland

Muni gives a list of projects to community councils they would like ranked. List of projects will get emailed out - Survey will be emailed out and will be closed around May 12th. If new items are proposed, they can be emailed to Heather Ireland and Steve Lindbeck. Additional projects should be emailed by Friday. Meg Zalatel added that this was a critical year to add to the list because of possible federal infrastructure dollars.

- c. Resolution regarding Seward Highway/36th Avenue Interchange Project

Moved and Seconded. No discussion. Vote: 23 in Favor, 0 Opposed, 5 Abstain

- d. Resolutions regarding Class 3.3 Treatment Center at Golden Lion Hotel building (moved to this item before the other 3 since it was

-First resolution - dated Feb 8th, 2020 is on the table and will remain there

-Second resolution - dated March 8th, 2020 by Beth A has been withdrawn

-Third Resolution Dated 4/12/21 - Mike Savitt Moved - Ric Wilson Seconded. Jim

Wright discussed resolution and shared it with members.

Voting on approval of the Golden Lion Resolution (4/12/21): 38 in Favor, 2 Opposed, 1 Abstain

- Motion passed

VII. MEMBER ANNOUNCEMENTS & COMMENT

April 24th, 5pm (40-45 minutes) - History of Rogers Park Neighborhood — meeting will be recorded and slides will be posted.

Chugach Electric will have a vote for Board of Directors - link in the agenda

VIII. ADJOURNMENT

Meeting Adjourned at 9:55pm

ROGERS PARK COMMUNITY COUNCIL
RESOLUTION 2021-02

A RESOLUTION OF THE ROGERS PARK COMMUNITY COUNCIL REGARDING THE
SEWARD HIGHWAY: 36th INTERCHANGE PROJECT

WHEREAS, Rogers Park Community Council (RPCC) was closely involved with the recent Alaska Department of Transportation and Public Facilities (DOT&PF), Midtown Congestion Relief Planning and Environmental Linkages (PEL) study;

WHEREAS, DOT&PF and its consultant DOWL are proceeding with preliminary design and environmental permitting of the first PEL project to move forward, the Seward Highway 36th Interchange (hereafter “project”);

WHEREAS, on March 16, 2021, the project team held its first public meeting; presented four highway alternatives for the interchange; stated that they are evaluating whether the Seward Highway should be elevated above 36th or depressed below 36th, with 36th remaining approximately at grade for all alternatives; and requested feedback on all project issues;

WHEREAS, the RPCC’s January 13, 2020 Resolution 2020-01 regarding the PEL supported a future depressed freeway through the council area (RPCC extends from 36th to Chester Creek), opposed an elevated freeway, advocated for the thoughtful design of non-vehicular components of future PEL projects, and advocated for a 35 mph speed limit on frontage roads;

WHEREAS, in the March 16 open house, the project team stated that all highway alternatives for the 36th interchange, whether the highway passes over or under 36th, would be designed to allow future extension of the highway under Benson, Northern Lights, and Fireweed;

WHEREAS, in the March 16 open house, the project team stated that the DOT&PF had not decided whether an Environmental Assessment was needed; and


WHEREAS, the no-build and at-grade alternatives will not be evaluated unless an Environmental Assessment is performed;

Now therefore, be it resolved that the RPCC continues to support:

1. a future depressed freeway through the council area, passing under 36th, as well as under Benson, Northern Lights, and Fireweed in the future;
2. non-motorized facilities along and across the highway corridor that are accessible, safe, buffered, well-lit, usable year-round, and designed to minimize the winter maintenance effort;
3. a maximum speed limit of 35 mph on frontage roads and 45 mph on the highway through the entire midtown area; and
4. an Environmental Assessment for the 36th Interchange Project.

NOTE: this resolution is intended to reinforce and confirm the relevance of Resolution 2020-01 as it pertains to the project at hand. Resolution 2020-01 requests several accommodations that will be pertinent to other future PEL projects.

THIS RESOLUTION WAS APPROVED by the Rogers Park Community Council on April 12, 2021 by a vote of: FOR 23, AGAINST 0, ABSTAIN 5.



Steve Lindbeck, President
Rogers Park Community Council

Rogers Park Community Council

Monday, May 10, 2021 – 7 PM

Meeting via ZOOM

AGENDA

- I. CALL TO ORDER AND WELCOME:** Steve Lindbeck, President
- II. APPROVAL OF MINUTES:** March 8 and April 12 meetings (action item)
- III. FINANCIAL REPORT:** Ric Wilson, Treasurer
- IV. APD COMMUNITY/NEIGHBORHOOD UPDATE (IF AVAILABLE)**
- V. B-3 ZONING PRESENTATION:** Municipal Planning Department
- VI. CAPITAL IMPROVEMENTS (CIP) SURVEY:** Heather Ireland
- VII. UAA CAMPUS FACILITIES MASTER PLAN:** Holly Spoth-Torres

- VIII. COMMITTEE REPORTS**
 - a. Nominations Committee – Linda Chase, Chair
 - b. Garden & Beautification Committee – Zach Zaletel, Chair
 - c. Midtown Congestion Relief Committee – Bob Butera, Chair

- IX. REPRESENTATIVE REPORTS**
 - a. Alaska Legislature Reports: 5 minutes each
 - i. House District 17 – Representative Andy Josephson
 - ii. House District 18 – Representative Harriet Drummond
 - iii. Senate District I – Senator Elvi Gray-Jackson
 - b. Anchorage School Board Representative Report: 5 minutes
 - c. Anchorage Assembly Reports: 5 minutes each
 - i. Seat F – Meg Zaletel
 - ii. Seat G – Felix Rivera, Assembly Chair

- X. ANNOUNCEMENTS AND MEMBER COMMENTS**

- XI. ADJOURNMENT**

TO JOIN THE ZOOM MEETING:

Join Zoom Meeting: <https://zoom.us/j/94514203881>

Meeting ID: 945 1420 3881

Call in: +1 253 215 8782

Upon entering the meeting, please log in with your first & last name, then “Member” or “Guest” with your affiliation. Individuals calling in by telephone must provide their name and affiliation. The host will assist you. To mute/unmute, tap *6. The meeting is open to the public and may be recorded.

Rogers Park Community Council: <http://www.communitycouncils.org/servlet/content/28.html>

Federation of Community Councils: <http://www.communitycouncils.org/servlet/content/home.html>

Municipality of Anchorage: <http://www.muni.org/pages/default.aspx>

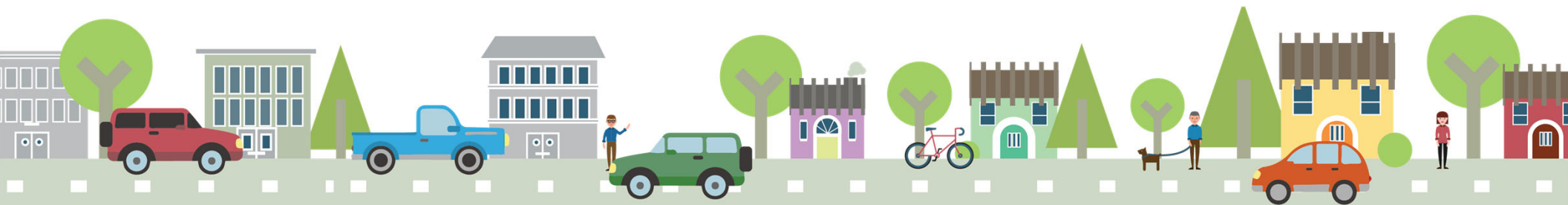
05.10.2021



36th avenue

INTERCHANGE

Seward Highway: 36th Avenue Interchange RPCC Meeting



PROJECT TEAM



Department of Transportation and Public Facilities

Sean Baski, PE
Project Manager

John Farr, EIT
Project Engineer

Consultant Team

Steve Noble, PE
Project Manager

Renee Whitesell, PTP
Transportation Planning

Rachel Steer
Public Involvement

Kelly Kilpatrick, PE
Transportation Engineering



PROJECT HISTORY AND BACKGROUND

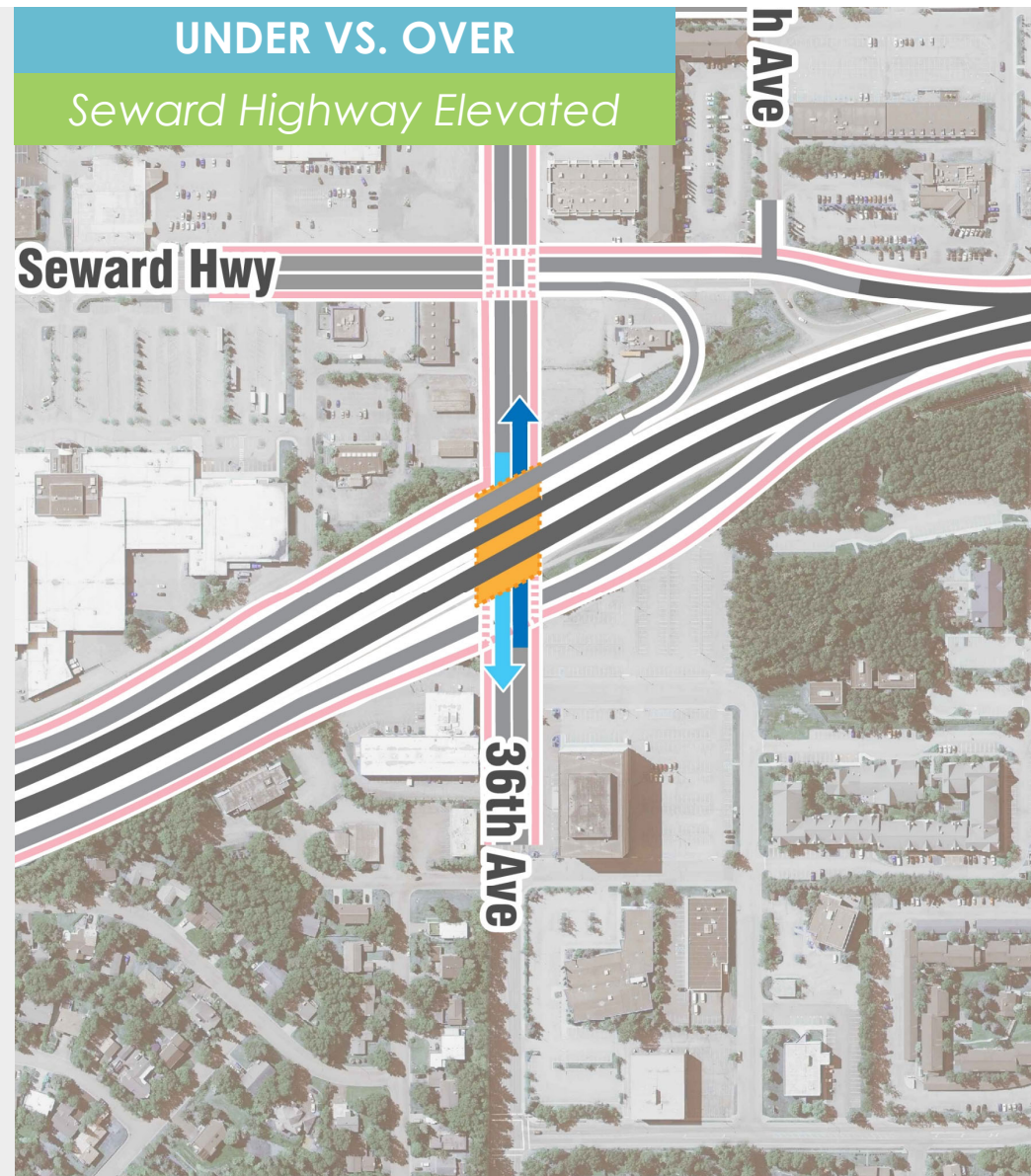
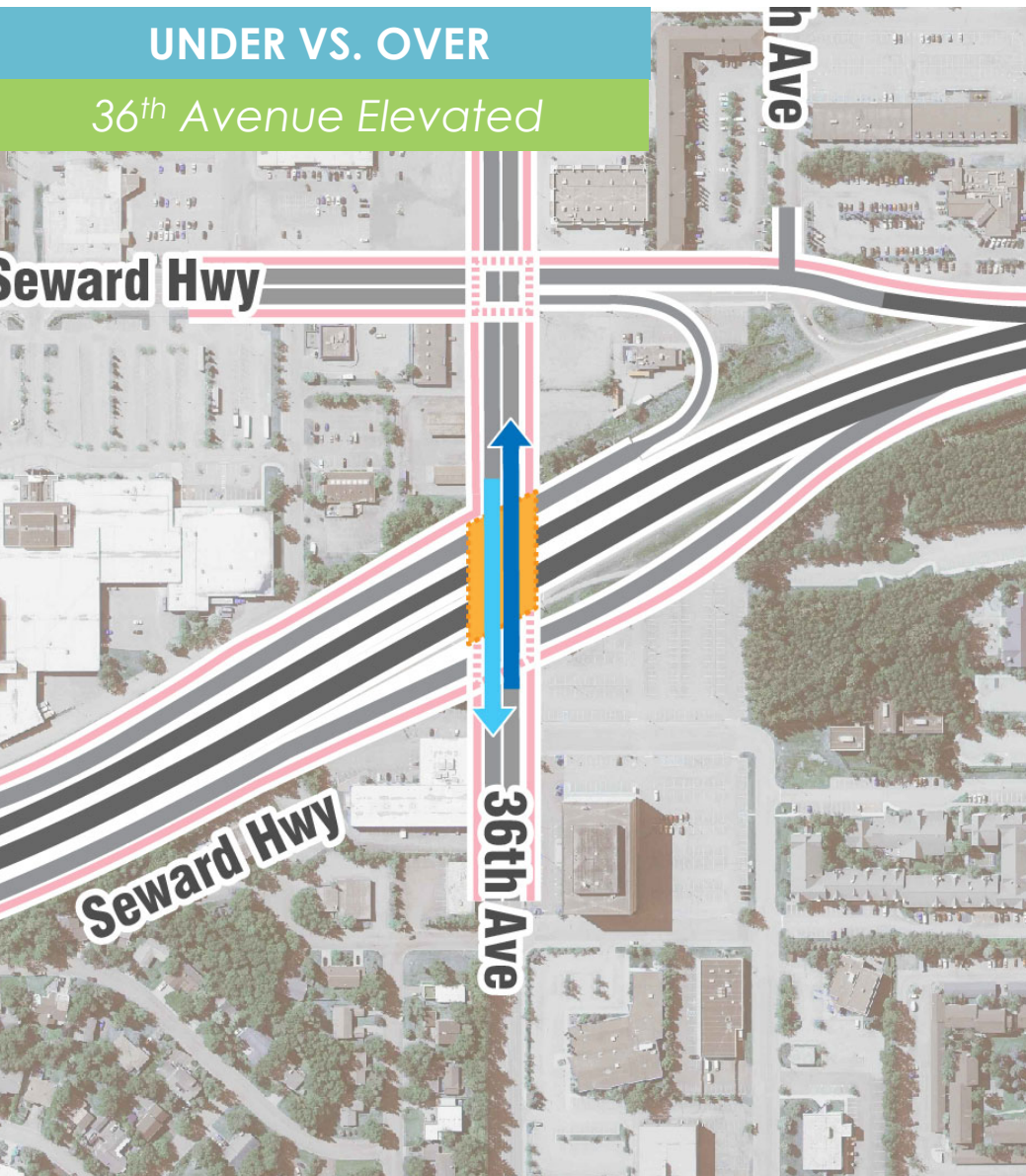
- Midtown Congestion Relief Planning and Environmental Linkages (PEL) Study concluded in 2020 and recommended 5 projects
- 36th Avenue Interchange is the first project moving forward
- Interchange was studied in 2013-2015 but did not progress
- This is a fresh design and environmental permitting effort



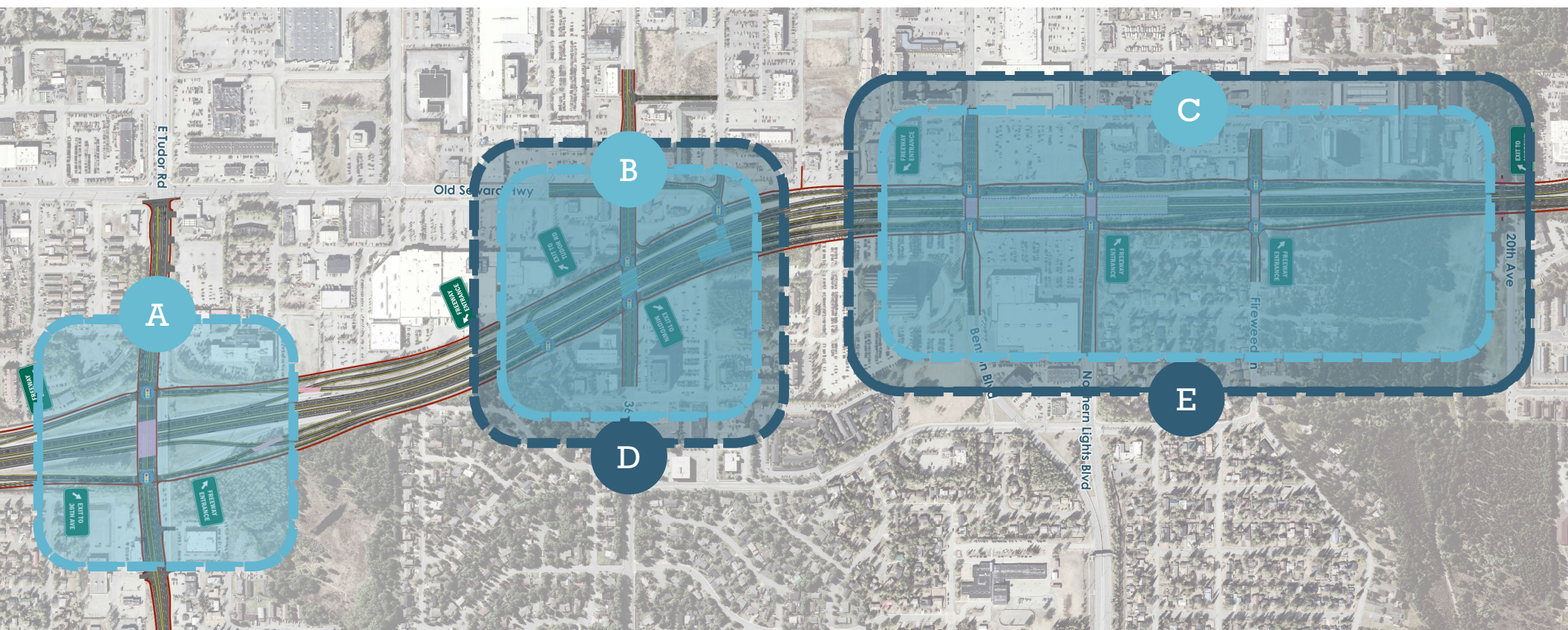
PROJECT STATUS

- **Narrowing down alternatives:**
- **Refining vertical and horizontal geometry:**
- **Initiating noise study:** Summer 2021
- **ROW Impacts**
- **Open House #2:** scheduled for September 2021

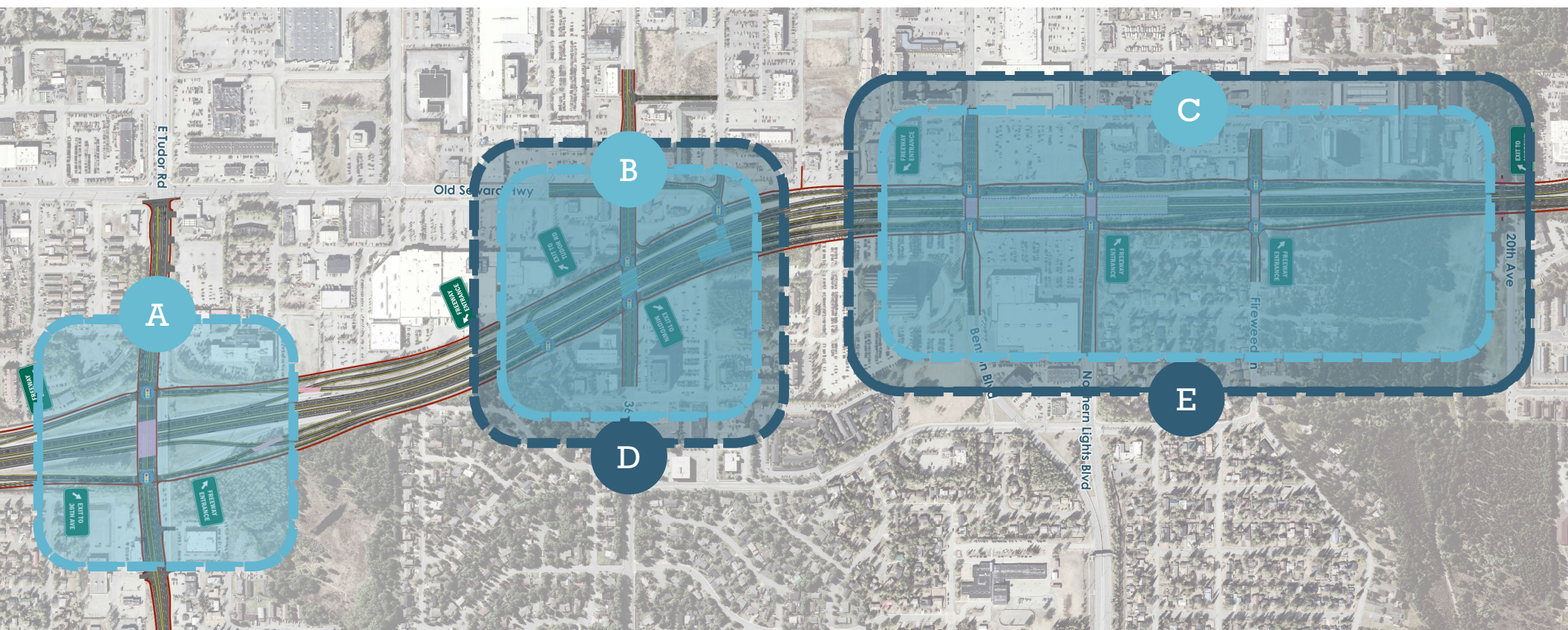




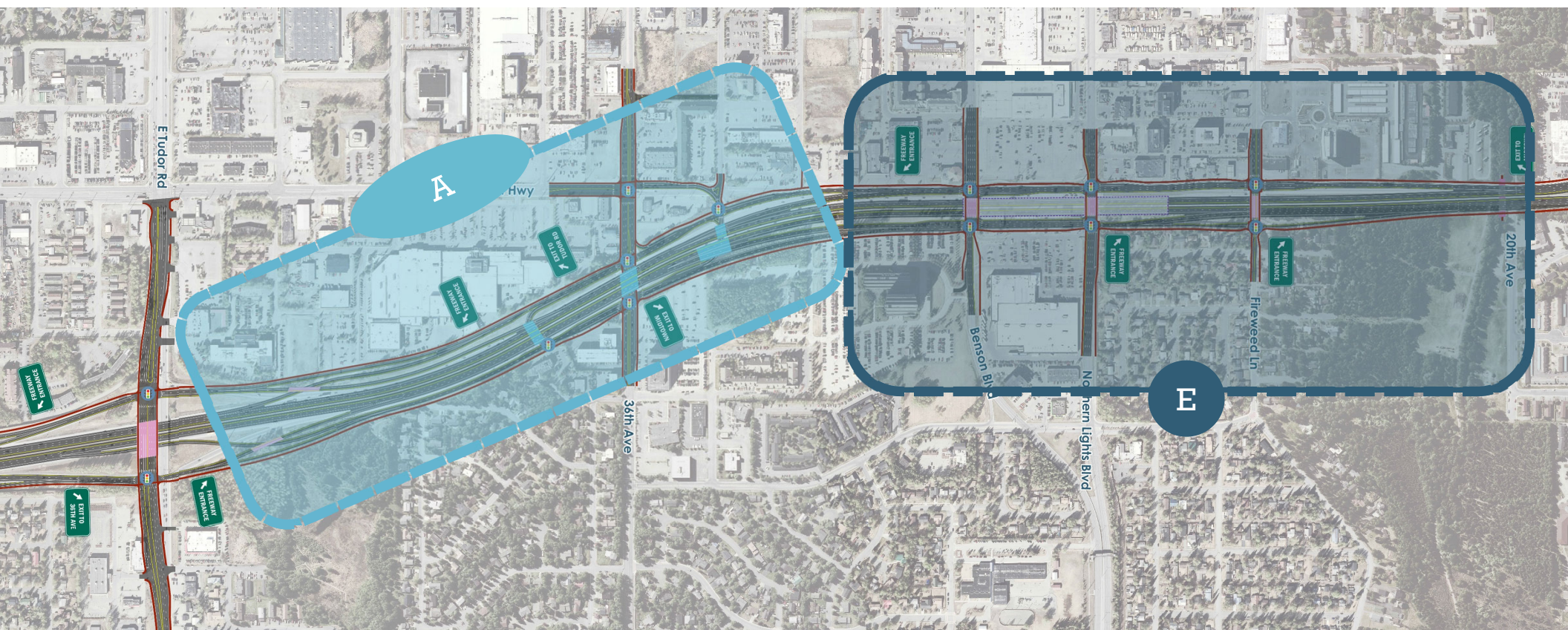
UNDER VS OVER



UNDER VS OVER



FULL BUILDOUT



PLANNED STUDIES/TIMELINE

- **Noise:** to understand likely impacts and identify whether any mitigation is eligible/warranted
- **Visual:** Evaluation through Municipality of Anchorage's Context Sensitive Solutions process
- **Right-of-Way:** understanding ROW impacts. In most instances, we expect only narrow strips of property will be purchased adjacent to the Seward Highway
- **Cost:** refining cost estimate



SCHEDULE

Task	Date
PEL Study	2017-2020
Public Involvement	Ongoing
Preliminary Environmental & Design	2020-2022
Final Design/ROW Acquisition	2022-
Construction	TBD





36th avenue INTERCHANGE

Thank You

Questions or Comments?

36thinterchange@dowl.com

www.36thinterchange.com



From: [Sean Baski](#)
To: [Jim Amundsen](#); [Bowland, Luke S \(DOT\)](#); [Linnell, John R \(DOT\)](#); [Shannon McCarthy](#); [Starzec, James A \(DOT\)](#); [Gardner, Shawn C \(DOT\)](#); [david.post](#); [Vanhove, Todd E \(DOT\)](#); [Reese, Jill \(DOT\)](#)
Cc: [Farr, John D \(DOT\)](#)
Subject: [EXT] Rogers Park CC Briefing Notes 5/11/21
Date: Tuesday, May 11, 2021 2:36:25 PM
Attachments: [2021_0510 RPCC Presentation 36th Ave Interchange.pdf](#)

WARNING: *External Sender - use caution when clicking links and opening attachments.*

Steve Noble and I attended (virtually) the Rogers Park Community Council last night and provided an updated presentation on the 36th Avenue Interchange project. DOWL quickly pulled together materials for this after we were asked to present last week.

Notable attendees: Assemblypersons Zaletel and Rivera, Senator Gray-Jackson, and Representative Drummond.

Council Member Dave Evans opened the conversation by telling members of the Community Council that the project team has determined the Seward Highway roadway profile is not going under 36th Avenue (i.e., we have made the decision). He let them know that Dave and Bob Butera had met with us a couple times and wanted the council to hear the briefing. One of their main priorities is to make sure the project remains compatible with the plan for a depressed highway north of 36th. Early in the presentation I addressed one of the primary concerns voiced by the Community Council as to why 36th Avenue is moving straight to a grade-separated interchange, rather than the at-grade intersection improvements identified in the PEL Study. I explained that DOT&PF was only going to get "one bite at the apple" to resolve 36th Avenue, and they also don't want to do two projects back-to-back on 36th Avenue which would be needed if the at-grade project proceeded.

The 15-minute presentation (see attached) followed up to reinforce the profile discussion, and also the approach of pursuing the grade separated interchange now.

There was significant time constraint as the agenda was very full. Primary concerns voice following the presentation were with regard to compatibility with the future project to address the Benson, Northern Lights, and Fireweed intersections, making sure the 36th Avenue interchange overpass would be compatible with the Seward Highway going under the east-west cross streets to the north. Council members were also concerned about the schedule on the project moving forward. In response to those issues, we told them our project was doing enough preliminary design to ensure the project was forward compatible. We were asked about the cost and timeline for the project so that the folks on Ingra Street are aware. We told them that there really wasn't a timeline for the segment between Benson Boulevard and 20th Avenue, but that it was likely to be after the 36th Avenue project. The priorities are Tudor and 36th Avenue first and then the next big project will depend on available funding and priorities. As to cost, we said we anticipated it to be between \$50-100M but we haven't narrowed it down any further than that.

We advised the Council that the noise analysis was moving forward this summer, and we would coordinate with their leadership on the field data collection as they aren't having any meetings in summer. We also let them know we are working toward an open house meeting in September, and hoping it would be in person.

Surprisingly, the over vs. under was not questioned during the meeting after Dave led us into it with that pronouncement.....

Following the meeting, Steve circulated a copy of the presentation to the Community Council President, Steve Lindbeck (attached). Mr Lindbeck replied thanking Steve for the presentation.

Planning to have a meeting with Assemblypersons Zaletel and Rivera to brief them on current happenings on the project.

Sean

--

Sean Baski, P.E., Project Manager

State of Alaska DOT&PF, Central Region Highway Design Section

P.O. Box 196900, Anchorage, Alaska 99519-6900

Phone 907.269.0547 | Fax 907.248.1573

Keep Alaska Moving through service and infrastructure.

Rogers Park Community Council
Monday, September 13, 2021 – Sign-On at 6:45 PM
Meeting via ZOOM

AGENDA

- I. CALL TO ORDER AND WELCOME:** Steve Lindbeck, President
- II. APPROVAL OF MINUTES:** May 10, 2021 meeting (action item)
- III. FINANCIAL REPORT:** Ric Wilson, Treasurer
- IV. FCC REPORT:** Jim Wright, FCC Representative

- V. COMMITTEE REPORTS**
 - a. Crime & Safety Committee – Linda Chase, Chair
 - b. Garden & Beautification Committee – Zach Zaletel, Chair
 - c. Seward Highway Project (36th Ave. Interchange) – Bob Butera, Chair, & Sean Baski, DOT

- VI. REPRESENTATIVE REPORTS**
 - a. Alaska Legislature Reports: 5 minutes each
 - i. House District 17 – Representative Andy Josephson
 - ii. House District 18 – Representative Harriet Drummond
 - iii. Senate District I – Senator Elvi Gray-Jackson
 - b. Anchorage School Board Representative Report: 5 minutes
 - c. Anchorage Assembly Reports: 5 minutes each
 - i. Seat F – Meg Zaletel
 - ii. Seat G – Felix Rivera

- VII. NEW BUSINESS: Election of Officers for 2021-2022 Year**

- VIII. ANNOUNCEMENTS AND MEMBER COMMENTS**

- IX. ADJOURNMENT**

TO JOIN THE ZOOM MEETING:

<https://us06web.zoom.us/j/84683936294?pwd=czFOVjYrRnVpRnRReWErMjIPMnBCQT09>

Meeting ID: 846 8393 6294

Passcode: 867297

Next Meeting of the RPCC Executive Board: September 30, 2021, 6:45 PM

Next Meeting of the RPCC: October 11, 2021, 6:45 PM

Rogers Park Community Council: <http://www.communitycouncils.org/servlet/content/28.html>

Federation of Community Councils: <http://www.communitycouncils.org/servlet/content/home.html>

Municipality of Anchorage: <http://www.muni.org/pages/default.aspx>

From: [Morgan McCammon](#)
To: [Sean Baski](#)
Cc: [Steven Noble](#); [Renee Whitesell](#); [Rachel Steer](#); [Katie Conway](#)
Subject: 36th Ave Interchange Update at Rogers Park Community Council
Date: Tuesday, September 14, 2021 8:22:00 AM

Sean Baski and Morgan McCammon attended the Rogers Park Community Council meeting this evening Monday, September 13, via Zoom. Sean provided the group an update on the 36th Avenue Interchange project, including that the project is in need of federal funding.

There were 48+ attendees, including State Senator Elvi Gray-Jackson, State Representative Andy Josephson, Assembly Members Meg Zaletel and Felix Rivera, and School Board Representative Carl Jacobs.

The following questions and comments were asked after Sean's presentation:

Sandra Christopherson asked about the project timeline. Sean stated that the project is currently developing the environmental documents required and the preliminary design. He did not have firm timing but did say that construction of the project would not begin until 2025 or 2026.

Bob Butera asked about the other 36th Avenue projects he has been hearing about. Sean updated the group that the muni was repaving Old Seward, including near 36th Avenue, and replacing the traffic signal, which involved a temporary signal being installed.

Bob Butera also asked for an update on the Tudor Road project. Sean said that Anchorage water and sewer had recently completed a project on Tudor near the New Seward Highway. Sean then added that the Tudor Road Interchange project also needs federal funding in order to proceed.

Dave Evans asked about the schedule for the environmental documents. Sean said that there would be another Open House scheduled later this year. He said he was unsure if this would be before or after receiving federal funding for this project. He also said that the Statewide Environmental Office would make the call on the type of environmental documents that would be needed for this project.

Thank you,

Morgan McCammon
Public Involvement Specialist

DOWL

(907) 562-2000 | office

(907) 865-1269 | direct

dowl.com

Tudor Area Community Council Resolution 2021-03

A RESOLUTION OF THE TUDOR AREA COMMUNITY COUNCIL REGARDING THE SEWARD HIGHWAY 36th INTERCHANGE PROJECT

WHEREAS, on March 16, 2021, the Highway project team held its first public meeting; presented four highway alternatives for the interchange; stated that they are evaluating whether the Seward Highway should be elevated above 36th or depressed below 36th, with 36th remaining approximately at grade for all alternatives; and requested feedback on all project issues;

WHEREAS, in the March 16 open house, the project team stated that all highway alternatives for the 36th interchange, whether the highway passes over or under 36th, would be designed to allow future extension of the highway under Benson, Northern Lights, and Fireweed;

WHEREAS, in the March 16 open house, the project team stated that the DOT&PF had not decided whether an Environmental Assessment was needed; and

WHEREAS, the no-build and at-grade alternatives will not be evaluated unless an Environmental Assessment is performed;

Now therefore, be it resolved that the TACC continues to support:

1. a future depressed freeway through the council area, passing under 36th, as well as under Benson, Northern Lights, and Fireweed in the future;
2. non-motorized facilities along and across the highway corridor that are accessible, safe, buffered, well-lit, usable year-round, and designed to minimize the winter maintenance effort;
3. a maximum speed limit of 35 mph on frontage roads and 45 mph on the highway through the entire midtown area; and
4. an Environmental Assessment for the 36th Interchange Project.

From: [Sean Baski](#)
To: [Jim Amundsen](#); [Bowland, Luke S \(DOT\)](#); [Linnell, John R \(DOT\)](#); [Shannon McCarthy](#); [Starzec, James A \(DOT\)](#); [Gardner, Shawn C \(DOT\)](#); [david.post](#); [Vanhove, Todd E \(DOT\)](#); [Reese, Jill \(DOT\)](#)
Cc: [Farr, John D \(DOT\)](#)
Subject: [EXT] Tudor Area Community Council debrief 5-6-21
Date: Friday, May 7, 2021 7:21:21 AM

WARNING: External Sender - use caution when clicking links and opening attachments.

I attended the Tudor Area Community Council last night. Nothing major to report. On the agenda was the resolution similar to RPCC ([current draft](#)). They postponed voting to next month.

Notable attendees: Assemblyman Rivera, Assemblywoman Zaletel, Representative Josephson, and Senator Gray-Jackson (left prior to my discussion).

I was asked if I wanted to say anything about the project. I told them no new news, we are still working on enviro doc and designs.

Questions:

- President Butler – asked about high water table and under vs. over. – finding significant challenges with under – in the neighborhood of 750k gallons a day would need to be pumped out, where would you send it, contaminated sites next to the pumping, what would that mean, water levels just measured this spring 6 ft higher than previous measurements, so worse condition than what we were previously looking at. Looking into it, but extremely difficult. Maybe not feasible.
- Assemblywoman Zaletel – asked about the two phases identified in the PEL, interim then grade separation, what is going on – Long conversation – only get one bite at the apple for 36th. Interim solution at-grade is expensive and doesn't buy us a lot of years. Fiscally constrained. Decision was made to move forward without an interim project at 36th.
- Assemblywoman Zaletel – asked about alternatives being analyzed and which pushes you under vs over- looking at no build, median u-turn, loop ramp. Other than no build both can likely go under or over. Decision to go under vs. over would not be based on the selection of the interchange types.
- Assemblywoman Zaletel – asked for an update to her on when community engagement is going to occur/what the plan is for it, etc. Will follow up with her.

--

Sean Baski, P.E., Project Manager

State of Alaska DOT&PF, Central Region Highway Design Section

P.O. Box 196900, Anchorage, Alaska 99519-6900

Phone 907.269.0547 | Fax 907.248.1573

Keep Alaska Moving through service and infrastructure.

TUDOR AREA COMMUNITY COUNCIL

Meeting Agenda

September 2, 2021

7:00pm

NOTE: THIS MEETING WILL BE HELD VIRTUALLY

To join this Zoom meeting select the appropriate link below and then follow the instructions

<https://us06web.zoom.us/j/89386238382?pwd=Q04wNkQzbi9Fb1JLVENHeHltNFdwUT09>

Meeting ID: 893 8623 8382

Passcode: 525283

Dial by your location:

+1 253 215 8782 US (Tacoma)

+1 346 248 7799 US (Houston)

+1 669 900 9128 US (San Jose)

+1 301 715 8592 US (Washington DC)

+1 312 626 6799 US (Chicago)

+1 646 558 8656 US (New York)

Meeting ID: 999 0403 9317

Find your local number: <https://zoom.us/j/abA8fkfQzH>

AGENDA

CALL TO ORDER: Tod Butler, Interim
Chair

APPROVAL OF MINUTES FROM PRIOR MEETING:

The last meeting was May 6, 2021.

ANCHORAGE ASSEMBLY REPORT:

Assembly members Felix Rivera and Meg Zaletel

ALASKA LEGISLATURE REPORT:

State Senator Elvi Gray-Jackson
State Representative Andy Josephson
(or their representatives.)

SCHOOL BOARD REPORT:

Margo Bellamy

OLD BUSINESS:

1. Update on Seward Highway and 36th Interchange Project by Alaska DOT&PF project manager Sean Baski.
- 2.

NEW BUSINESS:

- 1.

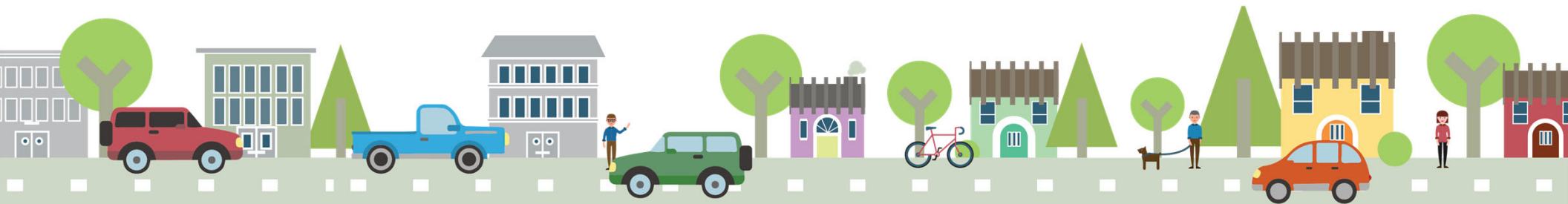
09.02.2021



36th avenue

INTERCHANGE

Seward Highway: 36th Avenue Interchange Tudor Area Community Council Meeting



PROJECT HISTORY AND BACKGROUND

- Midtown Congestion Relief Planning and Environmental Linkages (PEL) Study concluded in 2020 and recommended 5 projects
- 36th Avenue Interchange is the first project moving forward
- Interchange was studied in 2013-2015 but did not progress
- This is a fresh design and environmental permitting effort



WHY WAS THE 36TH INTERCHANGE SELECTED? — — — — —

- PEL Study recommended at-grade intersection improvements to construct frontage roads and implement two-phase signals
- Improved traffic flow on Seward Highway in short-term (modelled through 2028), but still had delays and safety issues associated with at-grade crossings (albeit much less than current)
- Full grade-separated interchange still needed to fully improve intersection past modelled 2028 volumes, improve safety, reduce delay above LOS F
- Concern that full interchange would be needed immediately on completion of at-grade project, creating several years of consecutive construction and rework of only recently completed investments
- Statewide Planning chose to move to full interchange immediately to overcome this concern and responsibly program constrained funds in STIP



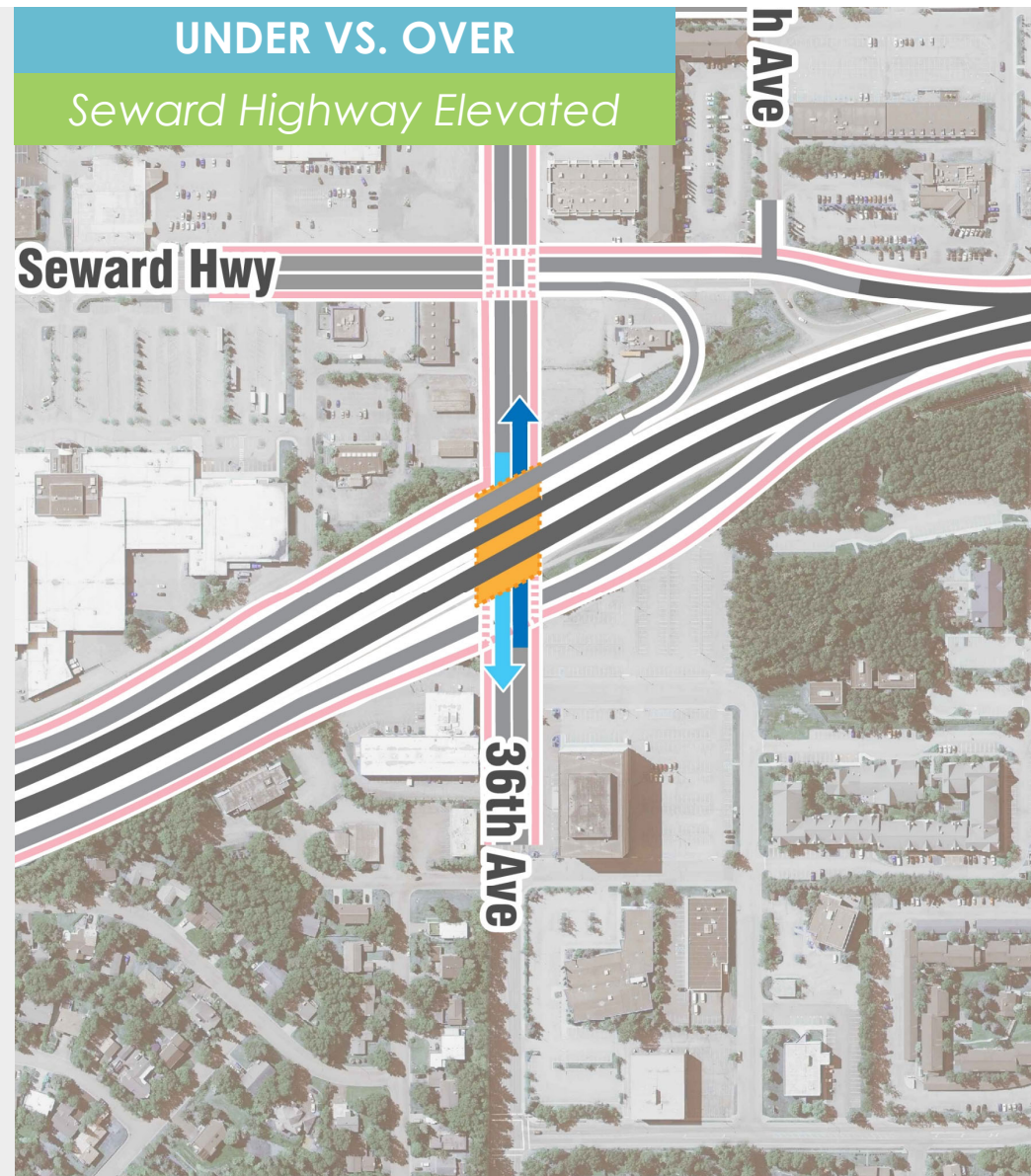
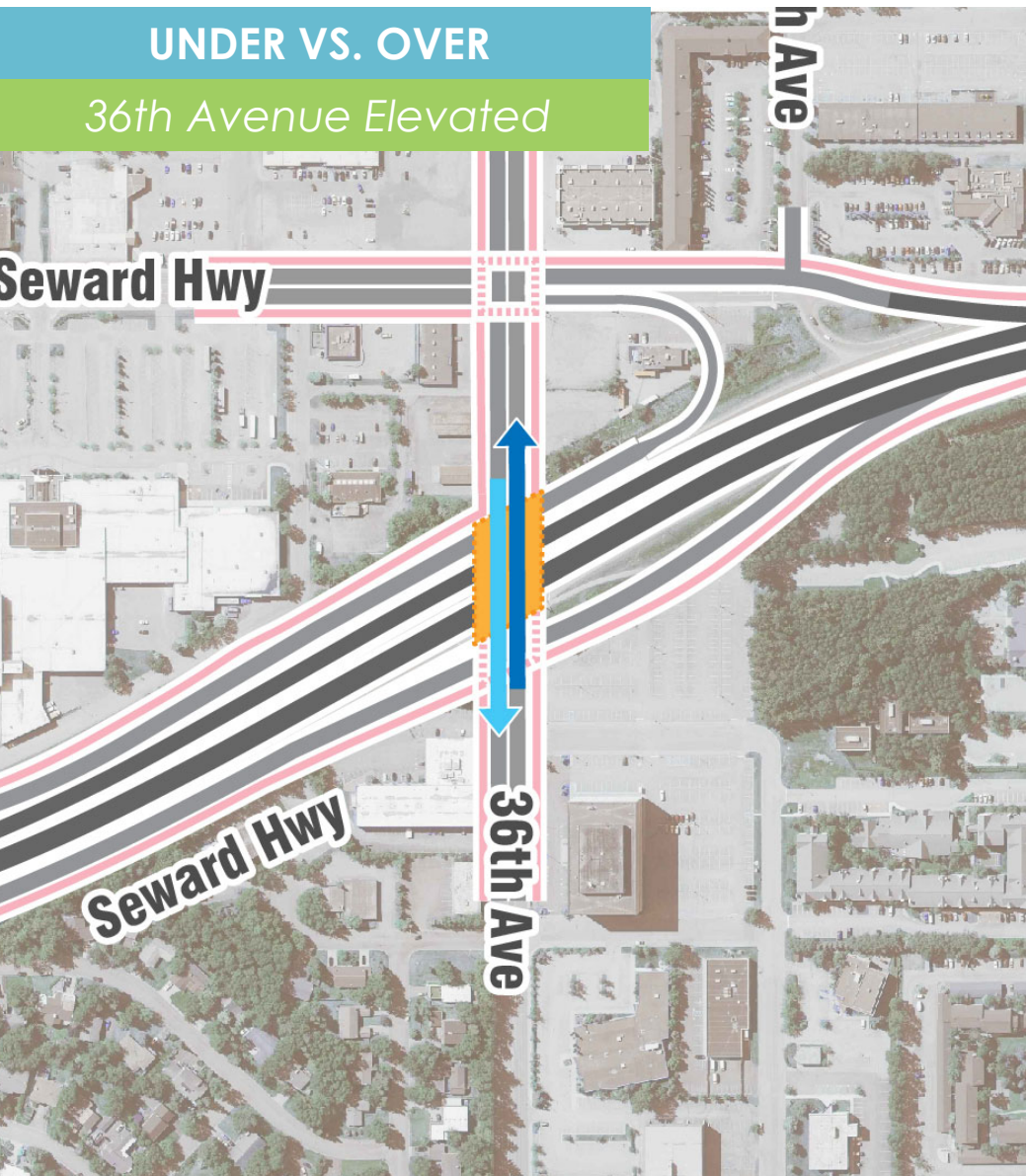
PROJECT STATUS — — — — —

- **Working on most promising alternatives:** Two loop ramp concepts
- **Refining vertical and horizontal geometry:** Minimizing vertical impacts
- **Initiating noise study:** 2022
- **Open House #2:** scheduled for late 2021

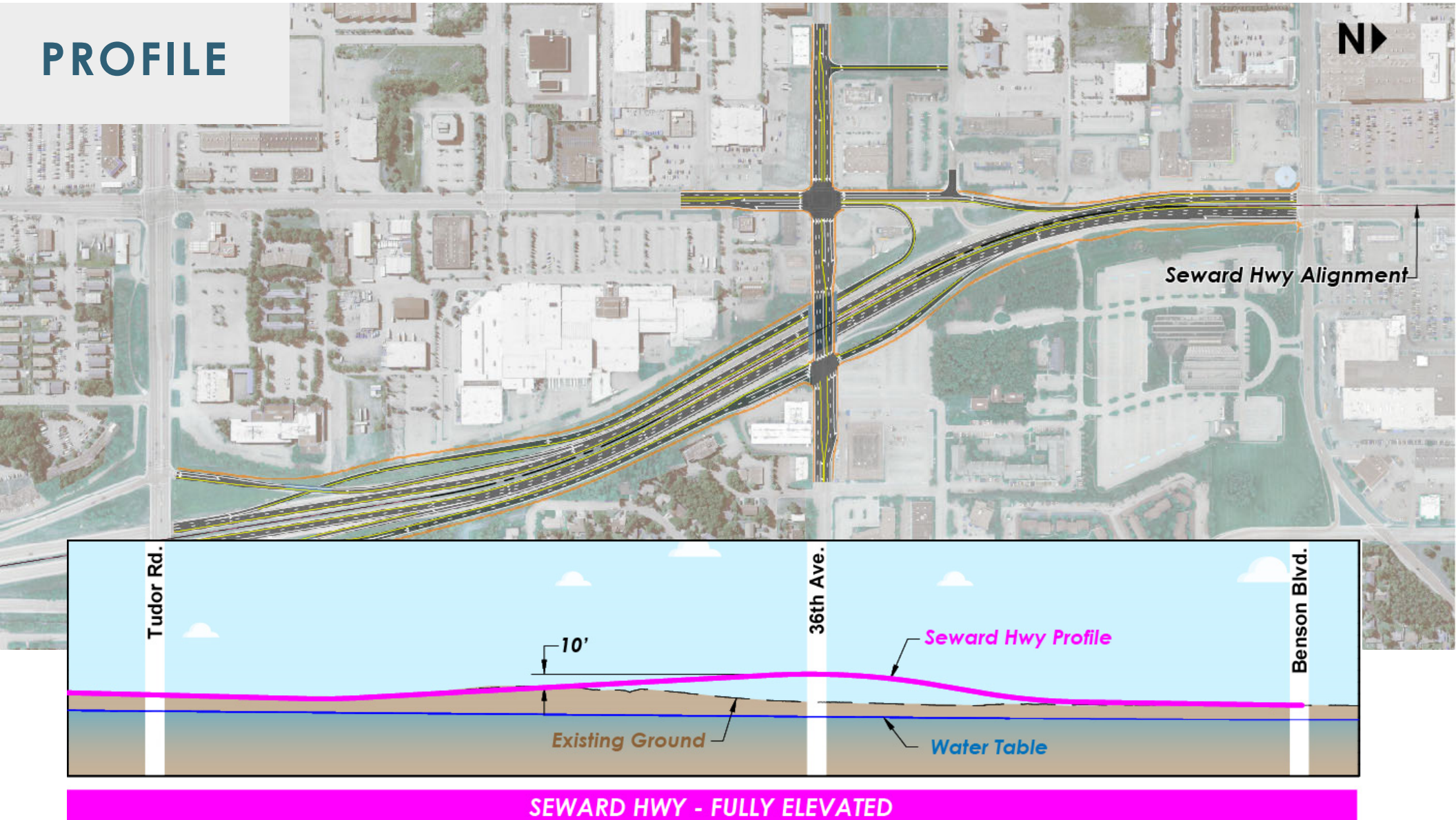
Everything at 36th Avenue is being designed to be forward compatible with a depressed freeway at Benson Boulevard, Northern Lights Boulevard, and Fireweed Lane, as recommended in the

PEL Study

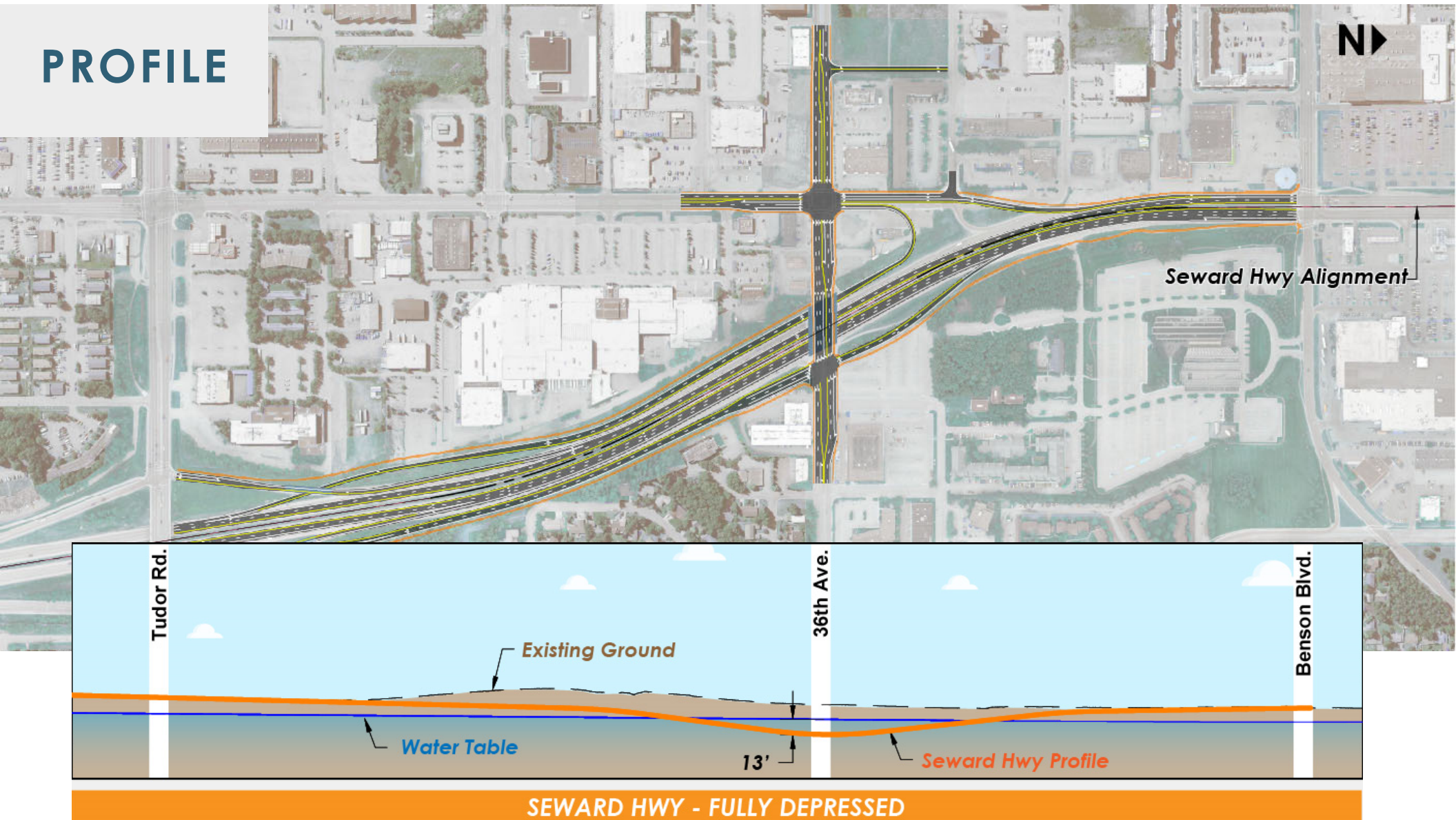




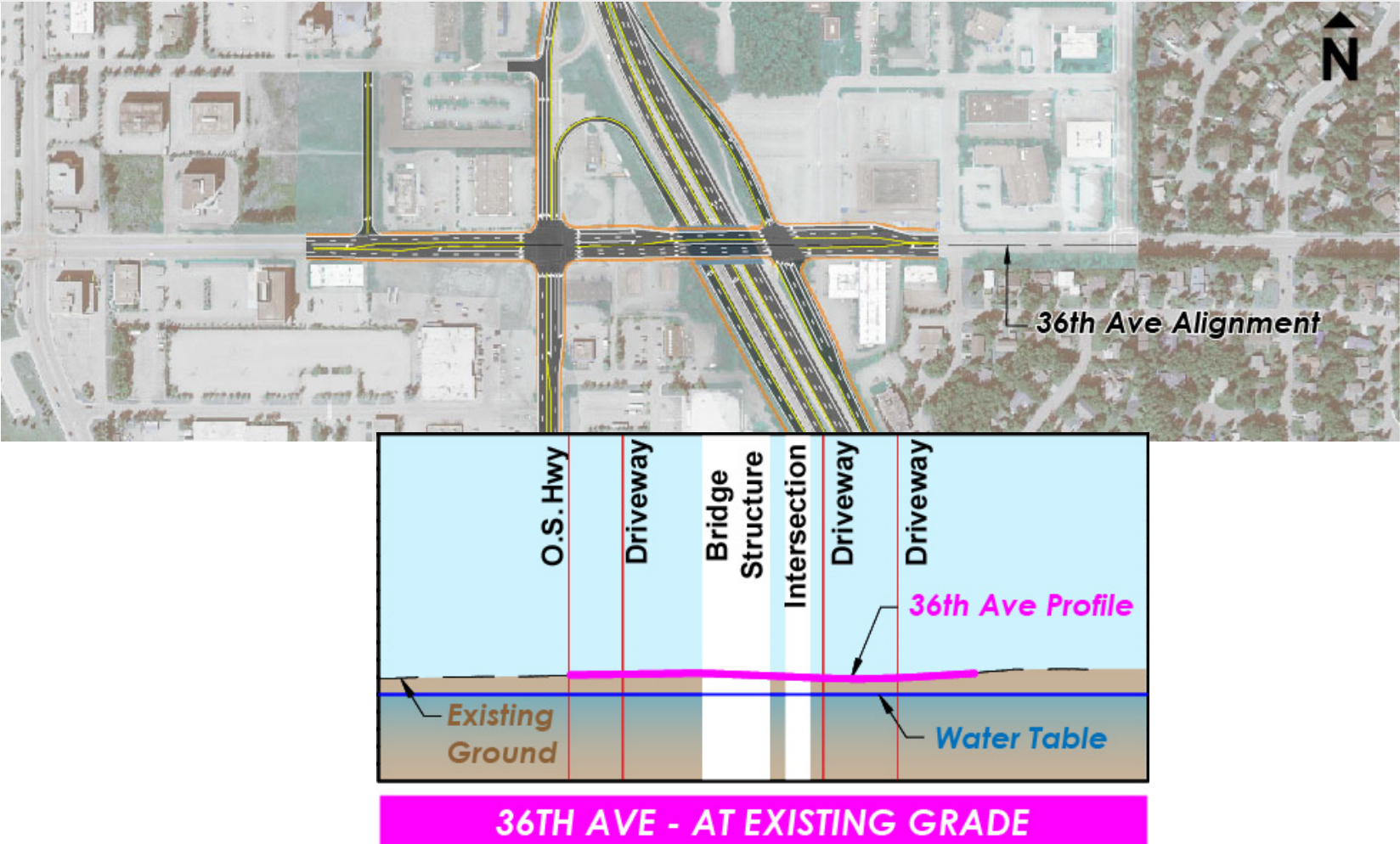
PROFILE



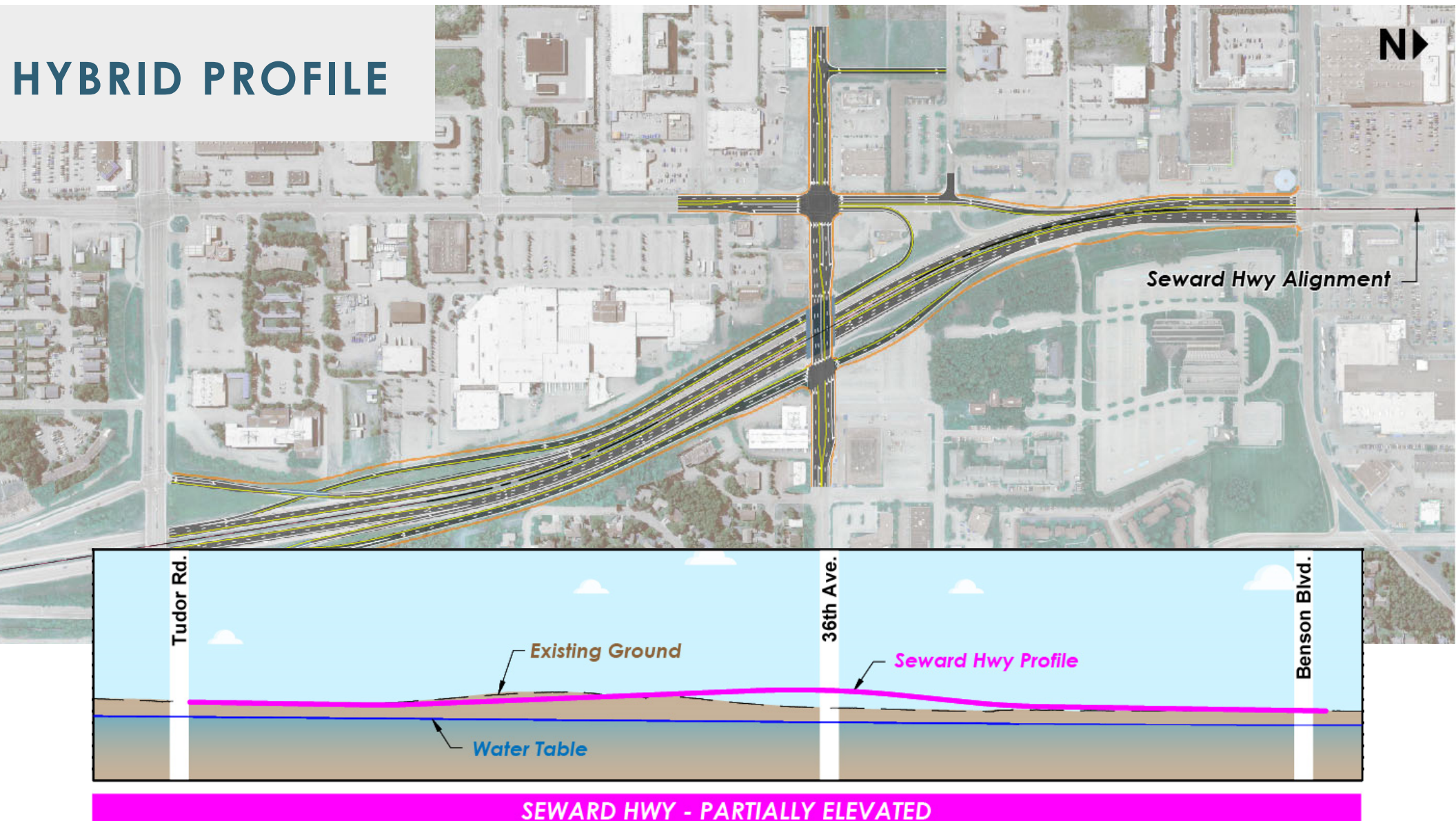
PROFILE



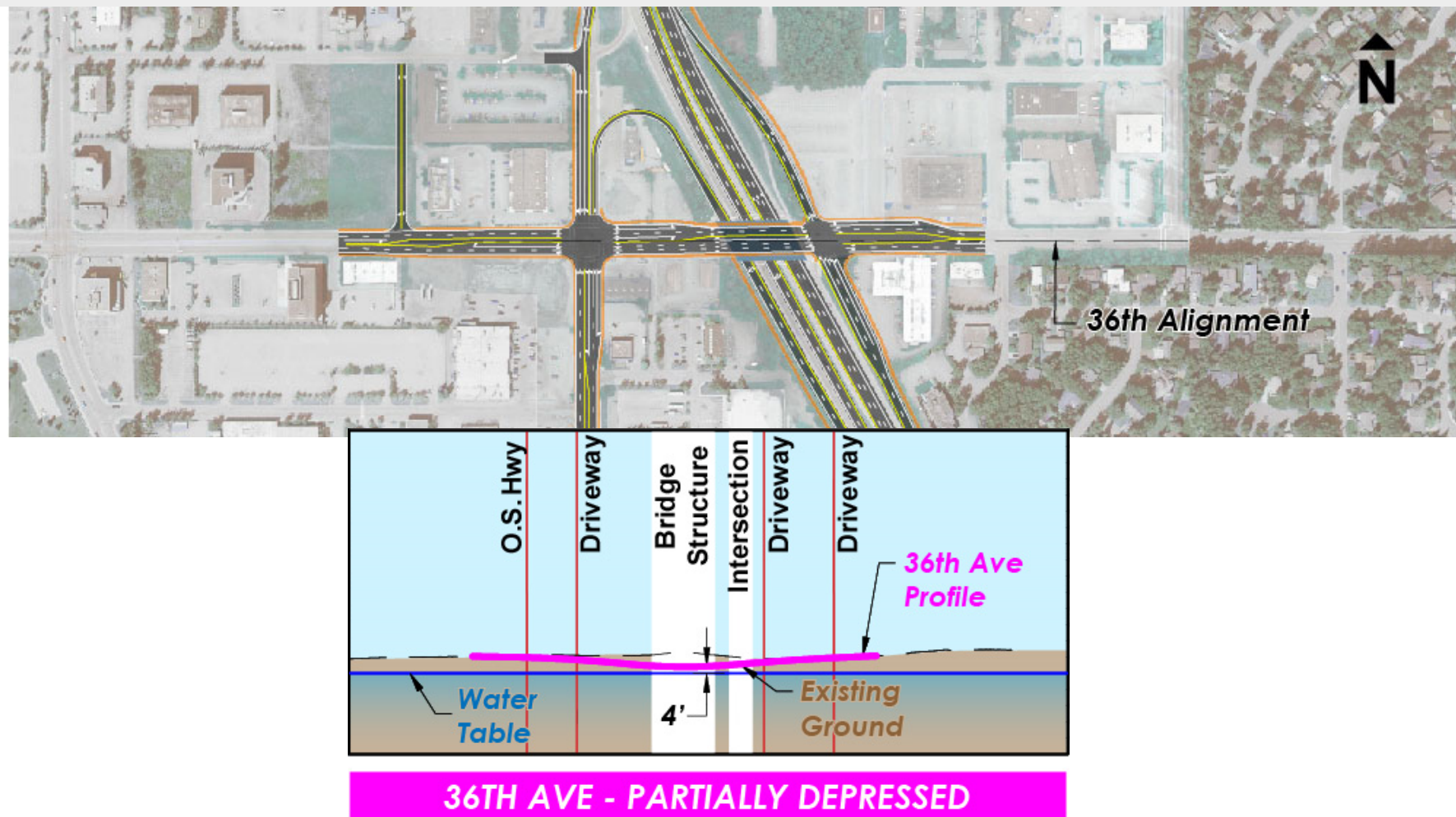
PROFILE



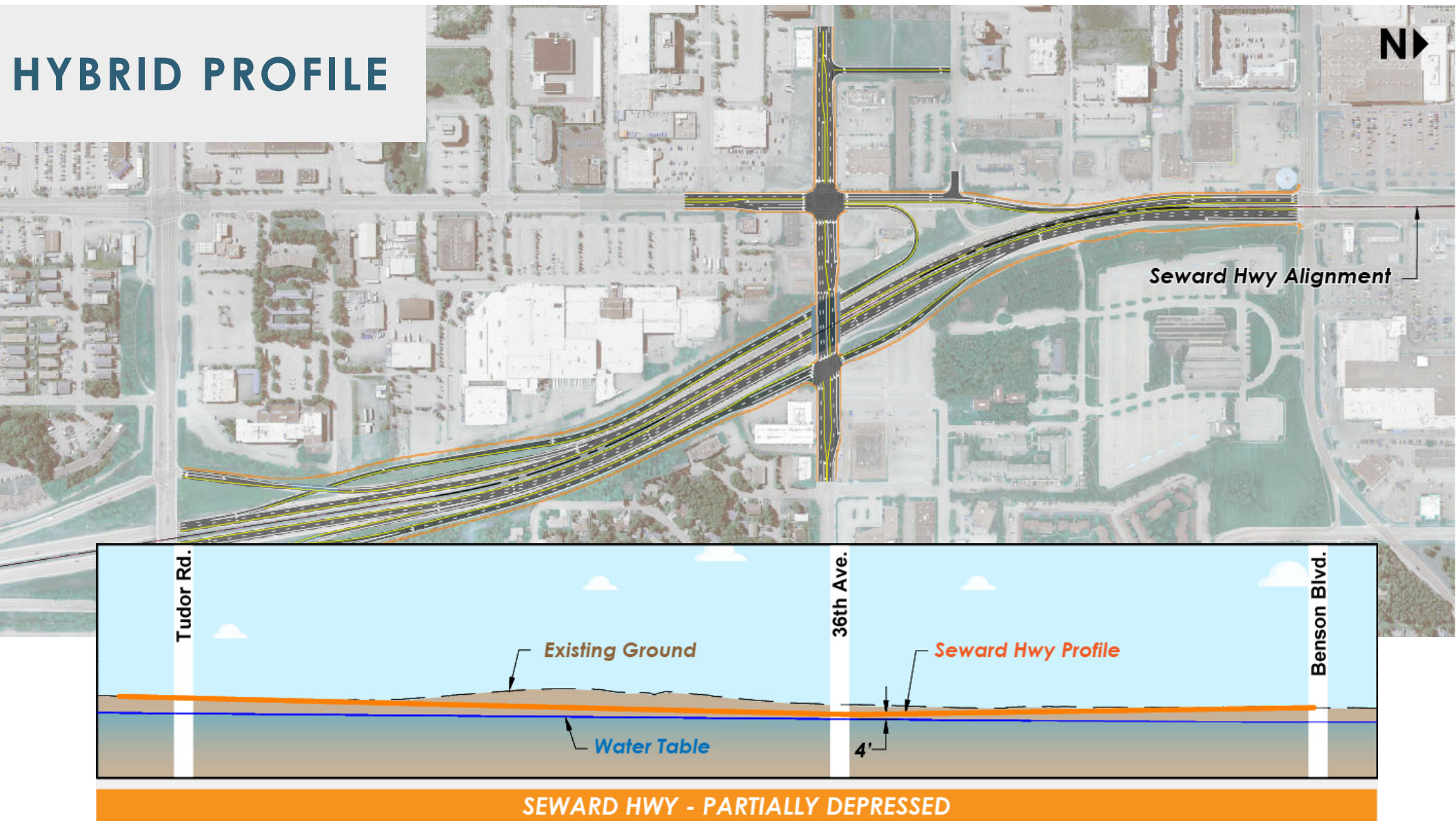
HYBRID PROFILE



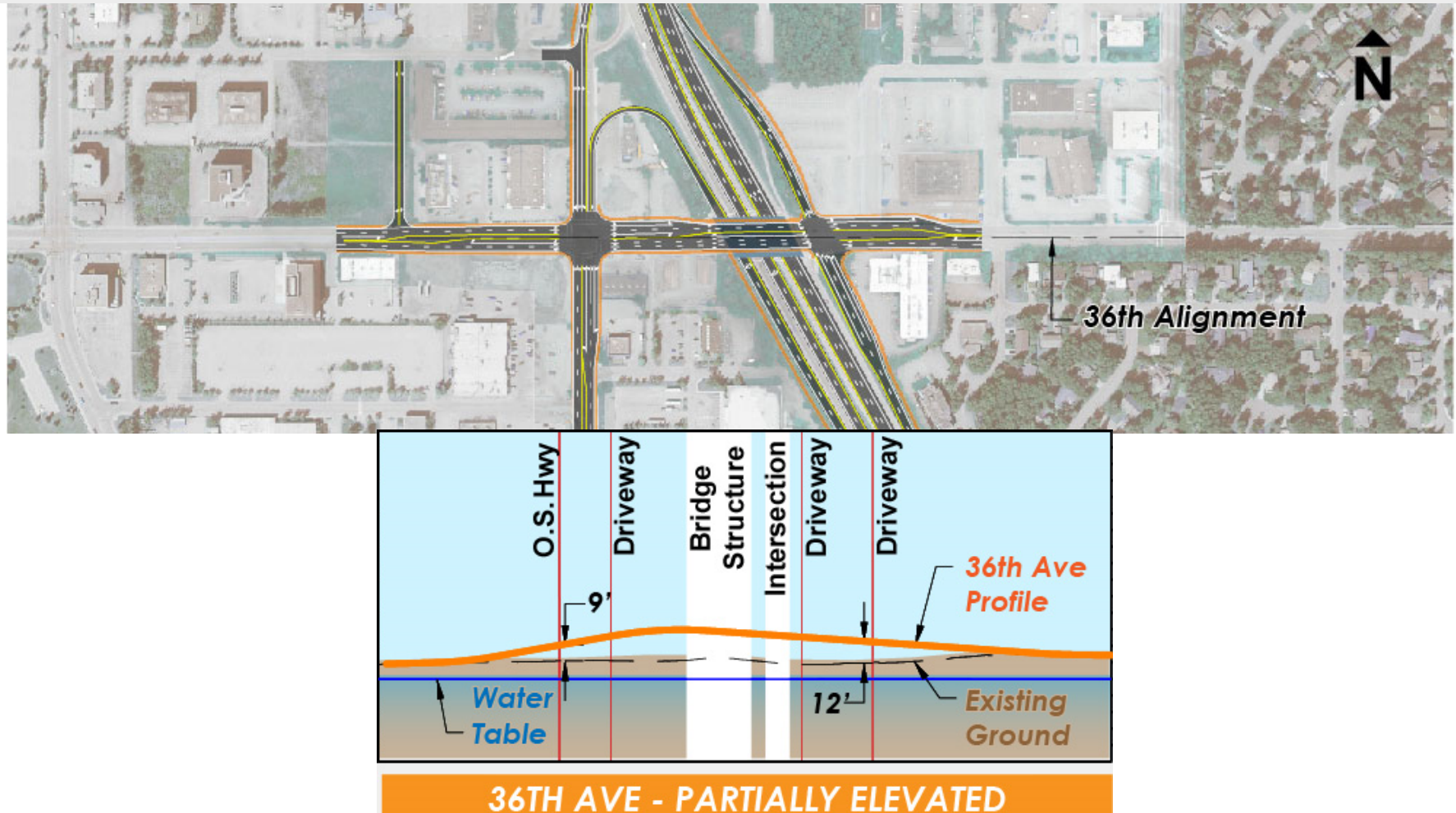
HYBRID PROFILE



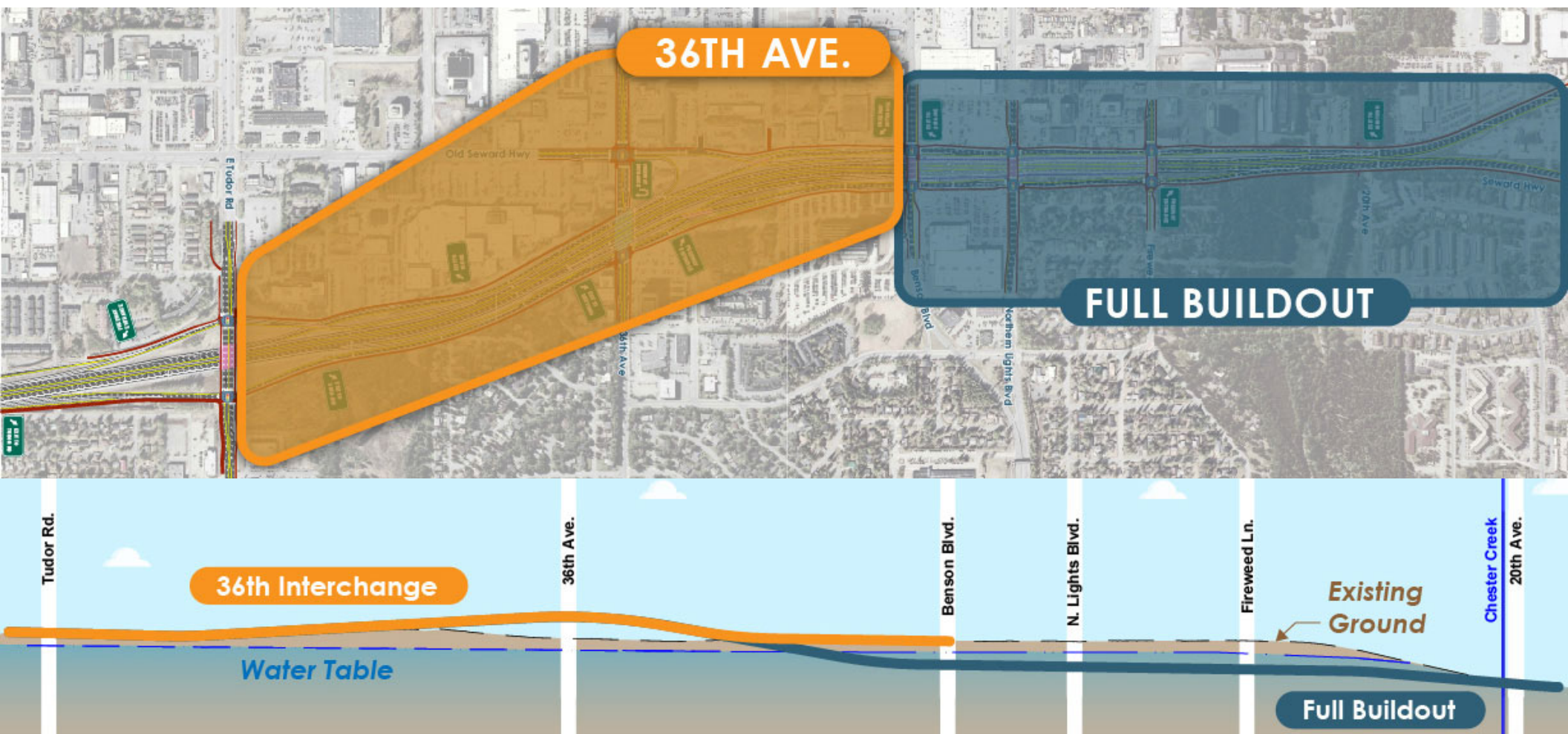
HYBRID PROFILE



HYBRID PROFILE



FULL BUILDOUT



OBSERVATIONS

- **Water Table:** high water table will require intersection to be pumped continuously (both groundwater and stormwater). Pumping volumes are significant and dewatering impacts could extend beyond the project area (currently being evaluated)
- **Contamination:** There is contamination on a property near the intersection, which will require treatment prior to discharge
- **Visual:** The entirely elevated highway is approximately 10 feet above the existing ground level between Tudor Road and 36th Avenue. Visual renderings are going to be prepared to show indicative views for upcoming open house
- **Noise:** Noise Study methodology has been developed and is currently under review. There are 7 residential properties that are “first row receivers” (immediately next to the project area)
- **Driveways:** Driveways on 36th Avenue are very close to the intersection, which create challenges with elevating or depressing 36th Avenue to reduce the height of an overpass



SCHEDULE

Task	Date
PEL Study	2017-2020
Public Involvement	Ongoing
Preliminary Environmental & Design	2020 - Ongoing
Final Design/ROW Acquisition	TBD
Construction	TBD





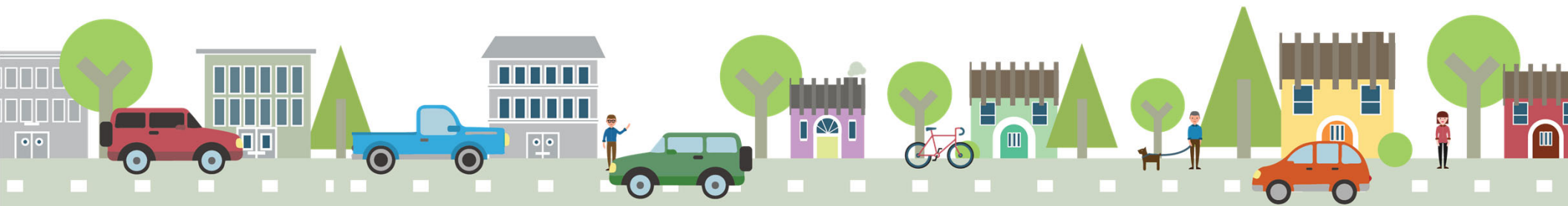
36th avenue INTERCHANGE

Thank You

Questions or Comments?

36thinterchange@dowl.com

www.36thinterchange.com



From: [Renee Whitesell](#)
To: [Sean Baski](#); [Farr, John D \(DOT\)](#)
Cc: [Steven Noble](#); [Katie Conway](#); [Morgan McCammon](#); [Rachel Steer](#)
Subject: Tudor Area Community Council
Date: Thursday, September 2, 2021 8:35:23 PM

Sean Baski and Renee Whitesell attended the Tudor Area Community Council meeting this evening, Thursday September 2. Sean provided an update presentation on the 36th Avenue interchange project, specifically focusing on early engineering to explore the profile of the interchange.

18 people were in attendance at the meeting. Notable attendees included Sen. Elvi Gray-Jackson, and Assembly Rep. Felix Rivera.

Several meeting attendees were Board members for the Geneva Woods HOA, including President Dave Forsland, and Board Member William Gornto.

Questions included:

- **B Gornto:** raised concerns about the elevated highway and noise – he gets some comfort that the road isn't proposed to be fully elevated from Tudor Road. He questions why we are planning to go under Benson, Northern Lights, and Fireweed as he believes there will still be water table concerns. He wants to ensure 36th isn't treated differently and is creating noise and visual concerns. Sean explained the need to view the project on its own as that is how it is proceeding, and noted there is a pooling issue, with nearly 1M gallons of water needing to be removed daily. He acknowledged the projects will have other issues too, that will need to be evaluated as part of the environmental process.
- **C Quigley:** asked about the 7 adjacent properties and whether these are the only ones that will be considered for noise. Sean noted the noise study will be publicly available and will apply national standards. He also noted that businesses, including the hotel and medical facilities would be considered and there are different noise categories that will be evaluated. Measurements are needed at multiple businesses.
- **D Forsland:** asks whether the existing barrier adjacent to Geneva Woods will be taken, and whether any part of Helvetia and St Gotthard will be taken. Sean noted if there is an existing noise barrier, then there will be one. It is too early to comment on where it would be located.
- **D Forsland:** noted there was a proposal at one time to make Helvetia and St Gotthard dead end streets and this would destroy the neighborhood – he wanted to know when he can raise concerns about the impacts to the neighborhood. Sean noted we do not want to adversely impact the neighborhood and this will be considered as the design progresses. He also noted that comments made previously have been documented.
- **D Forsland:** enquired when the right-of-way impact will be known. Sean noted this will not be until after the environmental document gets approved to move forward to environmental design. What is being presented today is still illustrative and there is no detail in the design yet.
- **D Forsland:** noted that he has seen projects where roads have destroyed neighborhoods and devastated homes – he views this as very concerning for himself and the neighborhood. He wants to see the final design and have the opportunity to voice concerns about impacts on neighbors.
- **T Butler (TACC President):** noted that sometimes on projects there is a need to balance the public good and private property. He said sometimes the common good will override personal benefits – would there be an option to consider purchasing the first row of homes and ensure

residents are informed and fairly compensated? Sean noted the purpose of the project is to connect the community east-west, provide sidewalks and pathways, and that this is the second highest crash occurrence intersection in the city. This project will move traffic more efficiently, but its core benefit is saving lives and connecting communities.

- **D Forsland:** enquired whether we are moving the problem down to 15th Avenue? Sean noted he does not think this is where the problem would reside.
- **W Gornto:** commented that if you are a homeowner and the state reduces the value of your home, then you would want some assurance that they would be made whole for the impact. He also noted the first row of homes aren't the only ones impacted – the sound would emanate out beyond the first row of homes. Sean noted the noise analysis would not just consider the first row, we are looking beyond this. For making people whole for the assumed reduction in property value, the goal is to increase the value of property through safer and improved access. He acknowledged there are some situations where properties are impacted. There are no means to make property owners whole unless there are right-of-way impacts.
- **W Gornto:** noted that hearing this, his strong position is that Seward Highway needs to go under 36th Avenue, and water needs to be pumped out daily. Sean noted at this point the highway will be 3-4' above the existing roadway, and residents will not even see the highway. Noise is still to be evaluated. He further noted that perhaps there is some middle ground that could be considered.
- **W Gornto:** commented he would like to invite the project team to present at the local neighborhood meeting. Sean said we would be happy to meet.
- **D Forsland:** noted he is less concerned about the elevation and more concerned about the widening of the highway and whether this will push the existing right-of-way closer to the residential property, as that will affect resale value. Sean discussed the balance between hearing from the public that we need to provide full access at 36th Avenue, and this does drive the width of the highway near the proposed interchange.

Renee has sent a follow up email to Mr Forsland and Mr Gornto to move forward with organizing a meeting with the Geneva Woods Homeowners Association.

Thanks,
Renee

Renee Whitesell, PTP
Senior Project Manager

DOWL

(907) 562-2000 | office
(907) 865-1161 | direct
(907) 602-6812 | cell

dowl.com

**APPENDIX C: STAFF COMMENTS FROM PRE-APPLICATION
CONFERENCE AND INFORMAL CONCEPT REPORT REVIEW**

Notes from CSS Pre-Application Conference
Seward Highway: 36th Avenue Interchange Project
December 9, 2021

Presentation

- Steve Noble with DOWL gave a presentation about the project.
- This project comes from the Midtown Congestion Relief PEL Study, and it is the first project identified in that study to move forward. It is not in the STIP yet; it is state-funded so far, but won't happen without federal funds. Alaska DOT & PF working on getting it into the STIP. The improvements are intended to improve safety, improve pedestrian and bike facilities, reduce congestion, improve access and connectivity.
- The PEL study included significant improvements to the north and south of the interchange and want to make sure our project is compatible with both.
- Sean Baski with Alaska DOT & PDF added that the concepts shown today are more advanced than the exhibits that have been previously presented. The project team worked with the community to get to the current alternatives: Seward Highway over 36th, Seward Highway under 36th, or a hybrid of the two options.
- If the Seward Highway would go under 36th Avenue, the new roadway profile would be 13 feet below the water table. It is not an entirely impossible design, but it would be expensive with high maintenance costs. The hybrid option would partially depress 36th Avenue over a partially elevated Seward Highway (so Seward Highway not as tall as the non-hybrid option, but still Seward Highway over 36th).
- The PEL study showed strong want for bike lane and separated pathway. The shallow water table in the interchange facility makes it a challenge to tunnel go under the Seward Highway.
- When asked to clarify on/off ramp plans and ROW, Steve discussed driveways north of 36th Avenue that would no longer have direct access to the Seward Highway. The sushi restaurant complex building would become right-of-way. There would be a new access route for Moose's Tooth and also Kinley's/Big Ray's. Sean added that these proposed designs are still in flux and could change.
- Key issues:
 - ROW acquisition
 - High water table
 - Contamination
 - Visual
 - Noise
 - Driveways
 - Wetlands/Sanctuary
 - Maintenance

Current Planning

- Planners noted public involvement challenges with several community councils and a homeowners association already activated over the homeless shelter and treatment center issues, Golden Lion property, and other projects in the area. Steve noted the project team has

been to several area community council meetings. Elizabeth Appleby with Municipality of Anchorage Planning Department added the project team should be sure to address the public outreach and public feedback in the DSR document.

- Elizabeth reviewed the CSS submittal process by referencing the table below in AMC 21.03.190:

TABLE 21.03-4: STREET AND INTERSECTION PROJECT REVIEW		
REVIEW	REVIEW BODY	PRIMARY DECISION
Concept Report	Project Management Team	Issue identification, "go, no-go" decision
Draft Design Study Report	Planning and Zoning Commission	Alternatives development, evaluation and screening criteria, alternative decision
Plans in Hand Design Drawings	Urban Design Commission	Approval of plans at 65 percent stage, including landscaping "theme"

- Francis McLaughlin with Municipality of Anchorage Planning Department reminded the project team that their application for the street review will need to explain how the street project meets planning documents. He pasted in the chat and projected the [2010 Anchorage Bicycle Plan](#) as one such planning document. Elizabeth also provided a link to the [Draft Non-Motorized Plan](#), which carries forward some items in the *2010 Anchorage Bicycle Plan*. The project will also need to be consistent with the [2040 Land Use Plan \(adopted 2017\)](#) and other area-specific and functional plans, including these from [AMATS](#) and from [Planning](#).
- Francis added that often the application for street review will address landscaping, fencing and sound barriers. Some sort of landscape plan or clarifying that in the drawings would be helpful, such as where fences or sound barriers are and what types will be used (or even to state that no landscaping will be provided). Show any bridges and wetlands/streams impacts. Those are typical questions that come up at these types of meetings.
- Sean said the project team will probably have detailed conversations with Planning on how to proceed on a number of those things. Ultimately, this project is not in final design. Noise walls are installed where they meet federal and state policy. We are not complete with the noise analysis yet; we know that where there are existing noise walls they will likely stay. We are in the very preliminary designs. DOT is always up for partnering opportunities, but DOT will not maintain landscaping.
- Elizabeth added to speak to utilities and storm drains as well for the project.

Long Range Planning

- Tom Davis in Long Range Planning asked how this would tie to other nearby projects. All projects would need to be consistent with planning documents.

- Daniel McKenna-Foster in Long Range Planning commented after the meeting that in some materials safety is the priority and in other it is vehicle flow. Those priorities can support different intents for the project. Clarify intent of changes in application.
- Long Range Planning will review plans as they have some concerns the project may not be consistent with them. Long Range Planning will provide additional follow-up comment.

Private Development

- Greg Soule with Private Development said he had no comments at this time. There are no S/A's in this project vicinity, so no conflicts. The access changes to reach Moose's Tooth will be beneficial; the current intersection has been problematic.
- Judy Anunciacion with Private Development asked if this project was related to signals that were replaced this summer. Sean replied it was not. The signal replacement was a Municipality of Anchorage project and this is a State of Alaska DOT & PF project. The lights would likely be completely revamped for the new project design.

Traffic Engineering (with some input from Current Planning on parking)

- Randy Ribble with Traffic Engineering asked if was there a current case for this project? Elizabeth replied that the Concept Report had been submitted and schedule as an information item before the Planning and Zoning Commission in January 2022. The link to Case 2022-0011 is here: <https://www.muni.org/CityViewPortal/Planning/Status?planningId=17496>
- Randy said that overall, Steve has done a good job. He doubts that Traffic Engineering will have any issues with the Concept Report level design; he has no comments at this stage of the project.
- Randy mentioned the importance of signs. Elizabeth added that it has been a common question to get asked about signs by Commissioners. This particularly true to pedestrian pathway signage or intersection signage. Be prepared to submit plans and answer questions about sign location and sign design at the Plans in Hand phase.

DOT Planning

- No comments at this time.

APPENDIX D: MCR PEL STUDY WETLAND DELINEATION



TECHNICAL MEMORANDUM

TO: U.S. Army Corps of Engineers
FROM: Alaska Department of Transportation and Public Facilities, Central Region
DATE: {DATE}
PROJECT: Midtown Congestion Relief

BACKGROUND

This Technical Memorandum serves to provide the United States Army Corps of Engineers (USACE) with sufficient information to determine regulatory jurisdiction of potential resources subject to Section 404 permitting under the Clean Water Act (CWA). The USACE is the jurisdictional agency with authority to permit the discharge of dredged or fill material into a Waters of the United States (WOUS) per Section 404 of the CWA. Outlined within the CWA, wetlands are categorized as "Other WOUS."

PROJECT INFORMATION

The State of Alaska Department of Transportation and Public Facilities (DOT&PF) is currently evaluating improvements to the Seward Highway corridor from the Tudor Road interchange to approximately the 20th Avenue intersection through Midtown Anchorage, Alaska (61.1935 North Latitude; 149.8678 West Longitude) (Figure 1).

DOT&PF determined the extent of the study area based on possible areas of impact from future road expansion. Within this study area are wetlands mapped by the Municipality of Anchorage (MOA) for planning purposes.

DOWL has conducted a Preliminary Wetland Delineation of an approximately 13.4-acre study area to identify areas that may fall under the United States Army Corps of Engineers (USACE) jurisdiction per Section 404 of the Clean Water Act (CWA). The intent of this delineation is only to establish jurisdictional limits potentially affected by future projects; therefore functions and values were not assessed.

The data herein is intended to provide the USACE with sufficient information to determine regulatory jurisdiction of aquatic resources subject to Section 404 of the CWA, and to evaluate the hydrological connectivity of such resources to a traditional navigable waterway, territorial sea, or navigable interstate waterway.

METHODS

DOWL staff conducted the wetland delineation fieldwork on September 11 and 18, 2018 to verify or update Anchorage Wetland Management Plan wetland maps. *Part IV of the 1987 Corps of Engineers Wetlands Delineation Manual* and the *2007 Regional Supplement to the Corps of Engineers Wetland Delineation Manual: Alaska Region Version 2.0* were used for verification and adjustment as needed.

Data for each sampling site's vegetation, topography, hydrology indicators, and soil profiles was recorded on Corps Routine Wetland Determination (RWD) forms and in photos (Attachment 1). In the event soil excavation was not necessary to make a wetland/upland determination, a

photographic point was taken. Using ArcMap GIS, an aerial was used as a basemap to digitally map wetland boundaries.

FINDINGS

DOWL staff completed three RWD forms and photographed 17 locations to document conditions, as shown in Table 1.

Table 1: Data Summary Collection

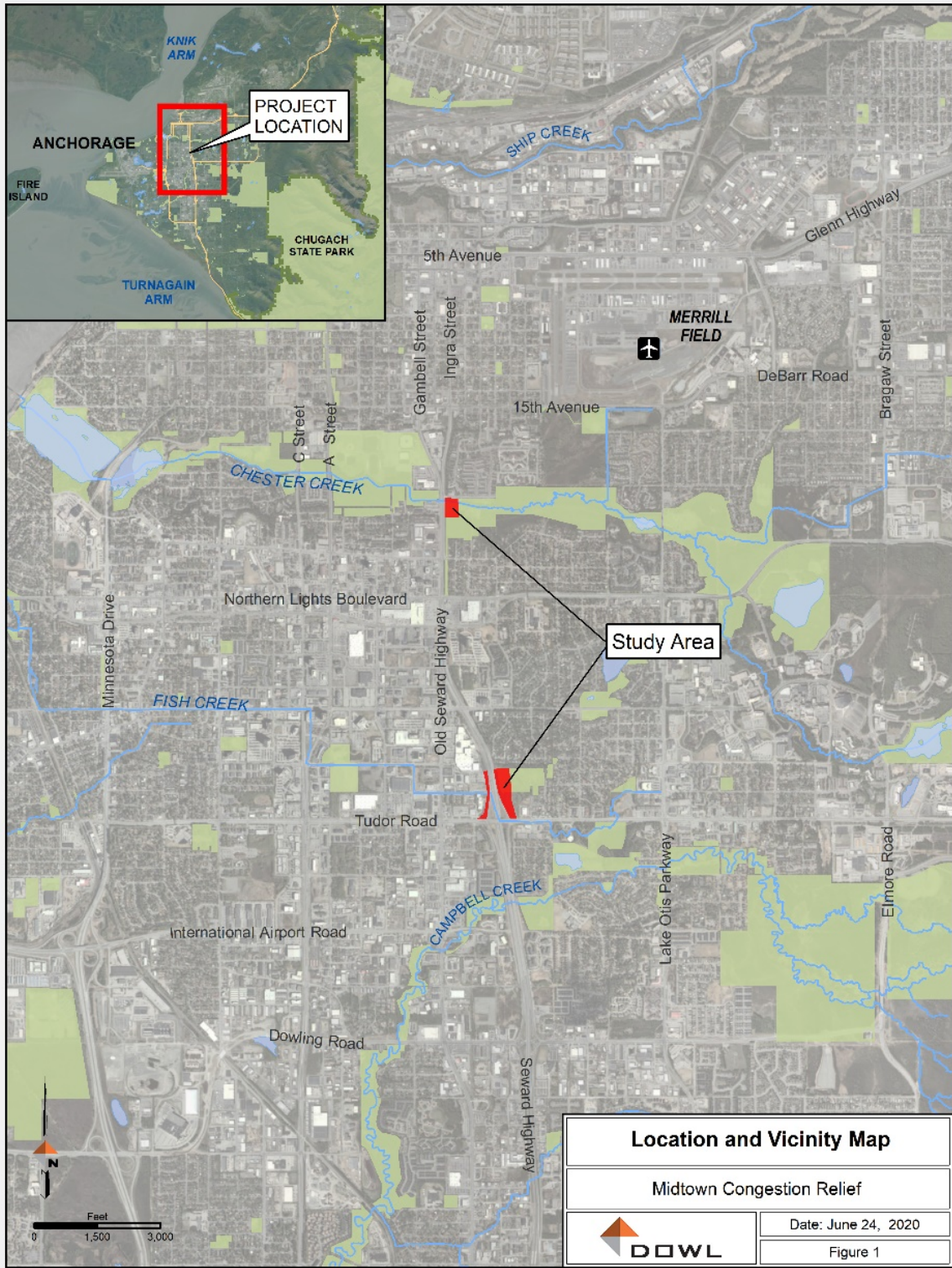
Data #	RWD Form	Jurisdictional Status	MOA Wetland Designation
1	Photo	Wetland	Wetland
2	Photo	Wetland	Upland
3	Yes	Upland	Upland
4	Yes	Wetland	Upland
5	Photo	Wetland	Upland
6	Photo	Upland	Upland
7	Yes	Wetland	Wetland
8	Photo	Wetland	Wetland
9	Photo	Upland	Upland
10	Photo	Upland	Upland
11	Photo	Upland	Upland
12	Photo	Wetland	Upland
13	Photo	Upland	Upland
14	Photo	Wetland	Upland
15	Photo	Wetland	Upland
16	Photo	Wetland	Upland
17	Photo	Wetland	Upland
18	Photo	Wetland	Wetland
19	Photo	Wetland	Wetland
20	Photo	Wetland	Wetland
21	Photo	Upland	Upland

DOWL identified 1.67 acres of wetlands, of which 1.11 acres are mapped as wetlands by the MOA. Of the 1.67 acres identified, 1.54 acres are connected hydrologically to Fish Creek via surface and subsurface drainage infrastructure (Figure 2). Fish Creek is a Relatively Permanent Water which directly connects to Cook Inlet, a Traditionally Navigable Water (TNW) therefore all wetlands connected to Fish Creek are considered a Waters of the U.S.

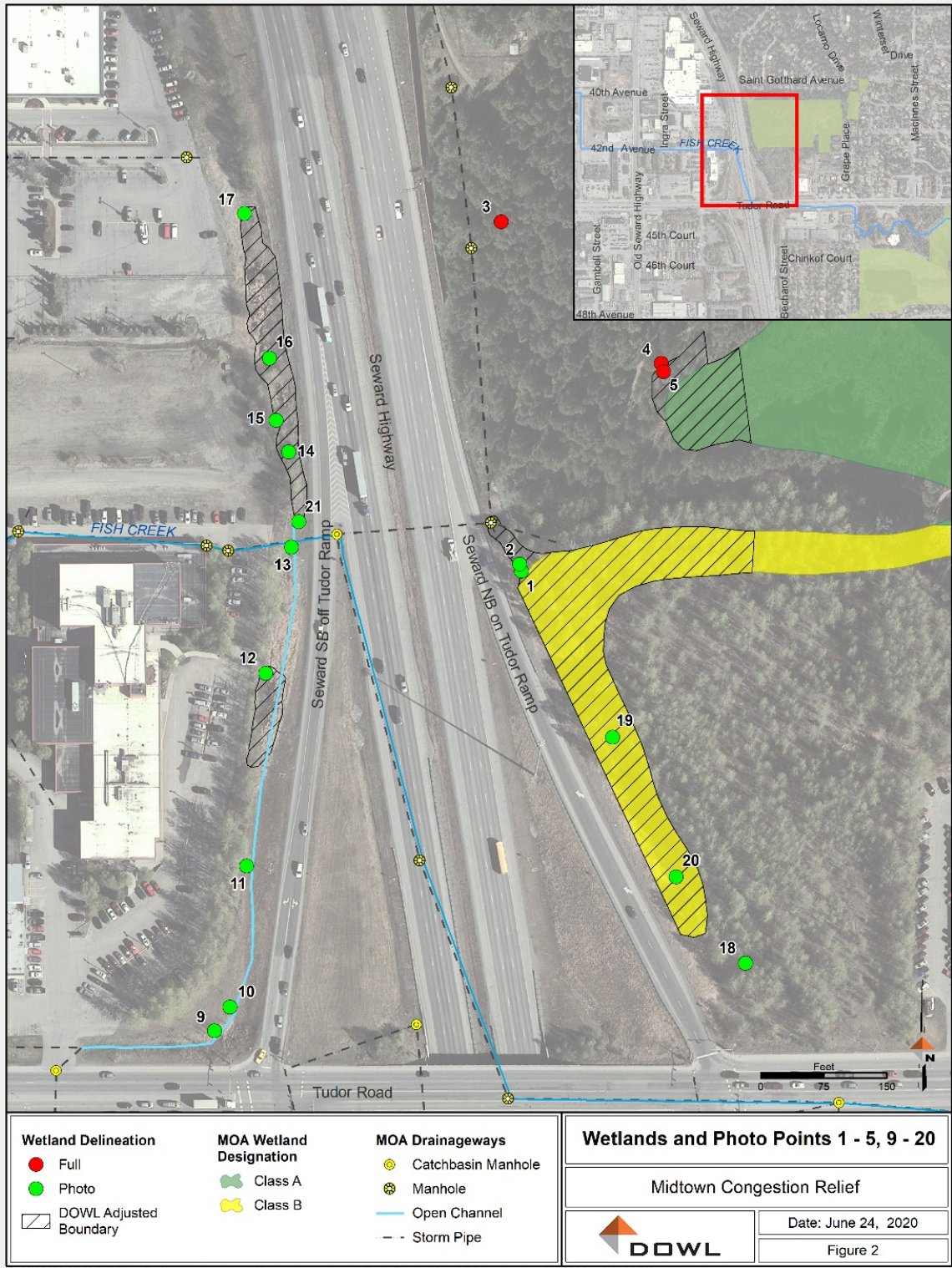
Approximately 0.14 acres of wetlands mapped near the Chester Creek trail lacks a surface connection to Chester Creek and is only linked hydrologically through subsurface flow (Figure 3).

Boundaries between uplands and wetlands determined by the MOA were shifted as follows (Figure 2, 3):

- Wetland boundary represented by data points 1, 2, and 18-20 was shifted 50 feet north (represented by) due to proximity to the storm water culvert.
- Wetland boundary represented by data points 4 and 5 was shifted 25 feet north as the wetland has grown closer to the southeast side of the boardwalk.
- Area represented by data points 12, and 14-17 were previously mapped as uplands, however wetlands have developed by the storm water culvert in a swale (approximately 115 ft by 30 ft), and in another area along the fence line (approximately 400 ft by 1 ft). Wetland vegetation is present throughout, most likely formed from a mixture of seasonal melt and storm water drainage.
- Wetland boundary represented by data point 7 was shifted approximately 125 feet to the north and the wetland area is smaller than previously mapped. The wetlands were moved north and out of the forest and into the grassy field.



TECHNICAL MEMORANDUM





ATTACHMENT 1: WETLAND DATA FORMS AND PHOTO LOG

WETLAND DETERMINATION DATA FORM – Alaska Region

Project/Site: MIDTOWN CONGESTION RELIEF Borough/City: ANCHORAGE Sampling Date: 9.11.2018
 Applicant/Owner: DOT & PF Sampling Point: 03
 Investigator(s): A.Morrill & T.Jameson Landform (hillside, terrace, hummocks, etc.): flat
 Local relief (concave, convex, none): Convex Slope (%): 0-1
 Subregion: Southcentral Alaska Lat: 61.18357 N Long: 149.8604 W Datum: _____
 Soil Map Unit Name: Cryorthents & Urban land, 5 to 20% slopes NWI classification: _____

Are climatic / hydrologic conditions on the site typical for this time of year? Yes X No _____ (If no, explain in Remarks.)
 Are Vegetation no, Soil no, or Hydrology no significantly disturbed? Are "Normal Circumstances" present? Yes X No _____
 Are Vegetation no, Soil no, or Hydrology no naturally problematic? (If needed, explain any answers in Remarks.)

SUMMARY OF FINDINGS – Attach site map showing sampling point locations, transects, important features, etc.

Hydrophytic Vegetation Present? Yes _____ No <u>X</u>	Is the Sampled Area within a Wetland? Yes _____ No <u>X</u>
Hydric Soil Present? Yes _____ No <u>X</u>	
Wetland Hydrology Present? Yes _____ No <u>X</u>	
Remarks:	

VEGETATION – Use scientific names of plants. List all species in the plot.

Tree Stratum	Absolute % Cover	Dominant Species?	Indicator Status	Dominance Test worksheet: Number of Dominant Species That Are OBL, FACW, or FAC: <u>3</u> (A) Total Number of Dominant Species Across All Strata: <u>6</u> (B) Percent of Dominant Species That Are OBL, FACW, or FAC: <u>50</u> (A/B)
1. <u>Betula papyrifera</u>	<u>60</u>	<u>X</u>	<u>FACU</u>	
2. <u>Picea glauca</u>	<u>3</u>		<u>FACU</u>	
3. <u>Populus balsamifera</u>	<u>10</u>		<u>FACU</u>	
4. _____				
Total Cover: <u>73</u>				
50% of total cover: <u>37</u> 20% of total cover: <u>15</u>				
Sapling/Shrub Stratum	Absolute % Cover	Dominant Species?	Indicator Status	Prevalence Index worksheet: Total % Cover of: _____ Multiply by: _____ OBL species <u>70</u> x 1 = <u>70</u> FACW species <u>0</u> x 2 = <u>0</u> FAC species <u>28</u> x 3 = <u>84</u> FACU species <u>98</u> x 4 = <u>392</u> UPL species <u>0</u> x 5 = <u>0</u> Column Totals: <u>196</u> (A) <u>546</u> (B) Prevalence Index = B/A = <u>2.79</u>
1. <u>Sorbus sitchensis</u>	<u>2</u>		<u>FACU</u>	
2. <u>Ribes laxiflorum</u>	<u>5</u>		<u>FACU</u>	
3. <u>Rubus idaeus</u>	<u>10</u>		<u>FACU</u>	
4. <u>Pronus emarginata</u>	<u>8</u>		<u>FAC</u>	
5. _____				
6. _____				
Total Cover: <u>25</u>				
50% of total cover: <u>13</u> 20% of total cover: <u>5</u>				
Herb Stratum	Absolute % Cover	Dominant Species?	Indicator Status	Hydrophytic Vegetation Indicators: <input type="checkbox"/> Dominance Test is >50% <input type="checkbox"/> Prevalence Index is ≤3.0 <input type="checkbox"/> Morphological Adaptations ¹ (Provide supporting data in Remarks or on a separate sheet) <input type="checkbox"/> Problematic Hydrophytic Vegetation ¹ (Explain) ¹ Indicators of hydric soil and wetland hydrology must be present unless disturbed or problematic.
1. <u>Equisetum fluviatile</u>	<u>70</u>	<u>X</u>	<u>OBL</u>	
2. <u>Epilobium angustifolium</u>	<u>3</u>		<u>FACU</u>	
3. <u>Calamagrostis canadensis</u>	<u>20</u>		<u>FAC</u>	
4. <u>Cornus canadensis</u>	<u>5</u>		<u>FACU</u>	
5. _____				
6. _____				
7. _____				
8. _____				
9. _____				
10. _____				
Total Cover: <u>98</u>				
50% of total cover: <u>49</u> 20% of total cover: <u>20</u>				
Plot size (radius, or length x width) <u>40 ft radius</u> % Bare Ground _____				
% Cover of Wetland Bryophytes _____ Total Cover of Bryophytes _____ (Where applicable)				
Remarks:				

Hydrophytic Vegetation Present? Yes _____ No X

Investigators:
AM/TJ

**Midtown Congestion Relief
PHOTOGRAPHIC LOG**

Date:
09/11/2018

Site Number: 1



Site Number: 2



Investigators:
AM/TJ

**Midtown Congestion Relief
PHOTOGRAPHIC LOG**

Date:
09/11/2018

Site Number: 3



Investigators:
AM/TJ

**Midtown Congestion Relief
PHOTOGRAPHIC LOG**

Date:
09/11/2018

Site Number: 4



Investigators:
AM/TJ

**Midtown Congestion Relief
PHOTOGRAPHIC LOG**

Date:
09/11/2018

Site Number: 5



Site Number: 6



Investigators:
AM/TJ

**Midtown Congestion Relief
PHOTOGRAPHIC LOG**

Date:
09/11/2018

Site Number: 7



Investigators:
AM/TJ

**Midtown Congestion Relief
PHOTOGRAPHIC LOG**

Date:
09/11/2018

Site Number: 8



Site Number: 9



Investigators:
AM/TJ

**Midtown Congestion Relief
PHOTOGRAPHIC LOG**

Date:
09/11/2018

Site Number: 10



Site Number: 11



Investigators:
AM/TJ

**Midtown Congestion Relief
PHOTOGRAPHIC LOG**

Date:
09/11/2018

Site Number: 12



Site Number: 13



Investigators:
AM/TJ

**Midtown Congestion Relief
PHOTOGRAPHIC LOG**

Date:
09/11/2018

Site Number: 14



Site Number: 15



Investigators:
AM/TJ

**Midtown Congestion Relief
PHOTOGRAPHIC LOG**

Date:
09/11/2018

Site Number: 16



Site Number: 17



Investigators:
AM/TJ

**Midtown Congestion Relief
PHOTOGRAPHIC LOG**

Date:
09/11/2018

Site Number: 18



Site Number: 19



Investigators:
AM/TJ

**Midtown Congestion Relief
PHOTOGRAPHIC LOG**

Date:
09/11/2018

Site Number: 20



Site Number: 20

(photo taken 6/24/2020)



SOIL

Sampling Point: 03

Profile Description: (Describe to the depth needed to document the indicator or confirm the absence of indicators.)								
Depth (inches)	Matrix		Redox Features				Texture	Remarks
	Color (moist)	%	Color (moist)	%	Type ¹	Loc ²		
0-2								organics
2-13	2.5YR 4/3	100					sandy-lc	3% gravel
13-26								organics

¹Type: C=Concentration, D=Depletion, RM=Reduced Matrix, CS=Covered or Coated Sand Grains. ²Location: PL=Pore Lining, M=Matrix.

Hydric Soil Indicators:

☐ Histosol or Histel (A1)

☐ Histic Epipedon (A2)

☐ Hydrogen Sulfide (A4)

☐ Thick Dark Surface (A12)

☐ Alaska Gleyed (A13)

☐ Alaska Redox (A14)

☐ Alaska Gleyed Pores (A15)

Indicators for Problematic Hydric Soils³:

☐ Alaska Color Change (TA4)⁴

☐ Alaska Alpine Swales (TA5)

☐ Alaska Redox With 2.5Y Hue

☐ Alaska Gleyed Without Hue 5Y or Redder Underlying Layer

☐ Other (Explain in Remarks)

³One indicator of hydrophytic vegetation, one primary indicator of wetland hydrology, and an appropriate landscape position must be present unless disturbed or problematic.

⁴Give details of color change in Remarks.

Restrictive Layer (if present):

Type: _____

Depth (inches): _____

Hydric Soil Present? Yes _____ No X

Remarks:

HYDROLOGY

Wetland Hydrology Indicators:		Secondary Indicators (2 or more required)
Primary Indicators (any one indicator is sufficient)		
<input type="checkbox"/> Surface Water (A1)	<input type="checkbox"/> Inundation Visible on Aerial Imagery (B7)	<input type="checkbox"/> Water-stained Leaves (B9)
<input type="checkbox"/> High Water Table (A2)	<input type="checkbox"/> Sparsely Vegetated Concave Surface (B8)	<input type="checkbox"/> Drainage Patterns (B10)
<input type="checkbox"/> Saturation (A3)	<input type="checkbox"/> Marl Deposits (B15)	<input type="checkbox"/> Oxidized Rhizospheres along Living Roots (C3)
<input type="checkbox"/> Water Marks (B1)	<input type="checkbox"/> Hydrogen Sulfide Odor (C1)	<input type="checkbox"/> Presence of Reduced Iron (C4)
<input type="checkbox"/> Sediment Deposits (B2)	<input type="checkbox"/> Dry-Season Water Table (C2)	<input type="checkbox"/> Salt Deposits (C5)
<input type="checkbox"/> Drift Deposits (B3)	<input type="checkbox"/> Other (Explain in Remarks)	<input type="checkbox"/> Stunted or Stressed Plants (D1)
<input type="checkbox"/> Algal Mat or Crust (B4)		<input type="checkbox"/> Geomorphic Position (D2)
<input type="checkbox"/> Iron Deposits (B5)		<input type="checkbox"/> Shallow Aquitard (D3)
<input type="checkbox"/> Surface Soil Cracks (B6)		<input type="checkbox"/> Microtopographic Relief (D4)
		<input type="checkbox"/> FAC-Neutral Test (D5)
Field Observations:		
Surface Water Present?	Yes _____ No <u>X</u> Depth (inches): _____	Wetland Hydrology Present? Yes _____ No <u>X</u>
Water Table Present?	Yes _____ No <u>X</u> Depth (inches): > 26"	
Saturation Present? (includes capillary fringe)	Yes _____ No <u>X</u> Depth (inches): > 26"	
Describe Recorded Data (stream gauge, monitoring well, aerial photos, previous inspections), if available:		
Remarks:		

WETLAND DETERMINATION DATA FORM – Alaska Region

Project/Site: MIDTOWN CONGESTION RELIEF Borough/City: ANCHORAGE Sampling Date: 9.11.18
 Applicant/Owner: DOT & PF Sampling Point: 4
 Investigator(s): A.Morrill & T.Jameson Landform (hillside, terrace, hummocks, etc.): Depression
 Local relief (concave, convex, none): Concave Slope (%): 0-1
 Subregion: Southcentral Alaska Lat: 61.18312 N Long: 149.8593 W Datum: _____
 Soil Map Unit Name: Icknuun peat, 0 - 3 % slopes NWI classification: PEM1C

Are climatic / hydrologic conditions on the site typical for this time of year? Yes X No _____ (If no, explain in Remarks.)
 Are Vegetation no, Soil no, or Hydrology no significantly disturbed? Are "Normal Circumstances" present? Yes X No _____
 Are Vegetation no, Soil no, or Hydrology no naturally problematic? (If needed, explain any answers in Remarks.)

SUMMARY OF FINDINGS – Attach site map showing sampling point locations, transects, important features, etc.

Hydrophytic Vegetation Present? Yes <u>X</u> No _____	Is the Sampled Area within a Wetland? Yes <u>X</u> No _____
Hydric Soil Present? Yes <u>X</u> No _____	
Wetland Hydrology Present? Yes <u>X</u> No _____	
Remarks:	

VEGETATION – Use scientific names of plants. List all species in the plot.

Tree Stratum	Absolute % Cover	Dominant Species?	Indicator Status
1. <u>Betula papyrifera</u>	<u>60</u>	<u>X</u>	<u>FACU</u>
2. _____	_____	_____	_____
3. _____	_____	_____	_____
4. _____	_____	_____	_____
Total Cover: <u>60</u>			
50% of total cover: <u>30</u> 20% of total cover: <u>12</u>			
Sapling/Shrub Stratum	Absolute % Cover	Dominant Species?	Indicator Status
1. <u>Alnus viridis</u>	<u>2</u>	_____	<u>FAC</u>
2. <u>Rubus idaeus</u>	<u>5</u>	<u>X</u>	<u>FACU</u>
3. <u>Salix bebbiana</u>	<u>5</u>	<u>X</u>	<u>FAC</u>
4. <u>Myrica gale</u>	<u>15</u>	<u>X</u>	<u>OBL</u>
5. _____	_____	_____	_____
6. _____	_____	_____	_____
Total Cover: <u>27</u>			
50% of total cover: <u>14</u> 20% of total cover: <u>5</u>			
Herb Stratum	Absolute % Cover	Dominant Species?	Indicator Status
1. <u>Calamagrostis canadensis</u>	<u>75</u>	<u>X</u>	<u>FAC</u>
2. <u>Equisetum fluviatile</u>	<u>25</u>	<u>X</u>	<u>OBL</u>
3. <u>Chamaenerion angustifolium</u>	<u>2</u>	_____	<u>FACU</u>
4. _____	_____	_____	_____
5. _____	_____	_____	_____
6. _____	_____	_____	_____
7. _____	_____	_____	_____
8. _____	_____	_____	_____
9. _____	_____	_____	_____
10. _____	_____	_____	_____
Total Cover: <u>102</u>			
50% of total cover: <u>51</u> 20% of total cover: <u>20</u>			
Plot size (radius, or length x width) _____ % Bare Ground _____			
% Cover of Wetland Bryophytes _____ Total Cover of Bryophytes _____ (Where applicable)			
Remarks:			

Dominance Test worksheet:	
Number of Dominant Species That Are OBL, FACW, or FAC:	<u>4</u> (A)
Total Number of Dominant Species Across All Strata:	<u>6</u> (B)
Percent of Dominant Species That Are OBL, FACW, or FAC:	<u>67</u> (A/B)
Prevalence Index worksheet:	
Total % Cover of:	Multiply by:
OBL species <u>40</u>	x 1 = <u>40</u>
FACW species _____	x 2 = _____
FAC species <u>80</u>	x 3 = <u>240</u>
FACU species <u>65</u>	x 4 = <u>260</u>
UPL species _____	x 5 = _____
Column Totals: <u>185</u> (A)	<u>540</u> (B)
Prevalence Index = B/A = <u>2.92</u>	
Hydrophytic Vegetation Indicators:	
<input checked="" type="checkbox"/> Dominance Test is >50%	
<input checked="" type="checkbox"/> Prevalence Index is ≤3.0	
<input type="checkbox"/> Morphological Adaptations ¹ (Provide supporting data in Remarks or on a separate sheet)	
<input type="checkbox"/> Problematic Hydrophytic Vegetation ¹ (Explain)	
¹ Indicators of hydric soil and wetland hydrology must be present unless disturbed or problematic.	
Hydrophytic Vegetation Present? Yes <u>X</u> No _____	

Sampling Point: 4

HYDROLOGY

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WETLAND DETERMINATION DATA FORM – Alaska Region

Project/Site: MIDTOWN CONGESTION RELIEF Borough/City: ANCHORAGE Sampling Date: 9.11.18
 Applicant/Owner: DOT & PF Sampling Point: 7
 Investigator(s): A.Morrill & T.Jameson Landform (hillside, terrace, hummocks, etc.): Swale
 Local relief (concave, convex, none): concave Slope (%): 0-1
 Subregion: Southcentral Alaska Lat: 61.20093 N Long: 149.8674 Datum: _____
 Soil Map Unit Name: Cryothents & Urban land, 5 to 20% slopes NWI classification: PEM1C

Are climatic / hydrologic conditions on the site typical for this time of year? Yes X No _____ (If no, explain in Remarks.)
 Are Vegetation no, Soil no, or Hydrology no significantly disturbed? Are "Normal Circumstances" present? Yes X No _____
 Are Vegetation no, Soil no, or Hydrology no naturally problematic? (If needed, explain any answers in Remarks.)

SUMMARY OF FINDINGS – Attach site map showing sampling point locations, transects, important features, etc.

Hydrophytic Vegetation Present? Yes <u>X</u> No _____	Is the Sampled Area within a Wetland? Yes <u>X</u> No _____
Hydric Soil Present? Yes <u>X</u> No _____	
Wetland Hydrology Present? Yes <u>X</u> No _____	
Remarks: ~100' long triangle, widest ~40'. Revisited site 9/18 & sample was still saturated even after 1 week of continuous sun.	

VEGETATION – Use scientific names of plants. List all species in the plot.

Tree Stratum	Absolute % Cover	Dominant Species?	Indicator Status
1. _____	_____	_____	_____
2. _____	_____	_____	_____
3. _____	_____	_____	_____
4. _____	_____	_____	_____
Total Cover: <u>0</u>			
50% of total cover: _____	20% of total cover: _____		
Sapling/Shrub Stratum	Absolute % Cover	Dominant Species?	Indicator Status
1. _____	_____	_____	_____
2. _____	_____	_____	_____
3. _____	_____	_____	_____
4. _____	_____	_____	_____
5. _____	_____	_____	_____
6. _____	_____	_____	_____
Total Cover: <u>0</u>			
50% of total cover: _____	20% of total cover: _____		
Herb Stratum	Absolute % Cover	Dominant Species?	Indicator Status
1. <u>Calamagrostis canadensis</u>	<u>8</u>		<u>FAC</u>
2. <u>Epilobium ciliatum</u>	<u>5</u>		<u>FAC</u>
3. <u>Carex rostrata</u>	<u>65</u>	<u>X</u>	<u>OBL</u>
4. <u>Phalaris arundinacea</u>	<u>3</u>		<u>OBL</u>
5. <u>Cinna latifolia</u>	<u>20</u>	<u>X</u>	<u>FACW</u>
6. _____	_____	_____	_____
7. _____	_____	_____	_____
8. _____	_____	_____	_____
9. _____	_____	_____	_____
10. _____	_____	_____	_____
Total Cover: <u>101</u>			
50% of total cover: <u>51</u>	20% of total cover: <u>20</u>		
Plot size (radius, or length x width) _____	% Bare Ground _____		
% Cover of Wetland Bryophytes _____	Total Cover of Bryophytes _____		
(Where applicable)			
Remarks:			

Dominance Test worksheet:
 Number of Dominant Species That Are OBL, FACW, or FAC: 2 (A)
 Total Number of Dominant Species Across All Strata: 2 (B)
 Percent of Dominant Species That Are OBL, FACW, or FAC: 100 (A/B)

Prevalence Index worksheet:
 Total % Cover of: _____ Multiply by: _____
 OBL species 68 x 1 = 68
 FACW species 20 x 2 = 40
 FAC species 13 x 3 = 39
 FACU species _____ x 4 = _____
 UPL species _____ x 5 = _____
 Column Totals: 101 (A) 147 (B)
 Prevalence Index = B/A = 1.46

Hydrophytic Vegetation Indicators:
☒ Dominance Test is >50%
☒ Prevalence Index is ≤3.0
☐ Morphological Adaptations¹ (Provide supporting data in Remarks or on a separate sheet)
☐ Problematic Hydrophytic Vegetation¹ (Explain)

¹ Indicators of hydric soil and wetland hydrology must be present unless disturbed or problematic.

Hydrophytic Vegetation Present? Yes X No _____

SOIL

Sampling Point: 7

Profile Description: (Describe to the depth needed to document the indicator or confirm the absence of indicators.)								
Depth (inches)	Matrix		Redox Features				Texture	Remarks
	Color (moist)	%	Color (moist)	%	Type ¹	Loc ²		
0-4								organics
4-11	10YR 3/2	100					Sandy l	5% gravel
11-19	10YR 3/2	100					sandy lc	15% gravel

¹Type: C=Concentration, D=Depletion, RM=Reduced Matrix, CS=Covered or Coated Sand Grains. ²Location: PL=Pore Lining, M=Matrix.

Hydric Soil Indicators: <input type="checkbox"/> Histosol or Histel (A1) <input type="checkbox"/> Histic Epipedon (A2) <input type="checkbox"/> Hydrogen Sulfide (A4) <input checked="" type="checkbox"/> Thick Dark Surface (A12) <input type="checkbox"/> Alaska Gleyed (A13) <input type="checkbox"/> Alaska Redox (A14) <input type="checkbox"/> Alaska Gleyed Pores (A15)	Indicators for Problematic Hydric Soils³: <input type="checkbox"/> Alaska Color Change (TA4) ⁴ <input type="checkbox"/> Alaska Alpine Swales (TA5) <input type="checkbox"/> Alaska Redox With 2.5Y Hue ³ One indicator of hydrophytic vegetation, one primary indicator of wetland hydrology, and an appropriate landscape position must be present unless disturbed or problematic. ⁴ Give details of color change in Remarks.	<input type="checkbox"/> Alaska Gleyed Without Hue 5Y or Redder Underlying Layer <input type="checkbox"/> Other (Explain in Remarks)
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Restrictive Layer (if present): Type: _____ Depth (inches): _____	Hydric Soil Present? Yes <u>X</u> No _____
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Remarks:
Problematic soil: recently developed wetland, at toe of slope, concave topo, Site sample still saturated even with a week of continuous sun

HYDROLOGY

Wetland Hydrology Indicators: <u>Primary Indicators (any one indicator is sufficient)</u> <input type="checkbox"/> Surface Water (A1) <input checked="" type="checkbox"/> High Water Table (A2) <input checked="" type="checkbox"/> Saturation (A3) <input type="checkbox"/> Water Marks (B1) <input type="checkbox"/> Sediment Deposits (B2) <input type="checkbox"/> Drift Deposits (B3) <input type="checkbox"/> Algal Mat or Crust (B4) <input type="checkbox"/> Iron Deposits (B5) <input type="checkbox"/> Surface Soil Cracks (B6)		<u>Secondary Indicators (2 or more required)</u> <input type="checkbox"/> Water-stained Leaves (B9) <input type="checkbox"/> Drainage Patterns (B10) <input type="checkbox"/> Oxidized Rhizospheres along Living Roots (C3) <input type="checkbox"/> Presence of Reduced Iron (C4) <input type="checkbox"/> Salt Deposits (C5) <input type="checkbox"/> Stunted or Stressed Plants (D1) <input type="checkbox"/> Geomorphic Position (D2) <input type="checkbox"/> Shallow Aquitard (D3) <input type="checkbox"/> Microtopographic Relief (D4) <input type="checkbox"/> FAC-Neutral Test (D5)	
Field Observations: Surface Water Present? Yes _____ No <u>X</u> Depth (inches): _____ Water Table Present? Yes <u>X</u> No _____ Depth (inches): <u>10</u> Saturation Present? Yes <u>X</u> No _____ Depth (inches): <u>1</u> (includes capillary fringe)		Wetland Hydrology Present? Yes <u>X</u> No _____	
Describe Recorded Data (stream gauge, monitoring well, aerial photos, previous inspections), if available:			
Remarks:			

